



WOKINGHAM BOROUGH COUNCIL

A Meeting of the **PLANNING COMMITTEE** will be held
Council Chamber - Civic Offices, Shute End, Wokingham
RG40 1BN on **WEDNESDAY 9 JUNE 2021 AT 7.00 PM**

Susan Parsonage
Chief Executive
Published on 1 June 2021

This meeting will be filmed for inclusion on the Council's website.

Note: Although non-Committee Members and members of the public are entitled to attend the meeting in person, space is very limited due to the ongoing Coronavirus pandemic. You can however participate in this meeting virtually, in line with the Council's Constitution. If you wish to participate either in person or virtually via Microsoft Teams please contact Democratic Services. The meeting can also be watched live using the following link:

<https://youtu.be/TAUM2ZyFAB4>



WOKINGHAM BOROUGH COUNCIL

Our Vision

A great place to live, learn, work and grow and a great place to do business

Enriching Lives

- Champion outstanding education and enable our children and young people to achieve their full potential, regardless of their background.
- Support our residents to lead happy, healthy lives and provide access to good leisure facilities to complement an active lifestyle.
- Engage and involve our communities through arts and culture and create a sense of identity which people feel part of.
- Support growth in our local economy and help to build business.

Safe, Strong, Communities

- Protect and safeguard our children, young and vulnerable people.
- Offer quality care and support, at the right time, to prevent the need for long term care.
- Nurture communities and help them to thrive.
- Ensure our borough and communities remain safe for all.

A Clean and Green Borough

- Do all we can to become carbon neutral and sustainable for the future.
- Protect our borough, keep it clean and enhance our green areas.
- Reduce our waste, improve biodiversity and increase recycling.
- Connect our parks and open spaces with green cycleways.

Right Homes, Right Places

- Offer quality, affordable, sustainable homes fit for the future.
- Build our fair share of housing with the right infrastructure to support and enable our borough to grow.
- Protect our unique places and preserve our natural environment.
- Help with your housing needs and support people to live independently in their own homes.

Keeping the Borough Moving

- Maintain and improve our roads, footpaths and cycleways.
- Tackle traffic congestion, minimise delays and disruptions.
- Enable safe and sustainable travel around the borough with good transport infrastructure.
- Promote healthy alternative travel options and support our partners to offer affordable, accessible public transport with good network links.

Changing the Way We Work for You

- Be relentlessly customer focussed.
- Work with our partners to provide efficient, effective, joined up services which are focussed around you.
- Communicate better with you, owning issues, updating on progress and responding appropriately as well as promoting what is happening in our Borough.
- Drive innovative digital ways of working that will connect our communities, businesses and customers to our services in a way that suits their needs.

MEMBERSHIP OF THE PLANNING COMMITTEE

Councillors

Chris Bowring (Chairman)	Angus Ross (Vice-Chairman)	Sam Akhtar
Stephen Conway	Gary Cowan	Carl Doran
Pauline Jorgensen	Rebecca Margetts	Andrew Mickleburgh
Rachelle Shepherd-DuBey	Bill Soane	

ITEM NO.	WARD	SUBJECT	PAGE NO.
1.		APOLOGIES To receive any apologies for absence.	
2.	None Specific	MINUTES OF PREVIOUS MEETING To confirm the Minutes of the Meeting held on 10 March 2021 and the Minutes of the Extraordinary Meetings held on 24 March 2021 and 18 May 2021.	5 - 38
3.		DECLARATION OF INTEREST To receive any declaration of interest	
4.		APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS To consider any recommendations to defer applications from the schedule and to note any applications that may have been withdrawn.	
5.	Barkham	APPLICATION NO 203539 LAND OFF BEARWOOD ROAD, WOKINGHAM Recommendation: Conditional Approval subject to legal agreement.	39 - 88
6.	Emmbrook	APPLICATION NO 202065 - 54 - 58 READING ROAD. WOKINGHAM Recommendation: Conditional Approval subject to legal agreement.	89 - 170
7.	Wescott	APPLICATION NO 203460 - FROG HALL, FROG HALL DRIVE, WOKINGHAM Recommendation: Conditional Approval	171 - 188
8.	Hurst	APPLICATION NO 210805 - "DOLPHIN SCHOOL", WALTHAM ROAD, HURST, WOKINGHAM, RG10 0FR Recommendation: Conditional Approval	189 - 214
9.	Bulmershe and Whitegates	APPLICATION NO 210448 - 57 CHILTERN CRESCENT, EARLEY, WOKINGHAM Recommendation: Conditional Approval	215 - 228

Recommendation: Conditional Approval**Any other items which the Chairman decides are urgent**

A Supplementary Agenda will be issued by the Chief Executive if there are any other items to consider under this heading.

GLOSSARY OF TERMS

The following abbreviations were used in the above Index and in reports.

C/A	Conditional Approval (grant planning permission)
CIL	Community Infrastructure Levy
R	Refuse (planning permission)
LB	(application for) Listed Building Consent
S106	Section 106 legal agreement between Council and applicant in accordance with the Town and Country Planning Act 1990
F	(application for) Full Planning Permission
MU	Members' Update circulated at the meeting
RM	Reserved Matters not approved when Outline Permission previously granted
VAR	Variation of a condition/conditions attached to a previous approval
PS	Performance Statistic Code for the Planning Application
Category	

CONTACT OFFICER

Callum Wernham	Democratic & Electoral Services Specialist
Tel	0118 974 6059
Email	democratic.services@wokingham.gov.uk
Postal Address	Civic Offices, Shute End, Wokingham, RG40 1BN

**MINUTES OF A VIRTUAL MEETING OF THE
PLANNING COMMITTEE
HELD ON 10 MARCH 2021 FROM 7.00 PM TO 9.30 PM**

Committee Members Present

Councillors: Simon Weeks (Chairman), Chris Bowring (Vice-Chairman), Stephen Conway, Gary Cowan, Carl Doran, Pauline Jorgensen, Abdul Loyes, Andrew Mickleburgh, Malcolm Richards, Angus Ross and Rachelle Shepherd-DuBey

Councillors Present and Speaking

Councillors: Shirley Boyt and Alison Swaddle

Officers Present

Callum Wernham, Democratic & Electoral Services Specialist

Neil Allen, Legal Specialist

Roger Johnson, Senior Assistant Engineer - Highways and Transport

Justin Turvey, Operational Manager – Development Management

Case Officers Present

Mark Croucher

Simon Taylor

Natalie Jarman

60. APOLOGIES

There were no apologies for absence.

61. MINUTES OF PREVIOUS MEETING

The Minutes of the meeting of the Committee held on 10 February 2021 were confirmed as a correct record and would be signed by the Chairman at a later date subject to the following minor amendments:

Agenda page 10 – “Malcolm Richards **acknowledged that** the units would have flat roofs **which would minimise the height...**”

Agenda page 10 – “...and queried whether any of the proposed **parking** spaces would be for disabled use.”

62. DECLARATION OF INTEREST

Andrew Mickleburgh declared a prejudicial interest in agenda item 65, on the grounds that he was a Member of the Earley Town Council Planning Committee which had made a recommendation regarding this application. Andrew added that he had formed a view regarding this application, and as such he would not participate in either the discussion or the vote for this item.

63. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

No applications were recommended for deferral, or withdrawn.

**64. APPLICATION NO.203456 - WAINGELS COLLEGE, WAINGELS ROAD,
WOODLEY**

Proposal: Full planning application for the proposed construction of a 3G synthetic pitch consisting of sports fencing, LED floodlights, storage container, spectator area and pedestrian access

Applicant: Mr John O’Keeffe

The Committee received and reviewed a report about this application, set out in agenda pages 15 to 58.

The Committee were advised that the Members’ Update included:

- Condition 19 to be headed “Drainage Implementation and Maintenance”;
- Additional Condition 20;
- Confirmation that the applicant had no intention for the floodlights to be retractable;
- Contextual information regarding the 3db noise increase;
- Additional photographs of the site along Waingels Road and from within the school campus.

Keith Baker, Woodley Town Council, spoke in objection to the application. Keith stated that having just 24 hours notification prior to Committee had not given speakers adequate time to prepare for this meeting. As such, no residents had registered to speak. Keith was thankful for the Woodley Town Council staff for notifying him of this item coming to Committee, who themselves were notified around 24 hours prior to Committee. Keith felt that the lack of notification was unacceptable and undemocratic. Keith stated that he supported additional sports facilities within the Borough, however this application would result in the loss of total pitches. Keith added that there were existing flooding issues on Waingels Road, however he was grateful for the case officer’s reassurances that this application would not add to these issues. Keith stated that this application would place further traffic on to the Waingels Road, with the traffic from the Charvil direction having to travel the whole length of the road to reach this facility.

Alison Swaddle, Ward Member, spoke in objection to the application. Alison stated that the lack of notice regarding this item coming to Committee was unacceptable. Alison shared her thanks to Woodley Town Council officers for their quick actions when informed that this item was coming to Committee. Alison stated that a Scout’s centre was approved at the previous meeting of the Planning Committee which would add additional traffic to the Waingels Road. Alison added that the proposed 5m high wire fence would have an impact on roosting birds and bats in the area. Adding to this, Alison stated that she could not find a bat survey or an on-site investigation report on the impact to birds as a result of the proposals. Alison concluded by stating that although she supported the provision of high class sporting facilities within the Borough, she could not support this application due to a number of outstanding issues.

Simon Weeks sought clarification that the final closing time for the facility was 9.15pm, queried whether the proposals would create an acceptable level of additional traffic on Waingels Road, and queried whether the issue of bats had been suitably considered. Simon Taylor, case officer, stated that the whole facility including the floodlights had a final closing time of 9.15pm. Regarding the issue of bats. Simon Taylor stated that the ecology officer was comfortable that a bat survey had not been submitted, as the hours of use meant that bats would primarily only be affected in the months of May and September for a small crossover period of time each evening. Roger Johnson, Senior Assistant Engineer – Highways, stated that traffic on the Waingels Road would increase as a result of the proposals, however this would predominantly take place during the quieter periods of the day and therefore it was not considered an issue.

Andrew Mickleburgh queried whether the proposals would create additional water run-off, queried whether Waingels Road was ever closed as a result of flooding, queried why a bat survey was not deemed necessary, and queried whether a specific figure should be attached to the biodiversity net gain requirement. Simon Taylor stated that the proposals would not create any additional surface water run-off compared to the current site. In addition, Thames Water were planning to carry out works on the Waingels Road in the future which could help the area. Regarding the absence of a bat survey, Simon stated that officers had taken note and were comfortable with the proposals as the closing time of 9.15pm minimised any harm. In addition, bats would also hibernate for between 6 to 7 months of the year. Referring to biodiversity net gain, Simon stated that officers were satisfied that the proposals would produce a suitable net gain of biodiversity.

Pauline Jorgensen queried whether there was another suitable part of the site to accommodate the proposals which was not in designated countryside, and queried why consultees had been given late notice of items coming to this meeting. Simon Taylor stated that all playing fields on the site were located within designated countryside, and policy CP11 allowed for recreational facilities in the countryside. Simon added that although the proposals included some urbanising elements, the proposals were still deemed acceptable. Justin Turvey, Operational Manager – Development Management, stated that officers aimed to tell Members and residents that items were heading to Committee on the day of agenda publication. On this occasion, there was an issue which meant that some consultees were not informed with the usual one weeks' notice. All consultees had been emailed, and all previous comments had been taken into account.

Rachelle Shepherd-DuBey queried why the proposals did not include focussed lighting, and queried whether the proposed 3G pitch would increase flooding. Simon Weeks clarified that the proposed 3G pitch would not cause any additional water run-off when compared to the existing pitches. Simon Taylor stated that the proposals included 15m high floodlights which were downward focussed and provided a satisfactory outcome in terms of light spill, whilst being consistent with other recent approvals at the school and in the area.

Gary Cowan queried what hours of use had been applied to the Luckley House School application. Simon Taylor stated that the original proposal for Luckley House School was for a 10pm finish time, which was then amended to be variable from 6.30pm in March to 9pm in the summer. Luckley House School was slightly different as the pitch itself adjoined a woodland.

Gary Cowan felt that he could not support the proposals as there were a lot of on balance conclusions within the officer report. In addition, Gary was of the opinion that an ecological appraisal should have been carried out for this application, and felt that trees were proposed to be cut down without assurances that they would be suitably replaced. Regarding the proposal to fell trees, Simon Taylor clarified that 3 trees were of substandard quality with the 4th tree being more sizeable however it was easily replaceable.

Simon Weeks commented that the trees lost at the temporary entrance should be replaced afterwards in situ.

Abdul Loyes stated that the traffic on Waingels Road was very poor, and was of the opinion that this item should be deferred in order to receive answers regarding some outstanding issues. Simon Taylor stated that the proposals would allow for some additional

afterschool sports activities on the site, which could extend the pickup and drop-off times and thereby reduce traffic on the road at peak times.

Stephen Conway commented that whilst he was sympathetic to the concerns raised both on the evening and during the consultation period, as a Committee Member, he had to listen to the professional advice which stated that the proposals were acceptable. The Committee would need to come up with specific technical reasons in order to present a robust case to overturn the recommendation.

Malcolm Richards queried whether the 9.15pm cut-off time was the time that the site must be vacated. Simon Taylor stated that due to the community use of the site, most bookings would be for 30 minute or 60 minute slots, meaning that a slot would end at 9pm allowing time for users to vacate the site prior to the lights switching off automatically at 9.15pm.

Angus Ross stated that many of the concerns had been addressed either within the report or on the evening by officers. Angus added that the site currently had 3 pitches on site now which were likely to be used, and the proposals would provide a surface which could be used in poorer weather conditions and would possibly make the use of the site more regularised. Angus stated that the community use agreement would allow residents to raise concerns should aspects of the usage of the site become troublesome. Angus stated that he would support the application.

Carl Doran queried what existing cycling infrastructure was present in the area as a comment within the report stated that the existing cycling infrastructure was adequate, and queried why trees had to be felled for the temporary access. Roger Johnson stated that he could not state what cycling infrastructure was available in the area. Simon Weeks clarified that the tree removal was to reduce interference with a better specimen tree and to remove a dead tree stem.

Chris Bowring stated that he was supportive of the application, and noted that the facility would bring a range of community benefits.

RESOLVED That application number 203456 be approved, subject to conditions and informatives as set out in agenda pages 16 to 23, amendment to the heading of condition 19 and additional condition 20 as set out in the Members' Update.

65. APPLICATION NO.203534 - LODDON COURT, LAMBS LANE, SWALLOWFIELD

Proposal: Full application for the proposed change of use of land from agricultural to mixed equestrian/agriculture plus erection a stable buildings with associated hardstanding. (Part Retrospective)

Applicant: Mr Kingsbury

The Committee received and reviewed a report about this application, set out in agenda pages 59 to 82.

The Committee were advised that there were no Members' Updates.

Emily Temple, agent, spoke in support of the application. Emily thanked officers for their thorough report. Emily stated that the family were expert horse owners, and the family itself was quite large and required space for them and their horses to live, and as such the use of the site would not be commercial. Emily added that the base of the stable buildings

had been constructed, however construction ceased when they were advised that full planning permission was required. Once the application was submitted, full reports related to trees and ecology were submitted. Emily stated that the stable would be timber clad in appearance, and away from the road and vantage points. The application would allow for mixed use to allow for grazing of both sheep and horses. Emily concluded by stating that no other changes were proposed to the wider land on the site.

Andrew Mickleburgh queried whether any additional large trees would be harmed by the proposals, and queried whether named permission could be applied to the family. Mark Chancellor, case officer, stated that the tree officer was happy with the proposals, as encroachment of the root protection area of the large tree was around 17.5 percent, and it was at around 20 percent when arboriculturists usually raised concerns. In addition, holes would be drilled on the base of the structure and the existing storage would be removed to help protect the tree. Mark added that officers were content with the relationship between the closer large tree and the proposed structures, and therefore the other large trees situated further away from the proposed structures would also be acceptable. Regarding personal permission, Mark stated that it would be unreasonable to apply a personal permission as the permission was being applied to the owners of the property, and planning policy guidance stated that personal permission should be avoided wherever possible.

RESOLVED That application number 203534 be approved, subject to conditions and informative as set out in agenda pages 60 to 61.

66. APPLICATION NO.203514 - 34 HILLTOP ROAD, EARLEY

Andrew Mickleburgh declared a prejudicial interest in this item and therefore took no part.

Proposal: Full planning application for the proposed conversion of bungalow to form two self-contained dwellings comprising 1 x 3 bed apartment at ground floor and 1 x 1 bed apartment at first floor. (Following recent construction of extensions permitted by application 191411) and installation of four roof lights (part retrospective)

Applicant: Mr J Singh

The Committee received and reviewed a report about this application, set out in agenda pages 83 to 106.

The Committee were advised that the Members' Update included additional conditions 7, 8, 9, and an additional informative.

Brenda Cutler, resident, spoke in objection to the application. Brenda stated that the area was already overdeveloped, with parking already being an issue. Brenda added that the proposals would create 6 properties in the space of the original single dwelling. Brenda stated that the flats would be out of keeping with the character of the area, and would add to parking issues. Brenda concluded that residents were upset with the proposals, and cited that number 30 Hilltop Road had been divided into two dwellings which had created issues including additional traffic.

Tim Marsh, on behalf of the ACER residents' association, spoke in objection to the application. Tim asked that a glazed window be provided on the proposed roof light, as it was only 1 metre in height rather than the standard height of at least 1.7 metres. Tim

added that the parking standards related to new developments, rather than within existing housing stock. Tim stated that development of rental properties within existing housing stock generally resulted in more vehicles within an already busy residential area. Tim was of the opinion that the proposals were out of keeping with the character of the area whilst being an example of overdevelopment. Tim asked that the application be refused.

Shirley Boyt, Ward Member, spoke in objection to the application. Shirley stated that a previous application on this site granted permission to turn habitable rooms into bedrooms, and this application followed up by turning a single property into an 8 bedroom dual property. Shirley was of the opinion that there was potential for the site to house 8 permanent residents, with only 4 car parking spaces. Shirley stated that the new dropped kerb was not present on the site drawings, and there would be no access to the front of the site for vehicles to park. Shirley added that soft landscaping was recommended for this scheme, however implementation was a different matter in this area.

Carl Doran queried 4 parking spaces met parking requirements, queried where the conditioned soft landscaping would go if the proposed car parking spaces went ahead, queried how the upper floor flat would use the rear amenity space, and queried why the roof light at 1 metre height was not considered as overlooking. Roger Johnson, Senior Assistant Engineer – Highways, stated that the parking requirements were met via the provision of 4 car parking spaces. Natalie Jarman, case officer, stated that 4 car parking spaces already had planning permission with no landscaping condition attached, and should this application be approved than landscaping would be conditioned which would provide betterment to the street scene. Regarding access to the amenity space, Natalie Jarman stated that the plans showed that both proposed properties would have access to the amenity space and refuse storage via gated access. Simon Weeks stated that he had sympathy regarding the concerns with the roof light, and suggested that the Committee may wish to condition the window to be fix shut and obscure glazed. Carl Doran stated that he would second this proposal should the Committee be minded to approve.

Rachelle Shepherd-DuBey queried where the landscaping would fit in amongst the driveway. Justin Turvey, Operational Manager – Development Management, stated that landscaping could be placed to the right hand side of the driveway, and in front of the bay window.

Adbul Loyes queried how policy CP3 applied to this application. Natalie Jarman stated that this application was starting at a point where planning permission had already been granted for the extensions. There could be some introduction of residential paraphernalia, however the only physical change would be the introduction of the roof lights. Based on this, officers felt that it was acceptable in terms of the character of the area.

Pauline Jorgensen queried whether there was a risk that a future application could convert the garage to a dwelling with side access. Simon Weeks clarified that the garage would provide storage for residents, and access needed to be maintained. Simon added that the Committee could not place a condition on a possible future application.

Stephen Conway stated that the Committee was being asked to judge the proposed change from two dwellings to one. Stephen felt that it was imperative to have obscure glazing on the roof light window. Stephen queried that as the rear window of the first floor dwelling would overlook the rear garden, whether this was acceptable in this specific instance. Natalie Jarman stated that on balance, the rear gardens would provide some

form of private amenity space. In many instances, flats would have a solely communal outdoor amenity space.

Malcolm Richards queried how 4 vehicles would could safely get in and out of the driveway. Natalie Jarman stated that the photo presented to the Committee was taken before the works, and it was proposed to extend the dropped kerb to allow the vehicles to reverse or drive straight on to the driveway.

Simon Weeks proposed that the roof light window on the flank of the property be conditioned to be obscure glazed and fixed shut. This was seconded by Carl Doran and carried by the Committee.

Carl Doran queried whether the issue of a window overlooking a neighbouring private garden went against the Borough Design Guide. Justin Turvey stated that it was standard for maisonettes to overlook rear amenity space from the upper floors.

RESOLVED That application number 203514 be approved, subject to conditions and informative as set out in agenda pages 84 to 85, amended condition requiring the roof light to be obscure glazed and fixed shut as agreed by the Committee, additional conditions 7, 8, 9 and additional informative as set out in the Members' Update.

67. APPLICATION NO.210022 - 33 COTTESMORE ROAD, WOODLEY

Proposal: Householder application for the proposed erection of a first floor side extension and part two storey part single storey rear extension, plus conversion of the garage to habitable accommodation and changes to fenestration

Applicant: Amandeep Garcha

The Committee received and reviewed a report about this application, set out in agenda pages 107 to 124.

The Committee were advised that the Members' Update included clarification that the revised plans were received on 9th February, and then re-advertised on 10th February.

Jenny Cheng, Woodley Town Council, spoke in objection to the application. Jenny stated that when Woodley Town Council first saw the planning application for this large extension, their reaction was that it was huge and it would change the street scene and make number 33 Cottesmore Road appear very different to other properties. Jenny added that the first version of this application had the first and second storeys extend to the boundary, creating a terracing effect. Jenny stated that the current application had not done enough to mitigate the issues caused by the extension. Jenny felt that there was insufficient parking provision considering the proposed 5 bedrooms within the extended house, whilst it would be impossible for three cars to come off of the road at the same time to park as there was a wall in the way which was not proposed to be demolished as part of this planning application. Jenny added that the extension would cause loss of light for neighbouring properties, and urged the Committee to refuse the application.

Barry Morfett, on behalf of a neighbour, spoke in objection to the application. Barry stated that he was speaking on behalf of one of the owners of a neighbouring property, Mrs Jane Plank. Barry stated that he and Mrs Plank would like to support the reasons for refusal as outlined by Woodley Town Council. In addition, the proposed extension would cause a significant loss of light to Mrs Plank's first floor landing window and rear garden. The loss

of light would require a house light to be on when using the first floor landing, which was not the case at present. When the properties were originally built, those with rear gardens were designed to ensure that they had a significant amount of natural light. Barry stated that the Borough Council's own planning guidance cited loss of light as a reason to refuse. Should this application be approved, it would ruin 25 years of careful gardening by Mrs Plank, and prevent future plantings. Should the application be approved, Mrs Plank requests that the side extension element be restricted to ground floor only. Barry concluded by stating that other concerns relating to inadequacy of parking provision, overdevelopment and terracing remained.

Simon Weeks sought clarification as to whether the proposals met parking standards. Roger Johnson, Senior Assistant Engineer – Highways, stated that the proposals would meet parking standards, and officers have asked for an extended dropped kerb allowing for each car to access its own parking space without the need to move other cars. Mark Croucher, case officer, confirmed that planning permission was not required to remove the wall in front of the driveway.

Simon Weeks sought clarification regarding the loss of light to the neighbouring first floor landing window. Mark Croucher stated that officers had conducted the 45 degree test, and found that the proposals would not breach this for habitable windows. Mark added that the two storey element of the proposals had been moved back from the boundary by 1 metre. Mark stated that from the existing neighbouring garage, the extension would extend approximately 2.2 metres beyond that which was fairly modest, which was considered acceptable and met the tests within the Borough Design Guide. Mark clarified that the landing window area would not be classed as a habitable room, and the loss of light to the garden would primarily occur in the morning hours.

Simon Weeks queried whether the applicant could use permitted development rights to develop a single storey rear extension which was significantly larger than the proposed extension of 2.2 metres. Mark Croucher stated that the applicant could develop a single storey extension of 3 metres under permitted development.

Angus Ross queried why there was no landscaping requirement for this application. Mark Croucher stated that some landscaping had been removed at the front of the property under permitted development. The front of the property had a drainage system, which meant that any hard surfacing would meet the drainage requirements. Where the proposals were in situ, and were lawful, landscaping requirements could not be imposed.

Malcolm Richards queried whether there was a percentage limit on extensions in urban areas. Mark Croucher stated that there was no percentage limit on extension increases in residential areas, and each application was based on the merits of how the proposals would impact neighbours and the appearance of the proposals.

RESOLVED That application number 210022 be approved, subject to conditions and informatives as set out in agenda pages 108 to 109.

**MINUTES OF A VIRTUAL MEETING OF THE
PLANNING COMMITTEE
HELD ON 24 MARCH 2021 FROM 7.00 PM TO 9.53 PM**

Committee Members Present

Councillors: Simon Weeks (Chairman), Chris Bowring (Vice-Chairman), Gary Cowan, Carl Doran, Pauline Jorgensen, Abdul Loyes, Andrew Mickleburgh, Malcolm Richards, Angus Ross and Rachelle Shepherd-DuBey

Councillors Present and Speaking

Councillors: Michael Firmager, Jim Frewin and Charlotte Haitham Taylor

Officers Present

Connor Corrigan, Service Manager - Planning and Delivery
Judy Kelly, Highways Development Manager
Mary Severin, Borough Solicitor
Callum Wernham, Democratic & Electoral Services Specialist

Case Officers Present

Andrew Chugg
Christopher Howard

68. APOLOGIES

An apology for absence was submitted from Councillor Stephen Conway.

69. DECLARATION OF INTEREST

There were no declarations of interest.

70. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

No applications were recommended for deferral, or withdrawn.

71. APPLICATION NO.210387 - LAND SOUTH OF CUTBUSH LANE (WEST OF OLDHOUSE FARM LANE) AND GATEWAY 4 PLOT AT TVSP

Proposal: Full application for the erection of Film studio stages and workshops (for a temporary period of 5 years). To include access to the site via Old House Lane / Cutbush Lane, car parking, ancillary buildings to support the use of the site and landscaping, with a workshop to be included on Gateway 4 plot at Thames Valley Science Park.

Applicant: Shinfield Studios

The Committee received and reviewed a report about this application, set out in agenda pages 5 to 92.

The Committee were advised that the Members' Update included:

- Correction that the size of the buildings should read 14,080m²;
- Various amendments of recommendations A and C;
- Updated list of plans associated with condition 2;
- Updated document reference for condition 8, and updated Construction Environmental Management Plan reference;
- Amendment of condition 17 to remove reference to hard landscaping as these details had since been approved;

- Additional condition related to a lighting plan;
- Clarification that 30 cycle spaces were proposed;
- Clarification that the Ecology Officer raised no objections and the package of offsite biodiversity enhancements had been secured;
- Correction to paragraph 10, to read 2017 rather than 2007;
- Correction to paragraph 51, to state 200m² of solar panels rather than 200 panels as stated;
- Correction to paragraph 61, to state that 10 blue badge spaces would be provided rather than the 9 stated;
- Letter of full support from Arborfield and Newland Parish Council;
- Two letters of support from the Department of Digital, Cultural, Media and Sport outlining the economic gains of the media sector;
- 4 further letters of support and a further letter of objection from residents;
- Letter of objection from the owners of Shinfield Grange – University College Estate of Management.

Nick Smith, applicant, spoke in support of the application. Nick stated that despite the current economic downturn, the film industry was 'booming', however a lack of studio space was limiting the growth of this sector. Nick added that there was a real and significant opportunity with this application to put Shinfield, Wokingham, and Berkshire on the global film studio map. Nick stated that the inward investment of millions of pounds in addition to the local job creation was significant, which would have lasting long terms benefits going forwards. Content production was the fastest growing industry in the UK, with quarter four of 2020 generating £1.2 billion of inward investment. Nick stated that there was an acute shortage of studio space to meet the needs of big names such as Disney, Netflix and Apple, and the Government was targeting a 65 percent growth which equated to approximately 1.8 million square feet of studio space across the UK. Provision of these spaces would benefit communities, creating investment opportunities and many new jobs. Nick added that a typical production would spend £80m to £100m in any 12 month period, with a significant proportion of this spend occurring locally. In addition, a typical production would require around 500 crew, which would create a number of local highly paid job opportunities. Nick stated that he hoped to be able to bring these benefits to Wokingham and Shinfield, and extended his thanks to Shinfield Parish Council and local residents for their support.

Nick Paterson-Neild, agent, spoke in support of the application. Nick stated that national policy supported the creative sector, which played an important role in supporting collaboration, innovation, productivity and sustainability whilst driving the economic prospects of the local area, whilst expressly supporting additional studio capacity. Nick added that this was the first step towards developing the University City Valley proposal. Nick concluded by stating that the development would allow for collaboration between creative content providers, whilst driving inward investment and providing local jobs.

Jim Frewin, Ward Member, spoke in support of the application. Jim stated that he was broadly in favour of the proposal and fully understood the need for the speed of this application, however it was important to ensure that planning policy was applied equally to all applicants. Jim asked that officers confirm that all due process had been applied to this planning application. Jim asked for clarity and confirmation regarding the officer comment that the development would not have a severe impact on the local highways network, and asked for analysis that confirmed this particularly for the M4 junction 11 blackboy roundabout on Lower Earley Way. Jim queried what modelling and analysis had been carried out to confirm contradictory statements within the report, one stating that there

would be significant morning and evening peak travel patterns, whilst the other stated that traffic would be spread out more evenly throughout the day. Jim stated that this development was proposed to be carried out on top of a number of other ongoing developments within the community, and queried whether a cumulative community impact had been undertaken. Jim stated that the local sewerage system had regularly failed residents in the area, and noted that the report stated that the development would not impact on this further, and asked for confirmation that this was indeed the case. Concluding Jim asked how many 100 year flood occurrences had occurred within the last 5 years, as this area regularly flooded.

Charlotte Haitham Taylor, Ward Member, spoke in support of the application. Charlotte stated her disappointment that the temporary studios were proposed on mainly open countryside, contrary to policy CP11, when other land within the science park had not been used first. Charlotte added that it was therefore especially important that the S106 agreement was secured in a timely fashion in order to secure measures to mitigate against this, and to agree a plan to dismantle the site should permanent permission not be granted. Charlotte asked that if the Committee were minded to approve the application, that the Chairman of the Planning Committee would be granted full oversight of the S106 agreement. Charlotte suggested that the Planning Committee may wish to put further detail into the landscape and planting, as the studios would be 18m high and would dominate the neighbouring countryside and college at Shinfield Grange, contrary to policies CP3, CC03 and TB21. Charlotte was of the opinion that further conditions should be required, to ensure the planting of further mature trees in order to protect visual amenity in a timely fashion. Charlotte was of the opinion that the current sewerage system did not have the capacity for a development of this scale. On balance, Charlotte stated that she was in favour of the proposals as it would, in time, bring with it economic prosperity to the area whilst creating hundreds of new jobs in a variety of different roles.

Simon Weeks queried whether Old House Farm was residential, queried whether the delay of further works to the science park would reduce the pressure on the upgraded road network, queried whether the longer operating time of the site would reduce peak traffic concerns, and queried what colour the major buildings would be as this would have an impact on the development's impact on the countryside. Christopher Howard, case officer, stated that there was a residential element to the Old House Farm which was located around 100m away from the proposed development. Christopher stated that part of the current proposals included that further works to the Science Park reserved matters would not progress until further modelling had been carried out, which would reduce traffic impact on the roads. Regarding the extended operating times of the site and the impact on the road network, Judy Kelly, Highways Development Manager, stated that the daily profile of the site meant a 7am to 7pm usage, which would peak before the normal network peak in the morning and after the usual 6pm network peak. Judy added that the site would create approximately 70 peak hour vehicle movements which could be accommodated by the road network. Relating to the colour and appearance of the proposed main buildings, Christopher Howard stated that they would be dark brown and grey clad with additional landscaping, which made the proposals acceptable.

Andrew Mickleburgh noted the supportive comments from the Parish Council and local residents, and the local jobs that the proposals would create. In addition, Andrew noted the concerns raised by the local Ward Members. Andrew queried what the parking standards were for the site and whether all parking needed to be provided on site by condition, queried whether the modelling done with Shepperton Studios was a like for like comparison, queried whether the additional trips related to the proposals had been

modelled in relation to increased vehicle emissions, queried how far the traffic modelling had been carried out geographically, queried how the proposals were considered acceptable in terms of appearance within the context its countryside surroundings, and queried why the application was for a temporary period rather than permanent. Judy Kelly stated that although the proposals would create 600 new jobs, not all personnel would be on site at the same time. There were no specific parking standards for film studios, however other similar studios had been looked at for comparison. Modelling suggested that 175 spaces would be needed in the worst case, and 181 spaces would be provided. In addition, the area was well connected via bus routes and greenways. In relation to the comparison with Shepperton Studios, Judy stated that this had been carried out on a per metre squared basis. Judy stated that all major junctions which could be affected by the proposals had been included in the modelling. The University of Reading had agreed not to bring forward the larger science park site until further modelling had been carried out, which would reduce the pressure on the road network. Christopher Howard stated that the trip generation would be much less than the amount consented for at the wider science park, which would result in a net reduction of vehicle emissions compared to the consented amount. Christopher Howard clarified that the appearance of the proposals had to be viewed in the context of it being a temporary structure. In relation to the application for temporary permission, Christopher stated that this was a decision made by the applicant, and all buildings would be required to be removed and the land restored by the end of the 5 year permission should an extension or application for permanent permission not be submitted and approved.

Rachelle Shepherd-DuBey queried how many apprenticeships would be offered on site, queried how the site would be heated, and queried whether additional photovoltaic panels could be installed to a total equivalent of 25 percent of the energy usage of the site. Christopher Howard stated that the S106 agreement required provision of apprenticeships on site or a financial contribution to the work skills plan. Christopher clarified that the workshop and stage buildings would not require heating due to the specifics of how the buildings would be constructed. Christopher clarified that the size of the stage building restricted the total number of photovoltaic panels that could be provided on site.

Abdul Loyes stated that the Shepperton Studios had very little residential dwellings nearby and therefore may not be comparable in highways terms. Abdul queried whether the expected noise levels would be acceptable. Judy Kelly stated that whilst Shepperton was more remote, the trip rate that had been used was a worst case scenario. As such, the proposals were possibly overestimating the trip rates and parking requirements as this site would have better access and public transport links. Regarding the issue of noise, Christopher Howard stated that conditions 21 and 22 related to this, and added that many loud noises heard in films or television shows were edited in post-production. In addition, no complaints had been received regarding noise from residents nearby to the studios located in Arborfield.

Chris Bowring stated that there were positives and negatives to this application. The positives included additional local jobs and investment, whilst the negatives included development within the countryside and an 18m high building which would result in poorer amenity within the countryside. Chris queried whether workshop 4 had existing planning permission, and queried whether it would be possible to excavate below the site in order to reduce the 18m building height. Christopher Howard confirmed that workshop 4 had existing planning permission under outline consent as a part of the science park, however it did not have consent to be used as a film studio. Christopher clarified that the science park gateway building was 15m in height, whilst the British Museum building was 12m in

height. Excavation below the site would limit access provision and drainage. Simon Weeks commented that excavation could be an option if the application was for permanent permission, however the limited period of use created different circumstances.

Pauline Jorgensen stated that she was largely in favour of the proposals. Pauline queried how the land would be restored at the end of the 5 year permission, should it be granted. Christopher Howard stated that condition 3 related to restoration of the site, and this was enforceable. Connor Corrigan, Service Manager – Planning and Delivery, stated that this was a standard condition for many schemes of a temporary nature. Connor added that should the applicant fall in to liquidation, responsibility to restore the land would fall to the landowner, the University of Reading.

Malcolm Richards queried what mitigation would be in place for local residential properties should deliveries for construction take place overnight to allow for early morning construction, queried whether the buildings being a green colour could allow them to fit in better, and queried whether any trees would be felled as a part of this application. Christopher Howard stated that apart from Old House Farm, the nearest residential property was 200m away. In addition the buildings would be fabricated offsite and the site was located in an isolated area. Christopher added that construction traffic would not pass any local residential properties. Christopher stated that the proposed materials and colours were similar to the British Museum buildings, which fitted in well with its surroundings. Christopher confirmed that some hedgerows would be cut along Cutbush Lane, however these would be compensated via landscaping conditions and biodiversity net gains. In addition, the remediation plan would seek to secure additional planting at the end of the permission period.

Gary Cowan was of the opinion that once the principle of development had been established, it was very likely that an application for permanent permission would be recommended for approval by officers. Gary added his agreement with the statements made by local Ward Members. Gary had concerns regarding the safety of the access proposals to the site, and sought assurances regarding this. Judy Kelly state that access to the site would not be off the junction of the Eastern Relief Rad with Cutbush Lane, as all traffic would go from the Eastern Relief Road roundabout junction via the link road through the science park. Judy stated that there would be no opportunity for vehicles to use the whole length of Cutbush Lane to gain access to the site.

Carl Doran was of the opinion that the temporary nature of this application was messy. Carl queried whether cyclists and pedestrians could still use Cutbush Lane, and queried the need for a 3m high boundary fence. Judy Kelly confirmed that Cutbush Lane would only be stopped for vehicle movements, and would therefore allow for pedestrians and cyclists to pass through in addition to emergency vehicles. Christopher Howard stated that the 3m fence was required due to the operational aspect of the studio, and would be of a mesh design.

RESOLVED That application number 210387 be approved, subject to conditions and informatives as set out in agenda pages 6 to 16, various amendments to conditions as set out in the Members' Update, and additional condition as set out in the Members' Update.

72. APPLICATION NO.201833 - LAND SOUTH OF OLD BATH ROAD, SONNING

Proposal: Outline application with Appearance and landscaping reserved for the proposed 57 dwellings suitable for older persons accommodation (Use Class C3) with consideration

of means of access, layout and scale to be determined. Following demolition of existing dwellings.

Applicant: Arlington Retirement Lifestyles

The Committee received and reviewed a report about this application, set out in agenda pages 93 to 136.

The Committee were advised that the Members' Update included:

- Amendment to condition 16;
- Correction to the correct affordable housing contribution in lieu.

Trefor Fisher, Sonning Parish Council, spoke in objection to the application. Trefor stated that Sonning Parish Council were shocked that this application had been recommended for approval, as Sonning was a designated limited development location, and this area in particular was characterised by detached houses with large gardens. Trefor was of the opinion that this development would be an overdevelopment of the site. Trefor stated that a similar proposal at Linkside was refused as the density of 32 dwellings per hectare was regarded as too high, whereas this proposal would result in 48 dwellings per hectare. Trefor stated that Sonning was a very small community, and this development would add almost 10 percent to the total number of households in Sonning. Trefor added that this area was dangerous for pedestrians, and the Parish Council was frequently made aware of speeding vehicles at crossing points. Trefor stated that pedestrians would have to cross the railway bridge with no pedestrian pathway to reach Woodley for shopping and amenities. Trefor added that should this scheme be allowed, many extra vehicles would have to pass over the Sonning Bridge which was already had significant traffic issues. Trefor stated that light pollution was a common complaint for residents, which would be made worse should the application be approved. Trefor added that the neighbourhood development plan was underway, and thus far there were no comments in favour of development at this specific site. Trefor asked that should the Committee be minded to agree to this development proposal, that their decision be delayed until the outcome of the questionnaire be known.

Paul Etherington, resident, spoke in objection to the application. Paul stated that the plan was for large developments to take place on two sides of his property, with the golf course green space being built on and then potentially this development in addition. Paul stated that he had sat on the Planning Committee at Maidenhead for a number of years, and therefore appreciated the large bundle of papers that Members had to consider. Paul felt that it was difficult for Members to get a proper feel for the area, however it was recognised that Sonning had no GP surgery, no dentist, no walkable shops, churches, bars or restaurants. With all of these factors taken into account, this development would create a large number of additional vehicle movements including delivery vehicles. Paul stated that the local rugby club had been enjoying success as of late, and had just built a brand new stand. On match days the entirety of Old Bath Lane, Pound Lane and other surrounding roads had cars parked on the pavements and verges, leaving no room for emergency services vehicles. Paul felt that the lack of local amenities meant that this development would cause many more vehicle movements than stated within the report. Paul stated that although Highways had no objections, they also had no objections to the golf course scheme whereby a fatality occurred a year after in a spot which locals knew was dangerous. Paul was not convinced that there was a demand for these units, as a

similar development in Twyford, close to local amenities, was not fully sold after a couple of years of being open.

Tim Burden, agent, spoke in support of the application. Tim stated that this proposal would meet the needs of a specific sector of the community. Tim added that it was recognised that this was an unusual site, especially when considering the bulk and mass of the adjoining Sunrise care home. Since the submission of this application, the scheme had been amended and consequently there were no outstanding objections from the Council's internal consultees. Tim stated that Highways officers were satisfied with the approach to car parking provision, as it had been demonstrated that there was a low level of car ownership by residents at retirement schemes. Tim added that the access to the site was deemed acceptable, and the proposals fully complied with all standards and policies. Tim stated that the proposal would be set back from the boundary, and there was planned reinforcing of trees and hedgerows in addition to enhanced planting. Tim added that high quality trees, and trees of value, would be retained. Tim stated that full contributions would be made in terms of affordable housing, and the proposals would cause no material harm to amenity values and the character of the area.

Michael Firmager, Ward Member, spoke in objection the application. Michael stated that he was extremely surprised that this application was recommended for approval, as the development was out of character with the area. Michael added that the proposals were overbearing at a height of 3 storeys, in an area where properties were predominantly 2 storeys in height. Michael felt that the overbearing nature of the development would result in a loss of privacy for local houses, especially due to the proposed balconies. Michael stated that this development would result in overdevelopment of an already congested area, especially when considering that the side road to the development already provided access to the busy rugby club. Michael added that the access to this proposed development would be onto the A4, which was an already extremely busy road with speeding traffic. In addition the site had poor bus links, and was located far away from any local amenities which would result in many additional vehicle movements. Michael was of the opinion that the parking allocation of 63 spaces for 57 flats was not acceptable, and there was not enough visitor parking provision. Michael added that removal of trees went against the declared climate emergency, in addition to the resulting loss of natural habitation and biodiversity should this application be approved. Michael stated that there was a strength of feeling across the whole village against this development.

Simon Weeks asked for confirmation that the site had outline planning permission for 6 large detached dwellings, and queried whether the bulk and mass of the adjacent sunrise building made this proposal acceptable. Andrew Chugg, case officer, confirmed that the site had outline consent for 6 detached dwellings. Andrew stated that the adjacent building was a very significant building, and therefore in part formed the context for the acceptance of the proposals.

Andrew Mickleburgh felt that the nature of the adjacent Sunrise building meant that this development would not have a significant impact on the character of the area. Andrew queried why some or all affordable housing could not be provided onsite, queried whether analysis of highways safety had been carried out in terms of access to the site, and queried whether the unsustainability of the site could warrant refusal. Andrew Chugg stated that the Wokingham Borough Council (WBC) affordable housing team requested a financial contribution rather than onsite provision of affordable housing, in part due to service charges and sustainability issues. Judy Kelly, Highways Development Manager, stated that the proposals would result in an additional 8 vehicle movements in the morning

peak and 6 movements in the evening peak. Offsetting this against the existing 3 houses would result in an additional 6 vehicle movements in the morning peak and 4 in the evening peak. Judy added that the infrastructure could be improved in the area, however officers had to consider the proportionality of this compared to the existing vehicle movements that the proposal would create. As such, it was not for this scheme to provide solutions to existing problems faced by residents. Judy added that it would be a requirement for the applicant to develop a travel plan. Judy stated that the NPPF only allowed for developments to be refused in highways terms if the cumulative impact of them was severe, which was not a sustainable point in this case.

Chris Bowring queried whether Members were being asked to approve the employment skills plan, queried whether the age restriction which allowed for more choice by older people was a positive for the application, queried whether the adjacent Sunrise building was taller in height than the proposed building, queried whether the Sonning golf club provided any context for this application, queried whether the parking was compliant with WBC parking standards, and queried what considerations had been given to cycle parking on the proposed development site. Andrew Chugg clarified that Members were not approving an employment skills plan as such, but instead the applicant would pay for officers to produce one on their behalf. Relating to the age restriction element, Andrew stated that he wanted to make clear in the report that there was no significant weight applied to the fact that the development would be for residents of 60 years and older, and there was no care need associated with the site. With this being said, the development would provide more choice for older people looking for accommodation. Andrew stated that the adjacent Sunrise property was higher than the proposed development, and the neighbouring residential dwellings were lower than the proposed height of block A. Referring to the Sonning Golf club development, Andrew clarified that this gave some context to this application as that development was allowed at appeal despite encroachment into the countryside. Judy Kelly confirmed that the parking was fully policy compliant, and as the parking was controlled by one entity then some of the disabled spaces could be reassigned should there not be a need for all of them. In addition, Judy confirmed that 12 cycle spaces were proposed and this number could be further increased should the travel plan indicate an increased need.

Abdul Loyes queried whether policy TBO6 was relevant to this application, as the application appeared to cause harm to the local area. Simon Weeks stated that any harm caused must be viewed in the context of the adjacent Sunrise building. Andrew Chugg stated that although TBO6 set a higher standard for redevelopment of residential land, this had to be viewed in the context of the adjacent Sunrise building. As such, it was deemed that the proposal would not be out of keeping with the character of the area, and the blocks of flats would be sufficiently spaced.

Carl Doran welcomed the provision of 40% affordable housing, though stated that he would prefer to have seen it provided on site. Carl was of the opinion that due to the adjacent Sunrise building, it was hard to argue that this property would be out of character with the area. Carl felt that due to the lack of amenities, parking provision should be higher as residents would require vehicles for everyday activities such as shopping and healthcare appointments. Carl queried whether this application would return to the Committee at the reserved matters stage if approved on the evening. Connor Corrigan, Service Manager – Planning and Delivery, stated that as a major application this item would return at the reserved matters stage.

Gary Cowan noted a Wokingham Town site, which had low parking provision, which had to expand the age range of residents and then did not meet parking standards. Gary had concerns that a similar issue could occur at this site, if approved. Gary queried whether affordable housing could be conditioned to be provided onsite. Simon Weeks stated that as WBC officers had asked for an affordable housing contribution, it would put WBC at a difficult position should this go to appeal.

Angus Ross stated his growing unease with this proposal. Angus stated that the proposal was not out of keeping with the area due to the context of the adjacent Sunrise building, met affordable housing requirements, and met WBC parking standards. Angus felt that the use of the term 'retirement' was misleading in this instance, as many residents would still be working within this age range. Angus queried whether there was sufficient turning space for delivery lorries and refuse lorries, queried whether the overlooking element had taken into account that this was a three storey building, and was of the opinion that the unsustainable nature of this site would warrant a reason for refusal. Angus asked that an informative be added to encourage the installation of sprinklers. Judy Kelly stated that the site layout had been tracked for a WBC refuse vehicle, which showed that the vehicle could travel to the southern part of the site and use the turning head at that point. In addition, the access road was 5.5m which would allow for two vehicles to pass each other. Regarding the overlooking comment, Andrew Chugg stated that the Borough Design Guide broke down the side to side and back to back distances. As such, the criteria for flats to dwellings back to back came to 30m. The closest relationship with number 4 Sonning Gate was 31.6m. In respect of Redgrave, the back to side arrangement was 19m, which was in excess of the 15m distance required by policy. Regarding sustainability, officers accepted that there were not many local shops however it was not unusual for new housing to be located this distance from shops, and it was located within settlement which made it hard to argue that it was located within an unsustainable location.

Malcolm Richards raised concerns regarding the sustainability of the site, especially considering that many residents would be of working age which would require them to travel to and from their place of work. Malcolm commented that the pedestrian routes within the locality were not safe, and would not be suitable to travel with a reasonable amount of shopping.

Angus Ross queried where the nearest shops were located. Judy Kelly stated that there were a number of amenities and facilities within the area, and the nearest shop was approximately 1.1km from the proposed development site. Connor Corrigan stated that officers understood concerns regarding sustainability, however it was very difficult to object to the application on these grounds as it was no different from many brand new housing developments. Chris Bowring commented that the sustainability would be the same for existing dwellings in the area.

Due to concerns raised relating to any potential change of conditions in the future to allow the facility to operate as, for example, sheltered accommodation, Simon Weeks proposed officers ensure that any change of conditions would return to the Planning Committee for approval. This proposal was agreed by Members and noted by officers.

Angus Ross proposed an additional informative, which encouraged the applicant to install sprinklers within the facility. This proposals was agreed by the Committee and added to the list of informatives.

RESOLVED That application number 201833 be approved, subject to conditions and informatives as set out in agenda pages 94 to 103, amended condition 16 as set out in the Members' Update, and additional informative encouraging the installation of sprinklers as resolved by the Committee.

**MINUTES OF AN EXTRAORDINARY MEETING OF THE
PLANNING COMMITTEE
HELD ON 18 MAY 2021 FROM 7.00 PM TO 10.55 PM**

Committee Members Present

Councillors: Simon Weeks (Chairman), Chris Bowring (Vice-Chairman), Stephen Conway, Carl Doran, Pauline Jorgensen, Abdul Loyes, Andrew Mickleburgh, Angus Ross and Rachelle Shepherd-DuBey

Councillors Present and Speaking

Councillors: Sarah Kerr and Maria Gee

Officers Present

Connor Corrigan, Service Manager - Planning and Delivery
Mark Cupit, Assistant Director Delivery & Infrastructure : Place and Growth
Judy Kelly, Highways Development Manager
Mary Severin, Borough Solicitor
Callum Wernham, Democratic & Electoral Services Specialist

Case Officers Present

Emy Circuit
Andrew Fletcher

72. APOLOGIES

An apology for absence was submitted from Gary Cowan.

73. DECLARATION OF INTEREST

Pauline Jorgensen declared a prejudicial interest in agenda items 76 and 77, on the grounds that although she had no involvement in these applications in her role as the Executive Member for Highways and Transport she had spoken to both supporters and objectors of these applications, and had been advised not to take part in the discussions or votes for these items.

Angus Ross stated that three of the supplementary planning documents had been signed by him in 2010 and 2011, whilst he was an Executive Member. Angus added that despite this he had no involvement with these applications since and would therefore take part in all of the discussions and votes.

74. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

No applications were recommended for deferral, or withdrawn.

75. DIVERSION OF FOOT PATHS IN SUPPORT OF APPLICATION NO. 192928

Proposal: Diversion of parts of Wokingham Footpath 24 / Wokingham Without Footpath 9 and Wokingham Without Footpath 5, and the stopping up of part of Wokingham Footpath 25, Wokingham Footpath 10 and Wokingham Without Footpath 5 in support of the planning application for the construction of the South Wokingham Distributor Road (SWDR) and the Eastern Gateway between Finchampstead Road and Waterloo Road.

Applicant: Mrs Jean Mulovi, Wokingham Borough Council

The Committee received and reviewed a report about this item, set out in agenda pages 5 to 20.

The Committee were advised that there were no Members' Updates.

Maria Gee, Ward Member, spoke in objection to this item. Maria was of the opinion that there were many good qualities about the proposed new footpath routes in the new proposed developments, including improved surfaces and improved access for disabled residents. Maria raised concerns that the footpaths were not suitable for shared use, and felt it was unclear what would be done to deter cyclists from using the existing footpaths without also deterring wheelchair users. Maria queried whether the Committee was convinced that the new designs would appropriately direct each type of path user. Prior to making changes to the existing network, Maria asked that the Committee look at the gaps between the boards of the boardwalk to see whether they had been considered in relation to the most commonly used wheels for pushchairs and wheelchairs. Maria sought confirmation that the boardwalk surface would not become slippery in winter. Maria was of the opinion that the design of the SWDR could potentially place obstacles in the way of increased pedestrian use due to a lack of controlled crossing points. Maria asked the Committee to consider whether the new designs were the best that they could be prior to removing or diverting the existing pathway network, and asked the Committee to confirm whether these plans had met Wokingham Borough Council's duties under the Public Sector Equality Duty.

Simon Weeks queried whether any of the signage related to the usage of the footpaths would be changing. Andrew Fletcher, case officer, confirmed that officers were not looking to prevent use of the paths by cyclists, and cyclists would be entitled to use the paths under permitted use.

Simon Weeks queried whether the boardwalk would meet current standards related to pushchairs and cyclists. Andrew Fletcher confirmed that the boardwalk would meet the current standards, and officers were happy with the designs.

Angus Ross stated that much work had been done to ensure the continuity of the public rights of way system in the area, whilst taking into account the needs of future housing developments and major roads. Angus added that many of the public footpaths were currently well used, and the modifications suggested would maintain safety and usability. Angus stated that footpath 5 was currently seldom used due to the relatively unsafe entry and exit, however the proposals would improve upon this and should make it a much more useful walking route.

Stephen Conway stated that Local Transport Note 1/20 (LTN 1/20) promoted clear separation between pedestrians and cyclists, and raised concerns that should these footpaths not be separated then some users may be deterred from using the routes and instead rely on cars for transport. Andrew Fletcher stated that Section 8.2.8 of the LTN specifically discouraged separation of routes which were 3m wide or less. In those instances, as was the case for these footpaths, a fully shared surface was best as otherwise each side would only have 1.5m which could cause additional issues.

Stephen Conway queried what measures would be put in place to reduce slippage on the boardwalk. Andrew Fletcher clarified that the boardwalk would be made of timber decking with grooved boards with anti-slip slats. Gaps between boards would be a maximum of 12mm wide, which exceeded standards.

Andrew Mickleburgh queried whether any of the proposed diversions could make the routes more prone to flooding, and whether any of the amended routes could cause issues for some users due to increased length. Andrew Fletcher stated the proposals would make the footpaths more usable for most of the year compared to their current routes, and the footpaths would only become unusable during very severe weather conditions. Currently footpaths 9 and 24 were flooded during severe floods, and the proposals included an alternative path off of the highway which should alleviate issues during severe floods. Relating to the distance, Andrew stated that any increase to distances would not cause an issue for any user.

Rachelle Shepherd-DuBey was of the opinion that a fully segregated route for cyclists and pedestrians would be more suitable for the proposed diverted footpaths. Andrew Fletcher stated that bridleways had allowed cycle use since 1969, and many bridleways were narrower than 3m and generally had very few issues.

Abdul Loyes queried whether there were any standards stating how wide a public right of way needed to be. Andrew Fletcher stated that he could not answer related to an adopted highways standard, however from a public right of way perspective there were no minimum width standards.

Chris Bowring commented that officers had given assurances that the proposed routes were acceptable as shared use footpaths, given the width of the pathways. Chris added that these routes were required for the South Wokingham Distribution Road, which was to be considered by the Committee later in the agenda.

Carl Doran queried why the boardwalk was chosen to be 4m wide. Andrew Fletcher commented that he had received the application and checked that the boardwalk was acceptable, which it was in this instance.

RESOLVED That the orders be made, subject to the recommendations as set out on agenda page 6.

75.1 Combined Minutes from Applications 203535; 192928; 190900; 190914 and 191068

Due to the linked nature of applications 203535, 192928, 190900, 190914, and 191068, the presentation, public speaking, and discussions for these items were taken as one. Each item was voted on separately.

Pauline Jorgensen declared a prejudicial interest in applications 203535 and 192828, and took no part in the discussions or votes on these items.

Peter Dennis, Wokingham Town Council, spoke in objection to application number 192928. Peter stated that this application would blur the boundary between Bracknell and Wokingham. Peter added that Wokingham Town Council had been consulted on this application since September 2019, and they wanted to see a fully segregated cycle lane. Peter stated that if Wokingham Borough Council (WBC) was seen to be taking the Climate Emergency seriously then they needed to invest in proper cycling infrastructure which would encourage children to cycle and walk to and from school. Peter added that officers had stated that there would not be space to segregate a cycle lane, however in his opinion there was room for this and in any case the cycle route needn't necessarily follow the main road. Peter queried why a Dutch style roundabout had not been considered, which would

have aided cyclists. Peter stated that cycling uptake was on the increase, and WBC should be looking to make it easier for the community to adopt sustainable travel methods. Peter asked that the Committee send the plans back for further review, to ensure that the designs fully promoted sustainable travel in an efficient and safe way.

Adrian Mather, Wokingham Town Council, spoke in objection to application number 203535. Adrian stated that the Town Council had met to discuss this application and had resolved to recommend a refusal, as they felt that it was unsafe. Adrian was of the opinion that the plans did not comply with CP1, CP2 or CP3 of the core strategy, and removal of crossing points went against both WBC and NHS guidelines. Adrian felt that removal of crossing points could lead to more accidents, especially when considering that there would be significantly more traffic passing through this area as a result of the applications being considered on the evening. Adrian stated that this application would fail to address the issues caused by the two Victorian era railway bridges, which did not comply with modern standards including for safe vehicle clearance heights. Adrian added that it was a terrifying experience to cycle under one of the bridges whilst a Heavy Goods Vehicle (HGV) passed through in the other direction. Adrian asked the Committee to consider whether the removal of pedestrian crossing points and the nature of the existing bridges would address what the town really needed in terms of safety and accessibility amongst other issues.

Andy Bowker, resident, spoke in objection to application number 203535. Andy commented that Members were elected to represent their residents and not WBC officers. Andy was of the opinion that a better option rather than the proposed scheme would have been to construct a large new roundabout and new bridge on the Eden Motors site. Andy added that this proposal had been considered and subsequently dismissed by officers, and a freedom of information request had shown that the replacement bridge had not been fully investigated and should still be considered as a better alternative to the proposed scheme. Andy stated that in April 2019 officers moved to a revised design which sought to maintain the pedestrian crossing on the south side of Finchampstead Road as well as adequate approach sightlines, which were both requirements of the safety audit. Andy added that this approach was the preferred option for over 18 months, however one month prior to submitting the plans officers had withdrew the need for both of these requirements. Andy was of the opinion that this would cause additional safety issues, and was done to avoid paying compensation to the landowners to purchase the land for this preferred option. Andy stated that the departure from standard did not relate to the pedestrian crossing, but instead directly related to the approach sightlines which will not be in place. Andy asked that the Committee refuse this application as it would, in his opinion, compromise on safety and not fulfil its purpose.

Adrian Betteridge, WATCH Wokingham, spoke in objection to application number 203535. Adrian stated that on average in the UK 11 cyclists were killed or seriously injured each week, and 1 in 4 of these involved a roundabout where the cyclists were stuck by a faster moving vehicle. Adrian added that National Planning Policy Framework required applications for development to give first priority to pedestrian and cycle movements, and was of the opinion that the proposals took a reasonably safe junction and replaced it with one designed to maximise the flow of vehicles at the expense of vulnerable road users. Adrian stated that the application claimed to meet standards, despite there being no provision for safe cycling on two of the three sides of the roundabout, and the removal of the existing pedestrian crossing which was to be replaced by either an uncontrolled crossing, a long diversion, or a possible alternative crossing further down the road. Adrian added that these changes had been made since the previous public consultation in the knowledge that children use this roundabout to walk and cycle to and from at least four

different schools. Adrian was of the opinion that the proposals failed to meet the Government's minimum guidelines, would not be considered reasonably adequate by any model measure, and in one case to go against the advice of an independent safety adviser. Adrian stated that the Government expected half of all local journeys to be walked or cycled by 2030, for the sake of the climate, air quality, and local congestion, and was of the opinion that this scheme would have the opposite effect. Adrian stated that in future, Active Travel England would not allow schemes such as this to be implemented. Adrian urged the Committee to make any approval conditional on the scheme being adapted for the benefit of pedestrians and cyclists.

Paul Evans, resident, spoke in objection to application number 203535. Paul stated that residents from 96 Finchampstead Road wished to object to the proposed scheme as they had not been consulted in the design. Paul stated that the current proposals would create an additional hazard when entering and exiting their driveways, due to the pavement for the central reservation and signage located outside of their driveway. Paul added that the driveways for 94, 94a, and 96 Finchampstead Road did not appear on any of the plans. Paul asked that a keep clear section of road be added to help alleviate issues for these properties when entering and exiting their driveways. Paul stated that no noise considerations had been given to houses on the eastern side of the road, which would be effected by additional traffic passing through the roundabout. Paul requested additional noise reduction measures to help mitigate these issues.

Alex Crann, resident, spoke in objection to application number 192928. Alex stated that the southern distributor road was a once in a lifetime opportunity to provide a road which prioritised safe and sustainable travel. Alex stated that the current designs included a shared use footway which was not suitable for both pedestrians and cyclists, which would lead to conflict and reduced journeys via sustainable travel methods. Alex was of the opinion that the current proposals would not promote sustainable travel, and once implemented would be very difficult to change and amend. Alex felt that the road had potential for excellent town centre access, however it needed to be done in the right way first time around.

Fitzroy Morrissey, resident, spoke in objection to application number 192928. Fitzroy raised concerns related to the flood risk associated with the southern distributor road development. Fitzroy added that he lived approximately 30 yards from Luckley brook, upstream of the proposed development, and in January and February of this year his property had suffered severe groundwater flooding which the Environment Agency believed was connected to the very high levels of the Luckley Brook at the time. Fitzroy commented that there was also serious flooding in the wood behind Luckley Brook, and considerable water build up underneath the bridge at the end of Luckley Brook and on the road towards Ludrove School Drive, where many residents came to walk. Fitzroy stated that these recent issues had been reported to both the Environment Agency and WBC's flooding team, but had not been taken into account in the assessment report for this development. Fitzroy requested clarification as to what measures would be put in place to ensure the development and proposed diversion and culverting of Luckley Brook would not create the risk of more regular and more serious flooding in the future, and asked the Committee to commit to monitor this risk both before and after the development process.

Jean Mulovi, on behalf of WBC, spoke in support of applications 203535 and 192928. Jean stated that both schemes had been developed with extensive public consultation and public exhibitions, and the team had worked closely with the team in charge of designing the South Wokingham Strategic Development Location (SW SDL) to achieve a

coordinated planning process. Relating to application 192928 Jean added that, wherever possible, utility provision including pedestrian and cycling infrastructure was made a priority. Jean stated that LTN 1/20 had been addressed in light of the new guidance and officers felt that the proposals conformed to the Gear Change requirements. Jean stated that there was no room for segregated pedestrian and cycle lanes, in part due to the existing highway boundary. Jean stated that the new road would provide 2.7km of new highway with shared pedestrian and cycle access. Designs included a footbridge over the Emmbrook, and as part of the overall scheme located in a flood plain the designs would better alleviate flooding concerns towards the existing Tesco's roundabout. Jean stated that the scheme was subject to an environmental impact assessment which had shown improved air quality, biodiversity, and increased levels of active travel as a result of proposals in the future. Jean stated that the scheme had been revised in order to meet the needs of users as much as possible. Relating to application number 203535 Jean stated that the proposals intended to replace the existing mini-roundabout which was a key constraint of getting into and out of Wokingham. Jean stated that there would be further changes made to the existing uncontrolled crossing point, which would be replaced by a controlled crossing point to be secured under condition 3 relating to application number 203535. Jean stated that a number of trees would be required to be felled as a part of this scheme, however due to insufficient space on site to replace all of the trees, mitigation replanting would take place as part of the SW Distribution Road, as agreed with the WBC Landscape and Ecology officers, and secured by condition.

Roger Tustain, on behalf of the applicant, spoke in support of applications 190900, 191914 and 191068. Roger stated that these applications, in conjunction with the Western Gateway and SWDR comprised the majority elements of WBCs core strategy allocation in accordance with policy CP21, which allocated approximately 2,500 dwellings to the land to the South of Wokingham. Roger added that the first phase of this accommodated 636 dwellings to the north of the railway line, now known as Montague Park, which was now substantially complete. Roger summarised that the proposals in front of the Committee comprised up to 1,649 new dwellings, which thirty five percent of which would be affordable, a new local centre providing new local shops and land for a community facility, a new two-form entry primary school, two SANGs, a network on on-site open space including allotments and play areas, on site drainage facilities, on and off-site cycling pedestrian strategy and a comprehensive public transport strategy. Roger stated that the residential aspects of the proposals were at outline stage, which demonstrated how the development could come forward consistent with design policy. If approved, a subsequent reserved matters application and discharge of conditions would need to be undertaken, ensuring a coherent approach to every phase of development. Roger stated that all properties would conform to national space standards, and WBC adopted parking standards. Roger added that the site had a number of constraints, however the WBC SWDR team and the consortium's team have worked with the WBC planning team and statutory consultees to evolve a fully integrated and robust set of proposals.

Craig Rawlinson, on behalf of the applicant, spoke in support of application numbers 190900, 191914 and 191068. Craig stated that there had been significant collaboration with the team delivering the SWDR to ensure consistency. Craig considered the site to be a good site in transportation terms for a residential development. Craig stated that the site would provide community facilities, including a local centre and primary schools. Provision of these facilities would minimise the need for travel off-site. Craig added that the site was within walking distance to the town centre, and a range of walking and cycling improvements and corridors would be secured via condition. Craig stated that a new bus service would run through the site, between Bracknell and Wokingham train stations at a

30 minute frequency. In addition, the scheme would provide improved bus stops both on and off-site, whilst the new SWDR would mitigate the traffic impacts associated with the development. Testing works had been carried out in conjunction with WBC highways officers, which had confirmed that some junctions in the town would need to be improved, and improvement schemes had been identified. Electric charging facilities and a car club would be located on-site, and funding would be available to link the site in with the WBC My Journey programme.

Chris Patmore, on behalf of the applicant, spoke in support of application numbers 190900, 191914 and 191068. Chris stated that close collaboration had been carried out between WBC drainage, highways, landscape and ecology master planning and engineering schemes to ensure that all of the schemes complemented each other in terms of flooding and drainage. Chris stated that there were a number of water courses across the wider site. The tributary of the Emmbrook would be improved and diverted alongside the proposed SWDR, to work to reduce flooding and produce a more useful and linked green corridor. The Emmbrook itself would not be impacted by the proposed SW SDL developments, and the Thames Water sewers would still be required to discharge into the Emmbrook, however a replacement attenuation scheme would be located in the SANG area would be provided as part of a wider landscaping and ecological scheme. Chris stated that extensive flood modelling had been undertaken and agreed between the Environment Agency and WBC, which adopted best practice and would reduce the overall discharge to the Emmbrook downstream of the combined development.

Maria Gee, Ward Member, spoke in objection to application numbers 192928 and 190900. In relation to application number 192928 Maria stated that WBC had a duty to ensure safe movement of pedestrians included those who were less abled. Maria commented that an average walking speed was between 3mph to 4mph, whilst an experienced cyclist could achieve speeds of 20mph. Maria added that if a group of pedestrians used the whole width of the path, this would create additional conflict and potential for accidents to occur. Maria stated that pedestrians and cyclists were being given much less space than motorists, which was not the right message to send to residents when WBC had declared a climate emergency which supported increased usage of sustainable methods of travel. Relating to application number 190900 Maria stated that SANGs should be attractive and accessible, however the proposed SANG was not located near the proposed development and was bordered by both a main road and the railway line. Maria drew comparisons with Dinton Pastures, where the areas adjoining the main road were the least visited. Maria felt that users of the proposed SANG would avoid the areas near the main road and railway line, and would not benefit from adjoining open space as users did at Dinton Pastures. Maria stated that residents of Montague Park could only access the SANG via Buckhurst Meadows, whilst the parking for the proposed new SANG was also located off of Buckhurst Meadows and was already often full at weekends and sometimes during the day. Maria felt that this land should not be converted into a SANG as it was in the wrong place both for enjoyment and use, and she urged the Committee to refuse the proposal.

Sarah Kerr, Ward Member, spoke in objection to application number 203535. Sarah stated that the proposals went against WBC core strategy, as improvements to the width of footway provision underneath the railway bridge was substandard and only located on the eastern side of the road were not included as part of this application. Sarah added that, according to Network Rail, WBC highways team had only two meetings with them, one of which being an initial "kick-off" meeting in March 2019. Sarah commented that all Committee Members were elected to represent the residents of the Borough, and all Committee Members had voted to declare a climate emergency at a previous Council

meeting. Sarah was of the opinion that the proposals would achieve a bigger roundabout leading to high speeds of vehicles, with no additional pedestrian or cycling infrastructure, thus making this junction more dangerous. Sarah did not agree that the proposals would achieve improvements towards increased active travel. Sarah was of the opinion that if the junction was genuinely about alleviating traffic issues, then the junction must be designed to make active travel measures safer and more appealing to reduce the numbers of vehicles on the road network in the first place. Sarah stated that the addition of the new pedestrian crossing on the northern arm would satisfy the current desire lines, however the southern controlled crossing was not included within the current list of conditions. Sarah commented that the omission of the uncontrolled crossing from the southern arm of the roundabout would still leave a splitter island, leaving people to try to cross here with severe sight line issues which Sarah felt had not been addressed. Sarah stated that a shared driveway between number 96 and 100 Finchampstead Road did not appear on the plans, and she did not know how they were going to be able to turn northbound out of their driveway with a splitter island in place. Sarah commented that the site was only located 600m south of an air quality management area, and was of the opinion that the proposals would shift a known problem and result in increased traffic, reducing air quality in an area heavily used by school children. Sarah urged the Committee to refuse this application, and for it to be replaced by designs which addressed the issues related with the bridge and offered people a choice of how they could travel.

Simon Weeks commented that the combined applications form part of the 2010 Core Strategy, which comprised a number of SDLs. Due to a number of reasons, the applications being considered on the evening had been significantly delayed compared to the other SDL sites, however they still formed a crucial part of the 2010 Core Strategy.

Simon Weeks queried why the improvements to the railway bridges were included in the Core Strategy but not within the applications in front of the Committee, queried issues relating to sight lines and speed of vehicles using the proposed western gateway roundabout and sought reassurances relating to vehicle flow speed so as not to dissuade pedestrians and cyclists, and queried whether we should be applying future standards to current applications. Judy Kelly, Highways Development Manager, stated that the proposals in front of the Committee would not prejudice any future works being carried out to the railway bridges. Judy added that the bridges saw between 10 and 20 HGVs pass under them during the peak hour. Judy stated that the western gateway was a traffic mitigation scheme to help alleviate the traffic caused by the SWDR and the proposed 1800 houses. The current mini roundabout would be over capacity regardless of whether the housing proposals went ahead. The junction had been modelled with the approved Tesco junction to the north and would bring with it considerable benefits in terms of the free flow of traffic in the future. Although primarily a highways capacity scheme, the design team had taken the opportunity to create and extend cycle routes in the area and provide additional toucan crossings as far as they could, given constraints in tying into the existing network. Judy felt that the scheme did a good job at addressing capacity issues at the junction whilst providing improvements to cycling and pedestrian access. Relating to applying future standards to current applications, an Executive decision had been made to apply LTN 1/20 to new schemes coming forward, and to embrace the principles of LTN 1/20 to schemes already in the design process such as the scheme's being considered this evening. Judy added that the schemes complied with most of the guidance issued in LTN 1/20 including the adopted guidance and policy of WBC.

Angus Ross stated that condition 6 of the SWDR application left ongoing concerns relating to the railway crossing at Star Lane, especially for pedestrians and cyclists. Angus

stressed the essential need to address this issue, as it was seen as a serious safety concern. Angus supported the general principle of the SWDR as it would hopefully reduce traffic on the Nine Mile Ride. A number of local residents had raised concerns relating to the stopping up of the Easthampstead Road, and Angus sought assurances that this was essential. Relating to the proposed SANG, Angus felt that although its proximity to the railway line was not ideal, it was closer and more accessible than the Thames Basin Heath and was approved by Natural England. Relating to the main proposed housing development, Angus queried whether the primary school would be subject to a separate planning application in time, should the application be approved on the evening. Angus queried whether there would be suitable electricity provision should all residents in the proposed dwellings charge electric vehicles at the same time. Angus raised concerns that monies were suggested to be allocated to sites in Barkham and Finchampstead rather than to sites directly affected by the proposals, and Angus queried whether it had been proven there were no more local schemes such as on Nine Mile Ride that could be addressed with these monies. Relating to the proposed smaller housing development, Angus sought assurances that the biodiversity net gain be achieved on site rather than via off site contributions. Finally, Angus sought assurances that any road with a bus route through it was wide enough to safely accommodate a bus.

Officers responded to a number of the above points. Relating to the Star Lane crossing point, Judy Kelly stated that there was an indicative scheme from the developer which showed a new minimum 3m wide shared pedestrian cycle routes on both sides of the Easthampstead Road, and detailed design would need to come forward in future. North of the railway, a S106 contribution would be required towards the Council to make any improvements, taking into account the constraints of the site. Relating to the Heathlands Road/Nine Mile Ride junction improvement, Judy confirmed that MOVA software with new ducting and space for maintenance would be provided. Over 20 junctions were looked at in the area, and the 8 chosen were the ones that the proposed developments would have an impact on. Relating to electric vehicle charging, load balancing technology was available to help balance peak loads across the network. Relating to the bus route query, Judy stated that the roads in question were a minimum of 7.3m wide, whereas the minimum standard was 6.5m. The stopping up of Easthampstead Road was due to the functioning of the SWDR junction with Easthampstead Road, in close proximity with the railway level crossing junction. As such the additional trains and traffic had been modelled in detail and to keep this section of road open would have required large master planning and amendments to the overall scheme, and instead an alternative arrangement of the Heathlands link road from the SWDR had been deemed to be a suitable alternative option. Emy Circuit, case officer, confirmed that the primary school would be granted outline planning consent should the SDL applications be approved on the evening, however approval of reserved matters would need to return in the future.

Chris Bowring queried whether 30mph would be the natural speed of SWDR, queried whether any indication could be placed on the proposed road to warn drivers that cyclists may also choose to use the road, queried whether the safety of the level crossing was a planning consideration, and queried whether road safety audits had been completed for both the roundabouts and the railway bridges. Judy Kelly stated that the road had been designed to a 30mph speed limit, and the road safety audit would address whether there were any issues of risk that vehicles may do in excess of that speed limit. Judy added that the nature of the design, including the central crossing islands and curving geometry would be features that would keep the speed limit to 30mph as was seen in north Wokingham SDL sites. Regarding the road safety audit, these were required for any change to a highway which would include both highway schemes. There was currently a preliminary

stage 1 audit, with three further stages between now and after the road had opened should permission be granted. Judy stated that there was not the available width to put a lane marking for cyclists on the highway, in part due to the areas with the central island crossing points being slightly narrower.

Carl Doran commented that although the SWDR and wider SW SDL had been within the planning process for a long time, it was unusual to see very late changes such as the addition of a signalised crossing for application number 203535. Carl was of the opinion that overall highway capacity improvements could not be achieved without changes to the Victorian bridges. Carl was of the opinion that having a shared pedestrian and cycle space was not what was best option for this scheme, and queried why the decision to only allow 3m for pedestrians and cyclists was made at the design stage. Relating to the proposed SANG, Carl stated that he had no objections to the change of use, but was not sure whether many local residents would be able to walk to the SANG from the residential properties. Relating to the proposed housing, Carl was pleased to see 35 percent provision of affordable housing, and was pleased to see there would be the recommended split of 70 percent affordable rent and 30 percent shared ownership properties. Carl was disappointed to see that the CCG had not commented on the application, despite the possibility of a new GP surgery being able to be accommodated on site, however Carl accepted that this was not a material planning consideration. Judy Kelly confirmed that application number 203535 would increase vehicle flow at the junction, which would bring the capacity below the 85 percent threshold of concern. Judy added that the issue relating to the Victorian bridge was a height issue rather than a width issue. Relating to the proposed shared pedestrian and cycle way, Judy stated that a 3m width worked well at the Montague Park development, including facilitating children walking and cycling to and from the Floreat Montague Park School. Judy added that having a small 50cm green buffer on the edge of the shared footway and cycleway allowed a long term option should small sections of the route cause concern in the future.

Stephen Conway stated that he had four concerns related to the proposed housing development. There were several prominent listed buildings that would not be in the same setting for future generations and Stephen was not confident that screening proposals would maintain their current settings, the border between Wokingham and Bracknell would now be less than 1km apart, the separation distances between existing buildings and proposed developments were policy compliant however the existing residents would still be subject to large and fundamental changes, and Stephen raised concerns that the 4 storey element of the residential design would be out of keeping with other Wokingham developments. Relating to the proposed SANG, Stephen was of the opinion that although the proposed location was not ideal there was no other alternative location. Relating the proposed new roundabout, Stephen was of the opinion that the desire would be to continue to cross the road at the current uncontrolled crossing point despite the addition of the new southern toucan crossing, which would cause additional safety risks for pedestrians and cyclists. Stephen added that LTN 1/20 was there to encourage pedestrians and cyclists to feel safe and confident, and this could be achieved in the SWDR via the creation of a wider segregated space which would remove the conflict potential between these users.

Officers responded to a number of the above points. Emy Circuit stated that a heritage assessment had taken place which showed that listed buildings would see some impact to their setting, however Historic England and WBC conservation officers had agreed that the proposals, including screening measures, were acceptable. Relating to issues of separation distances, officers had agreed to revisions regarding this which had been

deemed acceptable. In addition, reference was made to an assessment within the report and said the separation distances were comparable or greater than in other locations along the boundary and to the north. Relating to the 4 storey elements of the proposals, Emy stated that the majority of dwellings would be two or two and a half storeys rising to three in some areas, with 4 storey element being limited to the central neighbourhood area as a focal point. Emy stated that Natural England's advice for SANGs was that it was expected for most people to walk to a SANG which was located within 400m of their property, and much of the SDL would be located within this distance. Connor Corrigan, Service Manager – Planning and Delivery, stated that LTN 1/20 was not a major concern as it was guidance which had been predominantly followed for the Highways applications. The initial road safety audit had passed on all sections of the proposed road and roundabout modifications, which suggested no safety implications. Connor added that without the key highways elements, the residential units would not be able to go forward, leading to speculative and inappropriate planning applications from developers due to WBC not meeting its housing numbers. These applications would most likely be allowed at appeal, as an Inspector would conclude that WBC had not met its quota of approved houses. Connor stated that the Committee needed to balance the need for an extra two metres each side of the proposed SWDR against the cost of potential appeals and inappropriate developments being allowed across the Borough.

Pauline Jorgensen queried why a secondary school was not being provided on the proposed development site, and whether there would be trees on the small strip on land between the railway line and the proposed development to help lessen the impact of the noise from the railway. Emy Circuit stated that there had been no indication from the WBC education team that a new secondary school would be required on this site, and it was up to that team to come up with a strategy to address the peak in new secondary school aged children in future. Secondary education was covered by CIL contributions, and there was no policy requirement for a secondary school within the SDL site. Relating to the strip of land, Emy stated that additional landscaping details would be required, however she would expect to see some planting in that area to help mitigate noise from the railway.

Rachelle Shepherd-DuBey was of the opinion that it should be relatively simple to widen the pathway adjacent to the proposed SWDR to 4m, and added that she did not believe that the scheme would be altered or amended once built if there were issues. Rachelle asked that all proposed dwellings would be fitted with sustainable energy such as heat pumps or electricity rather than gas. Rachelle commented that the Montague Park community centre took a considerable amount of time to get fitted out, and worried that the proposed community building space may not be completed in good time. Rachelle asked that roof voids be avoided on the apartment buildings to reduce the risks of fire. Simon Weeks commented that building regulations would govern the issues relating to energy provision and roof voids rather than planning policy. Relating to the potential widening of the footpath, Judy Kelly stated that the SWDR application had been assessed in relation to the cycling level of service assessment, which was appended to LTN 1/20, and therefore the scheme would be eligible for funding under LTN 1/20 if it was put forward as such. Judy added that the 0.5m buffer on either side was required to ensure that people did not fall onto the carriageway or scrape handlebars against a wall. Relating to an earlier point raised about number 96 Finchampstead Road, this wasn't raised as an issue at the first safety audit stage, but it would be picked up at the second stage and the island could be modified slightly if required. Connor Corrigan stated that reducing the verge buffer would reduce the overall quality of the environment for cyclists and pedestrians. Connor added that LTN 1/20 had a large focus on very high usage areas such as urban cities and railway hubs.

Andrew Mickleburgh queried whether any modelling had been done to forecast pedestrian and cyclist traffic on the pavement of the proposed SWDR, queried how wide were the verges on the majority of the proposed SWDR and how wide they were required to be for reasons of safety and tree planting, queried what extent Network Rail influenced what was and was not possible regarding modifications to the rail bridges, queried what target of biodiversity net gain was applied to the proposed SWDR, and queried exactly how numbers 92 and 94 Finchampstead would enter and exit onto the road safely. Judy Kelly stated that modelling for pedestrian and cyclist use was very difficult, however school travel data was available from Floreat Montague Park School which indicated around 230 pupils using the paths in some way to get to school. Judy stated that a non-segregated route of 3m was deemed acceptable, and was also deemed acceptable in LTN 1/20 for routes of up to 300 pedestrians and up to 300 cyclists in the peak hour. Relating to the verges, these would have trees and street lighting which needed to be set away from the carriageways and away from the buffer zone for cyclists. Judy stated that there would be additional cycle routes through the future residential developments and through the proposed SANG, which would allow pedestrians and cyclists to disperse naturally. Network Rail would need to be involved in the design and approval of modifications to the railway bridge, as well as in discussions regarding the practicalities of construction and line closures. Relating to the property access query, Judy stated that should the splitter island cause any access issues, this could be addressed by shifting the island slightly. Emy Circuit stated that the biodiversity net gain would be 10 percent for both the residential parcels and the SWDR, if approved. There was also a mechanism to achieve these gains off-site if it was not possible to achieve the full 10 percent gain along the SWDR route.

Chris Bowring proposed an extension to the meeting of 30 minutes. This proposal was seconded by Simon Weeks and unanimously agreed by the Committee.

Abdul Loyes queried whether a condition could be added requiring provision of a GP surgery on site, and sought clarification as to the costs that WBC could incur should these applications be deferred or refused. Simon Weeks clarified that provision of GP surgeries was a matter for the CCG to decide, and their current model did not promote smaller GP surgeries fulfilling relatively local needs. Connor Corrigan stated that it would cost millions of pounds to redesign the highways and residential schemes, which could take several years. Connor added that the overall package provided a community facility, new schools and new highways. Connor stated that if the housing specifically was delayed, the planning inspectorate would likely be minded to approve speculative and inappropriate developments in order to fulfil the Borough's quota of housing delivery.

Stephen Conway queried how the whole scheme could be jeopardised by allowing for an additional 0.5m either side of the pathways. Connor Corrigan stated that 2m would be required either side, which would result in having to shift drainage, street furniture, and rearrange where residential units could be situated amongst other issues.

Chris Bowring commented that the only way to change application would be to refuse it, and in his opinion no evidence had been provided to show that there would be safety issues for pedestrians or cyclists for either of the highways scheme.

Relating to application number 192928, Stephen Conway proposed that this item be deferred to allow officers to explore the ways to enable segregation of pedestrians and cyclists. This was seconded by Rachelle Shepherd-DuBey and upon being put to the vote the motion fell.

Relating to application number 203535, the Committee resolved to amend condition 3 (relating to improved sight lines at the uncontrolled pedestrian crossing that currently exists) via delegation to the Chairman, and to add additional condition 13 (relating to provision of a toucan crossing on Finchampstead Road, towards the south side of the proposed roundabout) via delegation to the Chairman.

The resolutions for each individual item can be found in their specific minute.

76. APPLICATION NO. 203535 - LAND AT THE JUNCTION OF FINCHAMPSTEAD ROAD AND MOLLY MILLARS LANE

The substantive minutes for this item are contained in minute item 75.1.

Proposal: Full planning application for works at the junction of the A321 Finchampstead Road and Molly Millars Lane comprising replacement of the existing three-arm mini-roundabout with a larger roundabout; demolition of Nos 81 and 83/85 Finchampstead Road; and associated landscaping and changes to the pedestrian crossing environment.

Applicant: Wokingham Borough Council

The Committee received and reviewed a report about this application, set out in agenda pages 21 to 54.

The Committee were advised that the Members' Update included:

- Amendment to paragraph 11, to state that the building due to be demolished were number 81, 83, 84 and 85 Finchampstead Road;
- Amendment to paragraph 22, to state that transport modelling had assessed junction performance in 2036 as well as 2026;
- Clarification that reference to "Appendix B, Highway (General Arrangement Plan)" was located on page 52;
- Additional clause to condition 3, relating to removal of uncontrolled crossing and additional signalised crossing at Finchampstead Road South;
- Additional condition 12 (hours of work).

RESOLVED That application number 203535 be approved, subject to conditions and informatives as set out in agenda pages 22 to 27, additional condition 12 as set out in the Members' Update, amended condition 3 (relating to improved sight lines at the uncontrolled pedestrian crossing that currently exists) via delegation to the Chairman, and additional condition 13 (relating to provision of a toucan crossing on Finchampstead Road, towards the south side of the proposed roundabout) via delegation to the Chairman.

77. APPLICATION NO. 192928 - LAND SOUTH OF WOKINGHAM, EAST OF FINCHAMPSTEAD ROAD AND WEST OF WATERLOO ROAD, WOKINGHAM.

The substantive minutes for this item are contained in minute item 75.1.

Proposal: A full planning application for the construction of the South Wokingham Distributor Road (SWDR) between Finchampstead Road and Waterloo Road, including a link to Heathlands Road, together with associated works including demolition of Nos 76A and 76B Finchampstead Road. The SWDR will comprise a single carriageway, approximately 2.7km length and will also include a shared 3m wide cycle lane/foot way.

Applicant: Mrs Jean Mulovi, Wokingham Borough Council

The Committee received and reviewed a report about this application, set out in agenda pages 55 to 138.

The Committee were advised that the Members' Update included:

- Clarification that the expiry date was 31 May 2021;
- Clarification that comments received from Wokingham Without Parish Council on page 131 were received on 3 March 2019 and revised on 17 August 2020;
- Correction to paragraph 92 to replace the word westbound with the word eastbound;
- Reference to additional drawings received;
- Additional consultation response received from Wokingham Town Council.

RESOLVED That application number 192928 be approved, subject to conditions and informatives as set out in agenda pages 57 to 77.

78. APPLICATION NO. 190900 - LAND TO THE SOUTH OF ANNE'S MANOR
The substantive minutes for this item are contained in minute item 75.1.

Proposal: Full planning application for the proposed change of use of land from nil use to D2 for proposed Suitable Alternative Greenspace (SANG) with associated landscaping. To be read in conjunction with applications 190914 & 191068.

Applicant: Kingacre Estates Ltd

The Committee received and reviewed a report about this application, set out in agenda pages 139 to 178.

The Committee were advised that the Members' Update included:

- Clarification that the expiry date was 28 May 2021;
- Clarification that the comments contained on pages 172 to 176 were Wokingham Without Parish Council's initial comments submitted on 5 September 2019, and their revised comments contained on pages 177 and 178 were received on 28 September 2020.

RESOLVED That application number 190900 be approved, subject to conditions and informatives as set out in agenda pages 140 to 149.

79. APPLICATION NO. 190914 - LAND AT PHASE 2A OF THE SOUTH WOKINGHAM STRATEGY DEVELOPMENT LOCATION (SDL)
The substantive minutes for this item are contained in minute item 75.1.

Proposal: Outline application with all matters reserved except for principal means of access to the highways, for up to 215 dwellings, public open space, play areas, associated infrastructure and landscaping. To be read in conjunction with applications 190900 & 191068.

Applicant: Kingacre Estates Ltd

The Committee received and reviewed a report about this application, set out in agenda pages 179 to 276.

The Committee were advised that the Members' Update included:

- Clarification that the expiry date was 28 May 2021;
- Clarification that the comments contained on agenda pages 267 to 271 were Wokingham Without Parish Council's initial comments submitted on 5 September 2019, and their revised comments contained on pages 272 to 273 were received on 28 September 2020;
- Clarification of condition 53.

RESOLVED That application number 190914 be approved, subject to conditions and informatives as set out in agenda pages 181 to 209, and amended condition 53 as set out in the Members' Update.

80. APPLICATION NO. 191068 - LAND AT PHASE 2B OF THE SOUTH WOKINGHAM STRATEGY DEVELOPMENT LOCATION (SDL)

The substantive minutes for this item are contained in minute item 75.1.

Proposal: Hybrid planning application (part outline/part detailed) comprising an outline application with all matters reserved except principal means of access to the highways, for a mixed use development of up to 1,434 dwellings, a two-form entry primary school, local centre (A1, A2, A3, A4, A5 and D1 including community building D1/D2), public open space, play areas and associated infrastructure and landscaping; and a full application for the proposed Suitable Alternative Natural Greenspace (SANG), associated landscaping and temporary car park. – To be read in conjunction with applications 190900 & 190914.

Applicant: Keir Ventures Ltd and Miller Homes Ltd

The Committee received and reviewed a report about this application, set out in agenda pages 277 to 416.

The Committee were advised that the Members update included:

- Clarification that the expiry date was 28 May 2021;
- Clarification that the comments contained on agenda pages 407 to 411 were Wokingham Without Parish Council's initial comments submitted on 5 September 2019, and their revised comments contained on pages 412 to 413 were received on 28 September 2020;
- Clarification of condition 66.

RESOLVED That application number 191068 be approved, subject to conditions and informatives as set out in agenda pages 280 to 315, and amended condition 66 as set out in the Members' Update.

This page is intentionally left blank

Agenda Item 5.

Application Number	Expiry Date	Parish	Ward
203539	14/05/2021	Barkham/Wokingham Town	Barkham; and Evendons

Applicant	Mr and Mrs Pike
Site Address	Land off Bearwood Road, Bearwood Road, Wokingham, RG41 4SP
Proposal	Full planning application for the change of use of 14.77ha from existing private woodland to informal recreational land and associated infrastructure including pedestrian and vehicle access, car parking and footpath network and landscaping.
Type	Full
Officer	Stefan Fludger
Reason for determination by committee	Listed by Councillor Adrian Mather in the event the application is recommended for refusal. Major application.

FOR CONSIDERATION BY	Planning Committee on Wednesday, 09 June 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The proposal is for the change of use of 14.77ha of existing private woodland to informal recreational land. This would include the provision of a new car park with 18 spaces, a new access off Bearwood Road, surfaced footpaths, gates and street furniture, habitat restoration, removal of invasive species and landscaping.</p> <p>The scheme is acceptable in principle, promoting recreation in the Countryside. It would not result in harm to the character of the area and the level of works required to facilitate the change of use of the land are minimal considering the size of the site. It is not considered that the scheme would result in harm to Ancient Woodland and there would be significant removal of invasive species, secured by a long term plan. Wokingham Footpath 1 would be re-surfaced and this is a benefit of the scheme.</p> <p>The application documents refer to use of the land as Suitable Alternative Natural Greenspace (SANG). This is not a land use in Planning terms and this is not an application for SANG. The land will not be associated with any new housing developments and therefore will not be mitigating the impact of any development on the Thames Basin Heaths SPA.</p>

PLANNING STATUS
Countryside Protected Species Consultation Zones Ancient Woodlands Overhead cables 15m buffer Contaminated Land Consultation Zone Heathrow Aerodrome Consultation Zone Bat Roost Habitat Suitability Minerals Site Consultation Area Public Open Space Replacement Mineral Local Plan Tree Preservation Orders

Local Plan Update Submitted Sites
PRoW
SSSI Impact Risk Zones
Thames Basin Heaths SPA Mitigation Zones – 7KM
PRoW Within WB Consultation Zone
Green Routes and Riverside Paths Consultation Zone
Ordinary Watercourses Consultation Zone

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A. Completion of a legal agreement to secure:

- 1. The implementation of the works which are on land outside the applicant's control.**
- 2. Long term landscape and ecological management plan relating to the site, the forest and invasive species.**

If the Agreement is not completed within 3 months of the date of this resolution, Planning Permission will be refused unless the Operational Manager for Development Management in consultation with the Chairman of the Planning Committee agree to a later date.

B. Conditions and informatives:

Conditions:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Not including the exact location of the proposed footpath, which is not approved, this permission is in respect of the submitted application plans and drawings numbered 6768.PSCP.03 REV E, 6768.ES.01 REV C, 6768/ASP.HWD REV B, 6768/SK001/WS and 6768.ASP2, received by the Local Planning Authority on 17/12/2020 and revised plan numbered 6768.PS.02 REV F, received by the Local Planning Authority on 20/04/2021. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. No development shall take place, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

*Reason: In the interests of highway safety & convenience and neighbour amenities.
Relevant policy: Core Strategy policies CP3 & CP6.*

4. Prior to commencement of development, the following details shall be submitted to and approved in writing by the local planning authority
 - a) The materials to be used in the surfacing of the car park and a plan at a scale of 1:50 showing the design of the car park.
 - b) All public furniture including litter bins, information stands.
 - c) Materials to be used in pedestrian footpaths.
 - d) Rumble strip detail.
 - e) Wayfinding signage details.
 - f) Pedestrian bench details.
 - g) Details of the pond, including a cross section.
 - h) Detail and location of debris dam.
 - i) Full details of the proposed pedestrian bridges, including materials to be used in construction.

Reason: In the interests of the character of the area and the proper functioning of the area of open space.

5. Following the removal of Rhododendron within the '10 metre offset' as shown on plan numbered 6768.PS.02 REV F, but prior to the implementation of the proposed footpath, full details of the location of the proposed footpath will be submitted to and approved in writing by the LPA. The alignment/location of the footpath shall be designed as to have minimal harmful impact on trees and protected species as informed by the tree and biodiversity surveys as required by conditions 6 and 8. The footpath shall be implemented in the location approved.

Reason: The current scheme allows for flexibility in terms of the precise location of the footpath. Detail is needed following the clearance of Rhododendron as to the precise location of the footpath in the interests of the character of the area.

6. (a) Following the removal of Rhododendron within the '10 metre offset' as shown on plan numbered 6768.PS.02 REV F but prior to the implementation of the proposed footpath, an Arboricultural Method Statement and Scheme of Works which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 will be submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).

b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.

c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.

d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

7. (a) Following the removal of Rhododendron within the '10 metre offset' as shown on plan numbered 6768.PS.02 REV F, but prior to the implementation of the proposed footpath, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished contours, means of enclosure, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, play equipment, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

(b) Notwithstanding part (a), all soft landscape works for the car park area shall be carried out in accordance with the Proposed SANG Car Park Detail Plan (6768.PSCP.03 rev E).

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)

8. Following the removal of Rhododendron within the '10 metre offset' as shown on plan numbered 6768.PS.02 REV F, but prior to the implementation of the proposed footpath, updated surveys for protected species shall be carried out and the results used to provide recommendations, as per the mitigation hierarchy, to the location of the route of the path.

Reason: To secure protection for protected species which may be present.

9. Works are to be carried out in full accordance with the mitigation strategy given in paragraphs 4.5 to 4.16 inclusive of the submitted Outline Landscape and Ecology Management Plan (EPR, ref: 1908-1H, December 2020) unless otherwise agreed in writing by the council (not including the employment of a ranger – which does not need to be carried out).

Reason: To ensure that bats, a protected and priority species (as per the NPPF), and other protected species, are not adversely affected by the proposals.

10. The open space shall not be used until the vehicle parking spaces have been provided in accordance with the approved plans. The vehicle parking spaces shall be permanently maintained and remain available for the parking of vehicles at all times.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

11. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 08:00 and 18:00 Monday to Friday and 08:00 to 13:00 Saturdays and at no time on Sundays or Bank or National Holidays.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

12. Prior to the first use of the development hereby permitted details of secure bicycle storage for visitors to the open space shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before occupation of the development hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

13. Prior to the first use of the open space, the vehicular access shall have been formed and provided with visibility splays shown on the approved drawing number

6768.PSCP.03 REV E. The land within the visibility splays shall be cleared of any obstruction exceeding 0.6 metres in height and maintained clear of any obstruction exceeding 0.6 metres in height at all times.

Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.

14. No other development of the site as hereby approved shall take place until the access has been constructed in accordance with the approved plans.

Reason: To ensure adequate access into the site for vehicles, plant and deliveries associated with the development in the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.

15. Prior to the first use of the open space, the vehicular access shall be surfaced with a permeable and bonded material across the entire width of the access for a distance of 30 metres measured from the carriageway edge and shall include the disabled parking spaces

Reason: To avoid spillage of loose material onto the highway, in the interests of road safety. Relevant policy: Core Strategy policy CP6.

16. Prior to first use of the open space details of height barrier and location need to be submitted in writing and approved by LPA and delivered to the approved details.

Reason: To ensure that vehicles do not obstruct the highway whilst waiting for gates or barriers to be opened or closed, in the interests of road safety. Relevant policy: Core Strategy policies CP3 & CP6.

PLANNING HISTORY		
Application Number	Proposal	Decision
F/2007/0292	Application for the proposed change of use of land for the storage of vehicles awaiting recycling.	Allowed at appeal – 09/07/2008
F/2007/0293	Application for the proposed retention of a replacement building, an additional building and two mobile buildings.	Allowed at appeal – 09/07/2008
CLE/2009/0382	Application for a certificate of lawfulness for the existing use of the land for wet waste operations including collection, disposal and loo hire.	Refused – 14/04/2009
192419	Full application for change of use from existing private woodland to informal recreation including the provision of access off Bearwood Road, car park and associated works.	Refused – 28/01/2020

--	--	--

SUMMARY INFORMATION	
For Commercial	
Site Area	14.77 ha
Previous land use	Woodland
Proposed floorspace of each use	N/A
Change in floorspace (+/-)	N/A
Number of jobs created/lost	N/A
Existing parking spaces	0
Proposed parking spaces	18

CONSULTATION RESPONSES	
Natural England	No objection.
Berks, Bucks and Oxon Wildlife Trust	No comments received
Woodland Trust	Objection due to potential damage, deterioration and potential loss of ancient woodland.
Forestry Commission	Refer to policy and standing advice.
Health and Safety Executive	No objection.
Crime Prevention Design Officer	No comments received
National Grid	No comments received
Royal Berkshire Fire and Rescue	No comments received
Southern Gas Networks	No comments received
SEE Power Distribution	No comments received
Thames Water	No comments received
NHS Wokingham Clinical Commissioning Group	No comments received
WBC Biodiversity	No objection subject to conditions.
WBC Planning Policy	No objection.
WBC Drainage	No objection.
WBC Education (School Place Planning)	No comments received
WBC Environmental Health	No objection.
WBC Highways	No comments received
WBC Heritage & Conservation	No comments received
WBC Tree & Landscape	No comments received
WBC Cleaner & Greener (Waste Services)	No comments received
WBC Property Services	No comments received
WBC Public Rights of Way	Request further information prior to determination.
WBC Green Infrastructure	No objection, subject to conditions.

REPRESENTATIONS

Town/Parish Council:

Barkham Parish Council: Object to this application for the following reasons:

- There is insufficient information as to who will own and manage the proposed land if it is permitted. There is no funding identified.

- The Parish Council is unaware of any planned development in the area which would justify SANG to offset this. The application is premature in relation to the Local Plan Update.
- There are aspects of the site that currently make it unsuitable for consideration as SANG. This includes excessive noise from nearby A1 metal recycling operation. An abatement notice has been served which has not been complied with. The land may be polluted.
- The proposal involves the conversion of natural woodland to an artificial environment to mitigate the impact of development on the Thames Basin Heaths SPA.
- Many consultation responses in relation to this application suggest that the area should be opened up to cyclists, horses and quad bikes. The Parish Council questions if these are appropriate and they would result in an area where people can currently walk safely being converted into an area where they would have to compete with these faster options.
- At a time when Wokingham Borough Council are seeking to plant more trees, it must be possible to provide SANG facilities elsewhere that will not entail the felling of trees.
- At the very least, no approval should be given and no work undertaken until the issues of noise and ground contamination have been resolved.
- It is not appropriate for the applicant to downgrade the status of the land as Ancient Woodland. There are no wholly exceptional reasons for harm to the ancient woodland.
- There would be harm to wildlife.
- There is no long term business plan submitted.
- Ground contamination has not been sufficiently addressed.
- There is known archaeology in the area.
- The Greenway may be affected by this scheme.

Officer Note: This application does not relate to the provision of SANG. In planning terms SANG is not a use of land therefore the description of the application must reflect the use of the site as informal recreational space. The land would become SANG at the point that it is utilised in relation to new housing developments. Therefore, any comments in relation to the suitability of the land for use as SANG are not relevant to this planning application.

Wokingham Town Council:

Support this application however have the following concerns:

- The internal fencing will have a detrimental effect on wildlife movement within the area.
- The car park should not be included. It does not include disabled parking. There is a road safety issue on access to the highway to and from the car park.
- There should be no alterations to current bridleways.

Local Members:

Cllr Paul Fishwick:

The Planning Statement says that the application demonstrates that the site is sustainably located. However, from the Sindlesham side the footway ends at St Catherines Lodge and then there is a 50mph road to walk along. This route is also poor for cyclists due to the lack of infrastructure and high speed limit. Therefore, residents from this area will not be encouraged to use a sustainable and active travel mode but to access the site will be given only the choice of the car as there is no public transport provision. This goes against the Councils own core Strategies.

Officer Note: The site is not well connected to Sindlesham by footpath. However there is a pavement which extends along Bearwood Road to highways Avenue from the Barkham direction. A significant number of people will access the site from other entrances from Woosehill. The Highways Officer has not raised objection to this scheme.

Cllr Sarah Kerr:

The design is very much aimed at new people coming to enjoy this woodland and I would like to see further consideration of the community that currently enjoy it. There is some concern that dogs may be able to escape into neighbouring gardens. The footpath is higher than neighbouring gardens and this would therefore potentially lead to loss of privacy. The site is not sustainably located. There is no pedestrian access to the car park and residents from Sindlesham cannot access the site by foot. This will lead to more car journeys, appropriate infrastructure is needed. The speed limit on Bearwood Road should be reduced.

The footpath starts and ends at the car park and this is not designed with the local community in mind. There is only cycle parking at the car park, rather than at any other entrance. Why aren't bicycles to be allowed? Will the Council's Greenway be compromised?

Highland Avenue and Limmerhill provide 2 access points to the proposed SANG. Both of these routes are unadopted highways. It is important that consideration is given to ensure these routes are maintained to a standard that allows public access. The kissing gates need to be of appropriate width to allow mobility scooters through.

Officer Note: See above note In relation to Cllr Paul Fishwick comments. It is not clear that dogs could escape into neighbouring gardens. The land is already used for walking of dogs in any case. The Council's Highways Officer has not recommended a reduction in the speed limit on Bearwood Road. The applicant has not recommended that bicycles are permitted in the open space. Bicycles, the Green Way and the width of the kissing gates are discussed in the relevant sections of this report.

Neighbours: A total of 105 comments have been received, including 45 comments in support, 28 objections and 32 more general comments.

Support:

- The proposal would prevent the land being used for new houses and would keep the land open for public access, instead of being closed off (the land is private). The importance of the land has been recognised in the local plan update.
- There has already been detriment to the woodland as a result of increased footfall due to lockdown. Provision of footpaths will help prevent further harm and will make the land more accessible to more people.
- Rubbish and garden waste needs removing from the site for safety reasons and in case of introduction of invasive species.
- Will there be any removal of invasive species from the neighbouring WBC woodland? Removal of invasive species will be positive without any cost to the tax payer. The shortcut links shown on the wayfinding plan should be surfaced to the same standard as other paths.
- Trees should be replaced where felled and veteran trees avoided.
- There are a number of things shown on the wrong location on the plans.
- The current situation has shown how important local green space is to people's wellbeing. The space would be family friendly.
- We should secure as many open spaces in Wokingham as possible.
- The provision of a car park will be beneficial and will prevent nuisance parking.
- The proposal would protect the existing BMX area.
- This application secures the long term maintenance and use of the land for biodiversity net gain.
- Issues with the previous application have been overcome.
- Other SANG areas have prospered while keeping their natural beauty and wildlife. There will be better access for surrounding residents.

Comment:

- The area has been used for horse riding for a long time. It provides a safe route off of busy Bearwood/Barkham Roads. There are less places to ride horses due to lots of new housing development. Horses should be allowed into this space along with cyclists and dog walkers. Horses have not been destructive and should be allowed. A bridleway is required.

Officer Note: It is recognised that the proposed does not make provision for horses. However, While it would be beneficial to provide access for horse riders, it is not considered that the lack of appropriate provision would substantiate a reason to refuse this application. It is not unusual for formalised country parks or areas of public open space to not cater for horse riders and due to the narrowness of the footpaths, there is likely to be some conflict between horse riders and pedestrians.

- Cyclists and horse riders use the space which protects them from aggressive drivers. Horse riding is important to support mental health.
- The wood should be left as it is, however a long term management plan may be appropriate and could protect wildlife.
- The site is not suitable to be used as SANG, which should be free from intrusions. The fencing at the scrapyard could be improved, in the winter it is significantly visible. The scrapyard creates significant noise pollution. The stream which runs through the site is subject to chemical pollution.
- The 20 metre wide strip for the footpath encroaches into neighbouring private land.

Officer Note: Permission – if granted would not extend to works outside of the application site (apart from that secured by legal agreement). The width of the Rhododendron to be removed is sufficient even though it would be narrower than 20 metres in places because of being close to the edge of the site.

- The removal of trees may lead to surface water runoff into neighbouring gardens.

Officer Note: The Council's Flood Risk and Drainage Officer has not objected on this basis.

- The footpath is elevated adjacent Kent Close and Riding Way and therefore would provide views into neighbouring properties.

Officer Note: Please see 'overlooking' section of this report.

- Fencing around the edge of the site is not clear.
- Wildlife should not be hindered by fencing.
- Neighbours have entrances into the woodland from their gardens and this could be lost.

Officer Note: The fencing is shown on the plans. The Council's Ecologist has not objected on the basis of the provision of fencing. This area of land is private and the right in Planning terms to access it directly from adjacent gardens is not established. These accesses appear to have accumulated over time and there is no Planning reason to insist that they are retained. The landowner could restrict access to any part of the land at any time.

- There is an error in the wayfinding plan – Limmerhill Road is shown as Dorset Way.

Officer Note: This error is noted but it does not prevent the determination of this application.

- The outline landscape document states that barbeques would be allowed.

Officer note: There is no planning reason to prevent barbeques.

- Bicycles will be blocked by kissing gates. Limmerhill Road and Highlands Avenue are unadopted roads. This also applies to people with mobility scooters etc.

Officer Note: Please see relevant sections of this report.

- There is no acoustics report which deals with noise from the adjacent scrap yard. The scrap yard has been served with a noise abatement notice. There is concern that removal of Rhododendron will cause increased noise disturbance.
- The woodland should be left to thrive on its own. There is constant noise and pollution from the neighbouring scrap yard. Removal of Rhododendron may remove noise barriers and increase disturbance to local residents.

Officer Note: This is discussed in the relevant section of this report. The noise from the scrapyards is not considered to harmfully affect the function of the open space. The Council's Environmental Health Officer has not objected to the removal of Rhododendron.

- There is no way for pedestrians or cyclists (off-road) to visit the site from Sindlesham or Winnersh as there is no pavement to the site. This is not a sustainable location.

- The vehicle access is close to a bend in Bearwood Road. There is no pedestrian access off the road and there are highway safety issues. This does not support access on foot.

Officer Note: The site is not well connected to Sindlesham by footpath. However there is a pavement which extends along Bearwood Road to highways Avenue from the Barkham direction. A significant number of people will access the site from other entrances from Woosehill. The Highways Officer has not raised objection to this scheme.

- The removal of Rhododendron may have a negative impact on protected species.

Officer note: Please see 'ecology' section of this report. The Council's Ecologist considers the approach regarding Ecology to be appropriate.

- How will the BMX area be accessed?

Officer Note: This is not clear however this lies on private land and this is not determinative.

- There should be better access for the fire brigade and other emergency services.

Officer Note: It is noted that there is vehicular access provided. The level of access would be similar to that available at present.

- The footpath is higher than neighbouring gardens and this would therefore potentially lead to loss of privacy.

Officer Note: Please see 'overlooking' section of this report.

- The scheme is un-necessary.

Object

- This proposal is fundamentally the same as the previous application.
- The proposal is likely to have detrimental impacts on existing wildlife. The woods are home to a number of animals. The development is not required or necessary as this land is freely accessed – why is it marked as private on the application? The removal of trees will destroy habitat. The increase in the number of dogs on the land would harm wildlife.
- This area has been open for walkers for many years. Formalising it will destroy the character of the area. There are a number of comments in support. Do people know that this is already an established walking area?
- Limmerhill field has recently been closed to residents and this has led to more deer in neighbouring gardens and fatalities in the road. This will be made worse by the change of use of the site. The fencing surrounding the site will limit wildlife movement.
- The owners of this land have a lack of regard for flora, fauna and neighbours, This application is not submitted in the interest of the community. Ecology should be prioritised without formalising the space. The neighbouring scrapyards pollute the area.
- The land has been open for all to access for 30 years.

Officer note: Please see 'ecology' section of this report. The Council's Ecologist considers the approach regarding Ecology to be appropriate. The land is private and access to the

public has historically been allowed by the land owner(s). This could be withdrawn at any time.

- There are no details included as to how litter and fly tipping will be prevented. The pandemic has already led to increased use and littering.
- It is a breach of the equalities act to approve this scheme. Cyclists are vast majority male and horse riders are vast majority female. As a predominantly female group, horse riders must have 'protected characteristics'. Many horse riders are over 35 and therefore are defined as 'hard to reach' in terms of promoting physical activity. Equal access to horse riders and cyclists needs to be ensured.
- The proposal would not be properly accessible to cyclists – including those who commute to Reading.

Officer Note: There is no evidence provided that 'most horse riders are female and most cyclists are male'. Access for cyclists is discussed in the relevant section of this report.

- The car park is unnecessary.
- Wokingham already has a number of country parks and this one is not needed.

Officer Note: Need is not a material consideration in the determination of this application. The car park has been provided to provide an increased level of access.

- There would be harm to the Ancient Woodland.

Officer Note: Potential harm to the ancient woodland is considered in the relevant section of this report.

- A secondary fence adjacent the garden fences edging the site is not required.
- The proposal will facilitate private housing.

Officer Note: See main body of report in relation to access by horse riders and cyclists. The change of use of the land must be assessed on its own merits and it is not brought forward in relation to any housing scheme. There is no objection to the provision of an extra fence alongside the existing garden fences.

Friends of Foxhill:

Support the proposal. The scheme Preserves public access to a widely used open space. The path is more accessible to wider range of people including disabled. Many of the paths follow established informal routes. The car park now includes disabled bays.

There are a number of positive aspects in terms of wildlife conservation, including the restoration of the pond, and removal of invasive species.

However, there are incorrect road names listed on the plans. Are barbeques to be allowed on site? The fire brigade need to be able to gain access. The fencing does not go all the way around the site. No fencing is necessary. The 10 metres strip either side of the footpath goes into some neighbour's gardens. There will be inadequate space for screening. There are alternative paths available that wouldn't require tree removal. Would trees which have been removed be replaced? There is no mention of ancient and veteran trees. Would other protected species be protected? There are concerns relating noise from the scrap yard.

Officer Note: There is no objection to the provision of an extra fence alongside the existing garden fences. The fire brigade would have a similar level of access to the present site. See above notes in relation to other points.

A number of comments are made regarding Natural England SANG requirements and the suitability of the site as SANG. SANG is not a land use in Planning terms and this application is to be assessed as an area of informal public open space. Should the applicant seek to utilise the land as SANG, then this assessment of suitability would occur at the time of an application for housing.

There are a number of comments from other organisations below:

Loddon Valley Ramblers:

No objection subject to:

- The full preservation of the current route of Wokingham Within Footpath 1 and a condition being imposed to improve its surface and signposting through the site of the application; and
- The creation of the SANG having no impact on (and preferably helping to facilitate) the creation of the proposed Arborfield Cross to Wokingham Greenway.

Wokingham Access and Bridleways Officer – British Horse Society:

- The footpath network in the local parishes is extensive and allows walkers to enjoy many circular routes. However, the bridleway network in these parishes is disjointed with horse riders and cyclists having to use busy roads to link bridleways together and create circular rides. Wokingham Borough has plans for 13,000 new homes to be built by 2026, the increased traffic this will bring will make riding a bicycle or a horse on the roads more undesirable. Increasing the number of bridleways is essential to help provide these vulnerable road users a safer alternative.
- A survey of horse owners and business in Wokingham Borough was carried out in 2014. 100% of respondents agreed that increased development in and around Wokingham including the Arborfield development will dramatically increase traffic in the area making it more dangerous for riding on the roads. 2% of riders will continue riding on the roads even if the traffic is significantly heavier. 12% think they will stay at their yard but probably stop hacking out. 86% will consider moving out of Wokingham Borough to find livery in a quieter area. 100% of Equestrian Business owners agree that without more Bridleways to provide safer off road riding, it is likely that horse owners will choose to find livery out of the area. 92% agree that without new Bridleways their business opportunity within the Borough will be reduced. 60% of businesses expect to have to reduce their staff numbers. 50% of livery yards anticipate closure. 55% of the livery businesses who returned forms are within a 10 minute walk of a development. 13% of non livery businesses anticipate closure or having to move out of the Borough.
- It is concluded that without investment in bridleways, horse owners and businesses are likely to leave the area, costing the borough economically.
- A number of pathways in the woodland have been used in excess of 20 years and therefore people have the legal right to use them.
- The historic use of Foxhill has been for by walkers, cyclists and horse riders. To disregard the historic use is poor. Bridleways have been included in SANG before.

- The proposal is contrary to policy as it will not protect existing bridleways. The Green Route is proposed in this area.
- To exclude use by horse riders and cyclists goes against this and the Government's objectives to combat obesity and promote health and well being.
- Bridleways are important as they allow use by all non motorised users. Although it is usual SANG policy to exclude horse riders from SANG areas, this is discrimination as horses pose no threat to the wildlife, in particular ground nesting birds. Horse manure (unlike dog droppings) is fully biodegradable and poses no health concerns.
- Horse riders are disproportionately women and girls and therefore restrictions will disproportionately affect them.
- Exclusion of horse riders completely will be very inconvenient for riders who currently rely on this woodland for exercising their horses. The stables along the edge of Foxhill each has a gate or slip rail access directly into the woodland. Their use of this woodland is increased during rush hour when riding along Bearwood Road is very undesirable, also during the Winter time when the light is lost so early. These riders are able to ride here in the half light and when use of the roads is unsafe even with lights and reflective clothing.
- A suggestion is made for a bridleway route around the outside of the open space.

Officer Note: While the concerns of a number of horse riders are acknowledged, it is not considered that the lack of provision for horses would substantiate a reason to refuse this application. Horses may also lead to some conflict with other footpath users. Use of this site for horse riders has been informal and allowed by the landowner in the past. Please see relevant section of this report.

APPLICANTS POINTS
<ul style="list-style-type: none"> • The applicant has engaged with Wokingham Borough Council (WBC) Officers when designing this scheme. • This submission addresses the previous reasons for refusal of the refused similar scheme. • The application demonstrates that the Site is sustainably located and that a high-quality development will be delivered, experienced and enjoyed. The development accords with the relevant policies contained within the Development Plan for the Borough and is also consistent with national planning policies set out in the National Planning Policy Framework (NPPF).

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area

	CP9	Scale and Location of Development Proposals
	CP11	Proposals outside development limits (including countryside)
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB21	Landscape Character
	TB23	Biodiversity and Development
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide – Section 4
		DCLG – National Internal Space Standards

PLANNING ISSUES

Description of Development:

1. The application site consists of a large area of Woodland, interspersed by footpaths and open space. An electricity pylon crosses the site from north to south. To the west lies a breakers/scrapyard. To the east lies Woosehill, a 1980's housing estate with good pedestrian access to the site. An area of woodland owned by the Council lies to the east, just outside the application site and this is traversed by a public footpath. The site is private but has been left open to public access for a significant period of time, this has led to a good network of footpaths and some neighbouring properties have created access points into the woodland.
2. The application seeks the change of use of the site to informal recreational space, with the stated aim of becoming Suitable Alternative Natural Greenspace (SANG). In planning terms SANG is not a use of land therefore the description of the application must reflect the use of the site as informal recreational space. The land would become SANG at the point that it is utilised in relation to new housing developments.
3. Two applications have recently been received for a Definitive Map Modification Order to be made to recognise a number of footpaths and bridleways over this land.

4. It should be noted that this is the second submission of this application (previous ref: 192419). The previous scheme was found to be unacceptable for reasons relating to the impacts of the scheme on the character of the area, protected trees, ancient woodland, potential impacts on protected species, obstruction of a public right of way, lack of disabled parking and lack of disabled access provision.

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay unless material considerations indicate otherwise.
6. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories.
7. The proposal is in the Countryside and is therefore subject to Core Strategy Policy CP11. This policy states that proposals that contribute and/or promote recreation in, and enjoyment of, the Countryside are generally acceptable in principle subject to the impact on the rural character of the area and that that it does not lead to excessive encroachment or expansion of development away from original buildings. Paragraph 96 of the NPPF considers that access to a network of high quality open spaces and opportunities for sport and physical activity is important for the health and well-being of communities.
8. The use of the land as recreational space would not conflict with CP11. There are no 'original buildings' as referred to in CP11, however, the provision of a car park, footpaths, fencing and furniture would result in additional built form and encroachment into the Countryside. However, it is not considered that the encroachment is excessive, as the provisions are reasonably required to serve the space and the car park is of an acceptable size. Additionally, this scheme formally allows the public onto what is currently private land, providing significant community benefit and weighing in it's favour. It is therefore considered that the scheme is acceptable in principle.

Character of the Area:

9. Section 15 of the NPPF indicates that Planning policies and decisions should contribute to and enhance the natural and local environment by:

...

- b) *Recognising the intrinsic character and beauty of the countryside, and the wider benefits from natural capital and ecosystem services – including the economic and other benefits of the best and most versatile agricultural land, and of trees and woodland;*

...

10. Core Strategy Policies CP1 and CP3, require new development to maintain or enhance the high quality of the environment. TB21 of the MDD Local Plan indicates that

proposals should respect the landscape character of the area, retain or enhance existing landscape features such as trees and woodland and providing for appropriate landscaping. Landscaping should consist of locally native species. Policy CC03 of the MDD Local Plan requires development proposals to demonstrate how they have considered and achieved the following criteria within scheme proposals:

- a) Provide new or protect and enhance the Borough's Green Infrastructure networks, including the need to mitigate potential impacts of new development
- b) Promote accessibility, linkages and permeability between and within existing green corridors including public rights of way such as footpaths, cycleways and bridleways
- c) Promote the integration of the scheme with any adjoining public open space or countryside
- d) Protect and retain existing trees, hedges and other landscape features
- e) Incorporate high quality, ideally, native planting and landscaping as an integral part of the scheme.

11. The policy also states that development proposals which would result in the loss, fragmentation or isolation of areas of green infrastructure will not be acceptable.

12. The Wokingham District Landscape Character Assessment indicates the site is located within Landscape Character Area L1: Bearwood Wooded Sands and Gravels. Of the particular key landscape characteristics listed within the document, the following relate to this site:

- Undulating upland hills comprising London Clay overlain by sands and gravels of the Bagshot Beds and Head Gravel.
- Heavily wooded 'forested' character comprising large blocks of distinctive coniferous planting interconnected with swathes of mixed and deciduous woodlands.
- Very sparsely settled with small stretches of roadside ribbon development, some new executive style houses and isolated farm buildings.
- Woodland defines and screens the western boundary of the town of Wokingham and creates a wooded backdrop to views from the surrounding landscape.
- Recreational use including golf courses, horse riding establishments and network of public footpaths.
- Few roads creating a relatively secretive landscape.

13. The application site is consistent with the Wokingham District Landscape Character Assessment, being largely wooded with mixed deciduous woodland and being the backdrop to the western side of Wokingham. Rhododendron has also significantly colonised the area. The proposal would consist of the following operational development:

- Construction of a car park with 18 parking spaces (including 3x disabled spaces).
- Provision of a surfaced circular walking route.
- Provision of timber post and rail fences.
- Provision of two foot bridges.

- Provision of dog waste bins, litter bins, benches, wayfinding posts, information boards and gates.
14. The previous planning application on this site was similar in scope to the current proposal. However, the impact on trees on the site was much more significant. It included the clearance of native species to produce 'wildflower areas' and the creation of glades produced by tree removal.
 15. The site is heavily wooded and all trees are protected by a Tree Preservation Order. In line with the District Landscape Character Assessment, the importance of the trees to the character of the area is high and therefore any negative impact on the trees would be significantly harmful. Therefore, it is essential that the Local Planning Authority is able to understand how the trees would be impacted by the proposal. This is in line with the requirements of policies CP3, CC03 and TB21.
 16. Under the previous proposal, no details were submitted which allowed the Local Planning Authority to assess how the trees would be impacted. It is understood that the full impact on trees cannot be revealed until thick areas of Rhododendron, which has colonised and forms inaccessible dense thickets, has been removed. The scheme involves only modest removal of trees around the car parking area. Any other works to trees will be informed by surveys after the removal of Rhododendron within a 20m wide band on either side of the proposed location of the footpath. To facilitate the retention of trees, the applicant has stated that the precise location of the footpath is indicative and could be situated anywhere inside the 20m buffer of removed Rhododendron (which is shown on plan numbered 6768.PS.02.REV F). This will enable the footpath to be situated in order to avoid trees of significance. There is no objection to this approach, however it will be necessary to apply a condition which requires the submission of details relating to the exact location of the footpath when this has been determined and prior to its implementation.
 17. The car park is proposed to be in the same location as under the previous application. Issues were previously raised with regards the car park being too close to Bearwood Road, which is a Green Route. The tree lined character of this road is therefore specifically protected by policy CC03. There will be some tree removal in the location of the proposed car park and to create an access off Bearwood Road, however these will be replaced with new native tree planting around the car park as shown on the Proposed Car Park Detail Plan. It has also been agreed the Rhododendrons along Bearwood Road to the front of the proposed car park will be retained, but managed, as this is a significant characteristic of Bearwood Road in this area and will provide visual screening of the car park from the road. It is proposed to clear the rhododendrons for the car park and in the immediate vicinity to create an open wildflower area at the start of the trail. Given the above and the lack of objection from the Trees and Landscapes Officer, it is now considered that the location of the car park is acceptable, subject to the retention and management of Rhododendron along Bearwood Road and the implementation of the landscaping scheme.
 18. It is considered that the proposed benches and other furniture is modest and would not harmfully impact the character of the area. The site will be delineated by a timber post and rail fence. This would divide up the existing woodland and due to its extent would have some impact on the character of the area. However, it is unlikely to be significantly harmful and full details can be required by condition.

19. On balance, the proposal would retain the majority of trees on site, while successfully removing and managing significant amount of Rhododendron and other invasive species and opening up the area to more visitors. While this would have an impact on the character of the area itself, some Rhododendron would be retained along Bearwood Road and this would protect the character of the Green Route. In any case, the removal and management of the Rhododendron would have corresponding benefits and following this, a condition will ensure that a minimal number of trees are removed in order to facilitate the footpaths.

Ancient Woodland:

20. Part of the woods at Foxhill fall into Natural England's provisional Ancient Woodland Inventory. Natural England and the Forestry Commission define ancient woodland "as an irreplaceable habitat [which] is important for its: wildlife (which include rare and threatened species); soils; recreational value; cultural, historical and landscape value [which] has been wooded continuously since at least 1600AD." It includes:
- "Ancient semi-natural woodland mainly made up of trees and shrubs native to the site, usually arising from natural regeneration
 - Plantations on ancient woodland sites – replanted with conifer or broadleaved trees that retain ancient woodland features, such as undisturbed soil, ground flora and fungi"
21. Paragraph 175 of the NPPF states that *development resulting in the loss or deterioration of irreplaceable habitats (such as ancient woodland and ancient or veteran trees) should be refused, unless there are wholly exceptional reasons and a suitable compensation strategy exists...*
22. The supporting text of Policy CP7 of the Core Strategy states that the need for a development that affects Local Wildlife Sites, habitats or, species of principal importance in England for nature conservation, ancient woodland, veteran trees or features of the landscape that are of major importance for wild flora and fauna will be deemed to outweigh the need to safeguard the nature conservation interest where the development has clear social or economic benefits of regional or national importance. TB23 of the MDD Local Plan lists Ancient Woodlands as sites of Local Importance.
23. The submitted Outline Landscape and Ecology Management Plan (EPR, ref: 1908-1H, December 2020) attempts to make the case that none of this site is ancient woodland. The Council's Ecologist is not convinced by the details put forward by the applicant that it does not include parts of ancient woodland and therefore this is not accepted. The Council's trees and landscapes Officer has neither agreed or disagreed with this view, however they indicate that as there are to be very few tree removals from this area, this is not relevant. The Woodland Trust object to this proposal. They consider that, while access to Ancient Woodland by the public is important, this must be balanced against the harms which may occur. Natural England's standing advice recognises the potential for ancient woods to be affected by direct and indirect impacts associated with new development, with the following potentially relevant in this case:
- damaging or destroying all or part of them (including their soils, ground flora or fungi)
 - damaging roots and understorey (all the vegetation under the taller trees)
 - damaging or compacting soil around the tree roots
 - changing the water table or drainage of woodland or individual trees
 - increasing disturbance to wildlife from additional traffic and visitors
 - increasing light pollution
 - increasing damaging activities like fly-tipping and the impact of domestic pets

- changing the landscape character of the area”

24. The Woodland Trust has objected to this application on the basis of potential for damage, deterioration and loss of this ancient woodland. They point out that potential harm to Ancient Woodland relates not just to trees, but also to roots, ground flora, understorey, compaction of soils, changes in water table, disturbance to wildlife, light pollution, fly tipping and pets, changes to the character of the area. Their specific concerns with regard this application relate to:
- Potential for direct loss of ancient woodland habitat and soil from the installation of recreational infrastructure, including new paths, benches, sculptures/displays and informal play areas.
 - Intensification of the recreational activity of humans and their pets can result in disturbance to breeding birds, vegetation damage, trampling, litter, use of motor vehicles, vandalism and fire damage.
 - Additional pollution in the form of noise and light and dust pollution, during both construction and operational phases.
 - With increased human activity, it is more likely that the trees within the ancient woodland will need to be more extensively managed to avoid issues related to health and safety. For example, where trees and branches overhang public footpaths and other areas, whole trees or some of their deadwood features will likely need to be removed to ensure public safety. This is particularly relevant to any ancient or veteran trees present within the ancient woodland. Deadwood is an essential part of a woodland ecosystem, which many species rely on, and so the increased need to manage public safety will effectively sanitise the woodland and denigrate its ecological value.
 - Adverse hydrological impacts can occur where the introduction of hard-standing, such as footpaths, can affect the quality and quantity of surface and ground water. This can result in changes to the stable hydrological and nutrient condition of the woodland.
 - Any ancient and veteran trees within the woodland must be identified and afforded appropriate protection in the form of a root protection area/buffer zone.
25. The above view has been countered by the Council’s Ecologist. They believe that the proposed management option of continuous cover forestry is sympathetic to the ancient woodland status and, if an irregular forestry approach is taken, allows for discretion as to tree stocking rates and the size classes used to favour an ecological and climate change focus. The proposed management of the existing woodland in this application is different to the previous application and the current proposal would not result in a net loss of ancient woodland. Further to this, the introduction of a hoggin surface path through the wood, following the indicative route, would not be to the detriment of the ancient woodland. It may well prove a benefit in the long run – providing a suitable hard surface that can take the pedestrian footfall and providing a more desirable alternative than the use of other paths during periods when there is a high poaching risk. The circular walk has been designed to formalise existing desire lines in places so the path will not reduce the ground flora cover in these areas.
26. The Council’s Trees and Landscapes Officer has not objected to this proposal. They have indicated that the trees in the area of the site covered by Ancient Woodland would be relatively unaffected by the scheme. Additionally, no objection has been received from Natural England.

27. Considering the above, a balanced view needs to be taken. It is noted that the land is already extensively used for recreational purposes and this is evidenced by the ongoing applications for 2 modification orders which may demonstrate significant access and additional rights of way over the site. In any case, it is common knowledge that this area is well accessed by the public. What cannot be quantified at this point is exactly what the increase in footfall will be if the site becomes formalised recreational space (if any). The policy in relation to loss or deterioration of Ancient Woodland is worded strongly. The NPPF states that this should be 'wholly exceptional'. The supporting text of CP7 states that proposals affecting Ancient Woodland will be deemed to outweigh the need to safeguard the nature conservation interest where the development has clear social or economic benefits of 'regional or national importance'.
28. It cannot be said that the provision of formalised recreational space at Foxhill is of regional or national importance. However, the land is currently private and could be closed to the public at any point. It appears less likely that the owners would be less inclined to do so should they have formally laid the area out as recreational space. There is strong support from the local community for this land to be kept open for access by the public. In this way, the provision of a formalised recreational space, which is also more accessible to people does provide a notable local benefit. It appears likely that the provision of hard surfacing will result in some damage to ground flora, however as has been noted by the Council's Ecologist, this is largely along existing desire lines and therefore this will be minimised – harm is likely occurring already. The footpaths are also likely to incentivise walkers to remain on the hard surfacing and therefore lessen adverse impacts of trampling and poaching (which was seen extensively on the Officer's site visit). It is considered that the implementation of benches and other furniture would be minimal in terms of its effect on the woodland as they are modest in scale and quantity. It is noted that the areas designated as Ancient Woodland do not include the car park area and therefore the more significant tree removal in this location, while facilitating the use of the woodland for recreational purposes, would not lead to direct damage.
29. For the above reasons, it is considered that the most influential impact on the health of the woodland is likely to be an increase in footfall. For the reasons discussed, it is considered that the provision of footpaths will have a minimal impact on the ancient woodland as a whole and largely run along existing desire lines. They will likely encourage people to remain along these paths, rather than finding alternative routes across the land as is the case currently. The Council's Ecologist has raised no objection with regards to breeding birds, litter, vandalism or fire damage. Natural England have not objected to the proposal. The Council's Trees and Landscapes Officer has not objected in terms of the potential impact on trees.
30. It is not considered that there would be a significant increase in noise or dust pollution as a result of greater pedestrian access and the impact of creating the paths and installing furniture would be minimal. There is likely to be some removal of dead trees in relation to safety issues. However, the amount of hard surfacing proposed is minimal within the areas of ancient woodland and it is not considered that it would significantly impact the water table or surface water runoff. Ancient and veteran trees would not be significantly impacted because, as has been discussed, it is not considered that there would be a significant increase in footfall off the pathways. The location of the paths is being informed by further arboricultural surveys. The removal of Rhododendron and other invasive species may have an immediate impact on the ancient woodland

through disturbance, however it will have a long term beneficial impact where these species are having a harmful detrimental impact on the woodland and its ecology.

31. For the above reasons, it is not considered that the proposal would lead to the loss of deterioration of the Ancient woodland and therefore it is considered that the scheme is in line with policy and is acceptable. Additionally, the removal of invasive species will lead to a positive impact on the Ancient woodland and the site would be opened up to more visitors.

Residential Amenities:

32. New development should not result in a scale of activity that would be detrimental to the amenities of adjoining landowners (Core Strategy Policy CP3).
33. The nearest dwellings to the proposal are those within Woosehill (Ruskin Way, Tiffany Close, Riding Way, Kent Close, Dorset Way, Limmerhill and Chaucer Way). There are also neighbours at the scrapyards on Highland Avenue (the applicant), Silver Birches (Highland Avenue), Gleniffer Farm and numbers 148, 174 and 176 Bearwood Road. The land is already largely accessible to the public, despite being privately owned. It is not considered that the increase in the use of the land as public open/recreational space would be significantly harmful to neighbouring dwellings in terms of loss of light or overbearing impacts. While there would be more people accessing the land, any overlooking impacts would not be such that they couldn't be screened by an ordinary garden fence. A number of objections have been received in relation to the provision of the footpath along the northern boundary and it has been stated that because the land is raised in this location, there would be overlooking impacts on neighbouring properties. It is noted that this footpath is in the same location as the existing footpath. It is not considered that the impact would be significantly harmful compared to the existing situation and this is therefore acceptable.
34. In terms of noise, the dwellings to be most affected would be those close to the proposed new car park, namely numbers 148, 174 and 176 Bearwood Road. These would still be some distance from the proposal and the Environmental Health Officer has raised no objection to the location of the car park. It is therefore not considered that the proposal would be harmful from a noise standpoint.

Access and Movement:

Highway Safety:

35. Core Strategy Policies CP1 and CP6 require new development to be in accessible locations, provide access by a range of modes with emphasis on sustainable travel, provide appropriate parking, and mitigate any road safety or other highway related problems.
36. The Council's Highways Officer has indicated that it is unlikely that traffic from this proposed development would have an adverse impact on the highway network. They have also indicated that the access being wide enough for two cars is positive. However a condition would be required to ensure the provision of a height barrier. The hard surfacing of the access road is considered positive. There are less parking spaces proposed than under the previous application (18 instead of 25). The officer previously indicated (under the last application) that the number of parking spaces required for recreational space would be 1 space per 20 square metres. This is not realistic and therefore 18 spaces is acceptable. There is also sufficient disabled parking.

37. The visibility splays should have been based on the posted speed limits. Normally the visibility splay distances are calculated from actual speed surveys, however due to the pandemic none were carried out. The Highways Officer considers this acceptable. More stringent standards have been used which are therefore acceptable. Cycle parking and motor cycle parking will need to be provided and shown on a plan to ensure there is secure parking for any cyclist who may visit the space.
38. For the above reasons, the proposal is considered acceptable in terms of parking and impacts on Highway safety.

Flooding and Drainage:

39. Policy CC09 of the MDD Local plan relates to Development and Flood Risk. It indicates that:

All sources of flood risk, including historic flooding, must be taken into account at all stages and to the appropriate degree at all levels in the planning application process to avoid inappropriate development in areas at risk of flooding.

CC10 relates to Sustainable Drainage. It states:

All development proposals must ensure surface water arising from the proposed development including taking into account climate change is managed in a sustainable manner. This must be demonstrated through:

- a) A Flood Risk Assessment, or*
- b) A Surface Water Drainage Strategy.*

40. The site is currently a greenfield site and will largely remain so, except for the car park to the North West corner of the site, as well as footpaths. The car park would be gravel.
41. The existing watercourses and drains would be maintained with timber bridges and foot bridges constructed. The Officer has raised no objection to the proposal on this basis. Due to the minimal level of development proposed which would cause an increase in the level of runoff and the provisions outlined in the submitted Flood Risk Assessment, the proposal is acceptable in this regard.

Ecology:

42. Section 15 of the NPPF indicates that Planning policies and decisions should contribute to and enhance the natural and local environment by:

...

- d) Minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures;*

Policies CP7 of the Core Strategy and TB23 of the MDD Local Plan relate to Biodiversity and development. TB23 states that Planning permission for development proposals will only be granted where they comply with policy CP7 – Biodiversity of the Core Strategy and also demonstrate how they:

- a) *Provide opportunities, including through design, layout and landscaping to incorporate new biodiversity features or enhance existing*
- b) *Provide appropriate buffer zones between development proposals and designated sites as well as habitats and species of principle importance for nature conservation.*
- c) *Ensure that all existing and new developments are ecologically permeable through the protection of existing and the provision of new continuous wildlife corridors, which shall be integrated and linked to the wider green infrastructure network.*

Paragraph 99 of the government Circular 06/05: Biodiversity and Geological Conservation - Statutory Obligations and Their Impact within the Planning System (this document has not been revoked by the National Planning Policy Framework) states that:

“It is essential that the presence or otherwise of protected species, and the extent that they may be affected by the proposed development, is established before the planning permission is granted, otherwise all relevant material considerations may not have been addressed in making the decision. The need to ensure ecological surveys are carried out should therefore only be left to coverage under planning conditions in exceptional circumstances, with the result that the surveys are carried out after planning permission has been granted.”

- 43. The previous planning application was refused due to a lack of sufficient information being submitted in relation to the potential impacts of the proposal on protected species. The applicant has now suggested a plan which results in the removal of the Rhododendron in a 20m wide strip surrounding the proposed footpath and clearance of the car park area, along with various conservation measures, including the implementation of bat boxes prior to removal of trees. After this, follow up surveys would be carried out in order to determine the potential presence of protected species. The footpath would also then be laid in such a location as to avoid any trees which may host bats and any potential badger setts.
- 44. The Council’s Ecologist has confirmed that at this stage, sufficient protected species surveys have been undertaken to be able to determine the application and the proposed mitigation strategy for loss of trees with bat roost potential is sufficient to be confident that favourable conservation status is maintained. However, an update to the surveys may be required to inform the final route of the path which is chosen. This is in part because it will be possible to inspect trees more thoroughly, but it also relates to the fact that bats and badgers are mobile species that might well be discovered using different locations in the time between determination and implementation. The agreed approach would allow the exact location of the footpath to be finalised later and therefore avoid potential harm.
- 45. A number of neighbour comments have indicated that the proposal will affect wildlife and ecology by virtue of an increased number of visitors and an increase in the amount of fencing on site. The Councils Ecologist has raised no concerns with regards these aspects of the proposal.
- 46. The management of the site over the long term has been detailed in the submitted outline Landscape and Ecological Management Plan (LEMP). The plan includes the management of the woodland over time as continuous forestry cover. The Council’s Ecologist considers that this will deliver a biodiversity net gain, along with the removal

of invasive species. Paragraph 170 (d) of the NPPF indicates that proposals *should minimise impacts on and provide net gains for biodiversity including by establishing coherent ecological networks that are more resilient to current and future pressures*. It is considered that the biodiversity net gain provided by the management of the forest and invasive species is required in order to make this application acceptable. However, they are too general at this stage in order to secure appropriate management over the long term (circa 30 years) and it will need to be periodically reviewed by the Local Planning Authority.

47. For the above reasons, it is considered that the proposal is acceptable in terms of its impact on ecology and protected species. In fact, the scheme will lead to a biodiversity net gain and therefore this weighs significantly in its favour.

Public Rights of Way

48. MDD Local Plan Policy CC03 relates to Public Rights of Way. This indicates that Development proposals should demonstrate how they have considered and achieved the following criteria within scheme proposals:

...

b) *Promote accessibility, linkages and permeability between and within existing green corridors including public rights of way such as footpaths, cycleways and bridleways*

c) *Promote the integration of the scheme with any adjoining public open space or countryside*

...

Development proposals which would result in the loss, fragmentation or isolation of areas of green infrastructure will not be acceptable (green infrastructure includes public rights of way).

...

Proposals should be consistent with the Borough's Public Rights of Way Improvement Plan.

49. CP4 of the MDD Local Plan states that Planning permission will not be granted unless appropriate arrangements for the improvement or provision of infrastructure, services, community and other facilities required for the development taking account of the cumulative impact of schemes are agreed. Arrangements for provision or improvement to the required standard will be secured by planning obligations or condition if appropriate. Infrastructure includes public rights of way enhancements.
50. Wokingham Footpath 1 runs to the south of this proposed development, and forms part of the network of paths identified for the proposed SANG. The Council's Public Rights of Way Officer has requested that this foot path is surfaced along with the circular route being provided. The Officer has argued that this creation of a higher standard of surface and the subsequent deviation from the line of the footpath would have a detrimental impact on Footpath 1 by reducing the footpath to a lower standard of path, and would effectively deter users from continuing on the definitive line of the path. Parts of Footpath

1 are also included within the submitted wayfinding strategy. The proposed works are likely to increase the use of this site by residents who will be approaching from Limmerhill Road, which will increase the pressure and intensiveness of use along Wokingham Footpath 1. This increased use is likely to have a detrimental impact on the natural path surface as a result. The officer recommends that the entirety of Footpath 1 is surfaced to address the hierarchy of path issues mentioned above, and to accommodate the increased traffic along the path that the proposal is likely to generate, as highlighted below. If not, details will need to be provided which show how the footpath interacts with the new surface. The applicant has agreed to carry this out. As parts of the footpath are outside the control of the applicant and on land owned by WBC, this will need to be secured by Planning obligation.

51. With regards gates and barriers, the public rights of way officer has noted that these do comply with British Standard 5709:2018. This will ensure that they are suitable for use for all users, in particular for those with buggies and wheelchairs. Previous barriers (shown under the last application) along footpath 1 have been removed.
52. The PROW officer has indicated that the proposed kissing gates would be more appropriate if changed to the 'large' version, as opposed to the 'medium' version as shown on the current plans, which would be more suitable for people with powered mobility vehicles. It is noted that the gates are at least 1.5m in width. The previous application was refused partly for the width of the gates being less than 1.5 metres, which was recommended to be acceptable by the Green infrastructure Officer. Given that the gates are in accordance with the standard, it would not be reasonable to request a wider gate and those that are recommended are acceptable.
53. The Council has an aspiration to create part of the Greenway Route B through the Fox Hill area. This application does present an opportunity to adjust the line of the Greenway to run through the centre of the site, to create a route for pedestrians, cyclists and horse riders. The Public Rights of Way officer has indicated that this should be provided. The Public Right of Way Officer has referred to a number of policies to support this recommendation, however the only one which refers specifically to the formation of such a Greenway is Core Strategy appendix 7, which relates to the Arborfield Garrison. It states that *Greenways for cyclists and pedestrians running north-south and linking sites of historic interest and recreational value as well as Wokingham, Finchampstead North, Barkham Hill and Shinfield should be provided*. This is only in the appendix and it is not considered that this would hold sufficient weight to require this to be provided within this site. It is not considered that the brief assertion in the appendix of the Core Strategy holds sufficient weight to justify the inclusion of such a condition and this would render it unreasonable.
54. The proposal will lead to positive impacts in terms of opening up the site to a wider range of people who may have mobility issues. This is a benefit of the scheme and weighs in its favour.

Function as Public Space:

55. CP2 of the Core Strategy indicates the following:

To ensure that new development contributes to the provision of sustainable and inclusive communities (including the provision of community facilities) to meet long term needs, planning permission will be granted for proposals that address the requirements of:

- a) *An ageing population, particularly in terms of housing, health and wellbeing;*
- b) *Children, young people and families, including the co-ordination of services to meet their needs;*
- c) *People with special needs, including those with a physical, sensory or learning disability or problems accessing services;*
- ...

CP3 indicates that development proposals should provide for a functional, accessible, safe, secure and adaptable scheme.

Policy CC03 is also relevant.

- 56. The previous planning application included reasons for refusal relating to a lack of adequate disabled access. Under the current application, details of kissing gates have been included, which are now of sufficient width to allow for disabled access. Additionally, the paths are now surfaced which allows for greater mobility. The internal route within the space has been significantly improved since the last application. It is no longer as convoluted and makes use of existing paths.
- 57. A number of horse riders have comments on this application, noting that they currently ride horses across the site and that roads have become progressively busier – making this a more desirable place to ride. A number of objections have been received on the basis that the proposal does not make adequate provision for horse riders. While it would be beneficial to provide access for horse riders, it is not considered that the lack of appropriate provision would substantiate a reason to refuse this application. It is not unusual for formalised country parks or areas of public open space to not cater for horse riders and due to the narrowness of the footpaths, there is likely to be some conflict between horse riders and pedestrians. It is noted that cyclists would likely need to leave their bike at the cycle stand and then enter the space separately. While this would prevent cyclists going through the site, this is not considered sufficient reason to refuse this application and there would also likely be conflict with pedestrians.
- 58. The Environmental Health Officer has raised no objection to the provision of open space adjacent the adjacent scrapyards, which is known to cause noise issues. The Council's Green Infrastructure Officer has recommended that measures are utilised to reduce the amount of noise in the open space. It is not considered that the existing level of noise will significantly affect the use of the land as open space. In any case, any sound barriers or other methods to reduce noise from the scrapyards would likely have a harmful impact on the character of the Countryside/area. This application cannot be used to mitigate an existing noise problem from the scrapyards.
- 59. For the above reasons, the proposal is considered acceptable as a functional piece of public open space and the benefit of opening up the space to those with mobility issues weighs in its favour and makes this a positive scheme.

Habitats Regulations Assessment:

60. As this application does not result in a net gain in dwellings, it does not require an appropriate assessment regarding recreational pressure on the Thames Basin Heaths. The scheme is therefore acceptable in this regard.

SANG:

61. The details submitted with this application often refer to the open space as SANG. It is re-iterated that the land would not become a SANG until it is being used to mitigate impacts on the SPA. Therefore, any reservations which the Local Authority has over the potential function of the land as a SANG cannot constitute reasons for refusal. However, due to the clear intention of the land to be used as SANG, it is worth commenting on these aspects.
62. Natural England have not objected to the proposal and as has been discussed, in principle the use of the land for accessible green space is acceptable. However, were the land to be used specifically for SANG it would need to meet the criteria as set out in the Natural England guidance. Appendix 2 of the Guidance sets out the site quality checklist; the wording is described as precise and states that if the requirement is referred to as 'must' then it is essential and is an absolute requirement. These are listed below and are addressed in turn.

The 'must haves' are as follows:

- (a) For all sites larger than 4ha there must be adequate parking for visitors, unless the site is intended for local use, i.e. within easy walking distance (400m) of the developments linked to it. The amount of car parking space should be determined by the anticipated use of the site and reflect the visitor catchment of both the SANGS and the SPA.
- As an area of public open space, this is likely to be largely used by local people from adjacent Woosehill. As a SANG however, it is likely that people would travel from new developments, which are not in the immediate vicinity. Therefore a carpark is required. Natural England have not objected to the car park proposed in this application.
- (b) It should be possible to complete a circular walk of 2.3-2.5km around the SANGS.
- There is no concern in this regard.
- (c) Car parks must be easily and safely accessible by car and should be clearly sign posted.
- There is no concern in this regard.
- (d) The accessibility of the site must include access points appropriate for the particular visitor use the SANG is intended to cater for.
- (e) The SANGS must have a safe route of access on foot from the nearest car park and/or footpaths.

- There is no concern in this regard.
- (f) All SANGS with car parks must have a circular walk which starts and finishes at the car park.
- There is no concern in this regard.
- (g) SANGS must be designed so that they are perceived to be safe by users; they must not have tree and scrub cover along parts of the walking routes.
- Concern has been raised by the Green Infrastructure Officer under the previous application regarding the density of the woodland and the perception of safety because of the density of trees on the site.
- (h) Paths must be easily used and well maintained but most should remain unsurfaced to avoid the site becoming urban in feel.
- While paths would be surfaced in this instance, it is not considered that this will result in an urban feel to the woodland, which is relatively dense and the surfaced footpaths are similar to other SANG in the Borough.
- (i) SANGS must be perceived as semi-natural spaces with little intrusion of artificial structures, except in the immediate vicinity of car parks. Visually-sensitive way markers and some benches are acceptable.
- The route is quite close to adjacent houses in parts, however this is considered acceptable.
- (j) All SANGS larger than 12ha must aim to provide a variety of habitats for users to experience.
- The proposal would largely incorporate woodland. However, as this is not an agricultural site and consists of ancient woodland, it is not considered that it would be appropriate to begin changing the nature of the site and therefore this is acceptable. The site would be pleasant to experience as it is.
- (k) Access within the SANGS must be largely unrestricted with plenty of space provided where it is possible for dogs to exercise freely and safely off lead.
- There is no concern regarding this.
- (l) SANGS must be free from unpleasant intrusions (e.g. sewage treatment works smells etc.).
- The Environmental Health Officer has not objected to the scheme in relation to noise from the adjacent scrap yard. The Green infrastructure officer has recommended measures to restrict noise from the adjacent scrapyards but as has been discussed it is considered that the proposal is acceptable without these. As has been discussed, this is not considered harmful from the standpoint of using this land as public recreational space.
- (m) SANGS should be clearly sign-posted or advertised in some way

- Details have not been submitted regarding this.
 - (n) SANGS should have leaflets and/or websites advertising their location to potential users. It would be desirable for leaflets to be distributed to new homes in the area and be made available at entrance points and car parks.
 - Details have not been submitted regarding this.
63. The Council is not convinced that there is currently a low level of use of the land, which could affect the capacity of any SANG. The Natural England Guidelines accept that if there is a low level of use, then mitigation capacity can be determined as per the normal standards. Evidence of greater than low level use will need to be discounted from the mitigation capacity. Natural England Guidelines indicate that SANG needs to be managed in perpetuity. It is not clear what mechanism the applicant would seek to ensure this at this time.

PLANNING BALANCE AND CONCLUSION

64. The proposal would lead to the creation of an area of informal recreational land in an existing woodland. A number of issues have been raised by local residents, largely regarding the lack of need for formalisation of the woodland, access issues for horses and cycles and impacts on ecology and neighbouring amenity. A number of other comments have been received in support of the application. The proposal would not harm the character of the area, neighbouring amenity or highway safety. It would not lead to increased risk of flooding and would formally open up an area to a wider range of people, for example by providing formalised and surfaced paths for people with mobility issues. A number of different responses have been received regarding potential impacts on the Ancient Woodland. It is not considered that the scheme would result in harm by virtue of the negligible impacts on trees (which are also protected by TPO) and this is supported by the Council's own officers. A legal agreement will be used to secure a biodiversity net gain on the site, including the significant removal and management of invasive species.
65. On balance, it is considered that the scheme would result in a positive impact on the site, especially regarding the provision of open space for local people and the removal of invasive species and it is therefore recommended for approval.

The Public Sector Equality Duty (Equality Act 2010)
<p><i>In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief.</i></p> <p><i>The relevant protected characteristics in this case are age and disability. Specific issues have been discussed relating to accessibility of the site to people with mobility issues – which may relate to either protected characteristic. The proposed footpaths are considered to improve access for people with mobility issues as they will create a firmer and more reliable surface upon which to walk and on which to wheel mobility vehicles. It is noted that the proposed kissing gates are 1.5 metres wide and the Councils Public Rights of Way Officer has indicated that this will not cater for all powered mobility vehicles. However, the gates do accord with the British Standard and would accommodate the</i></p>

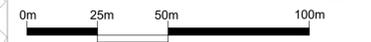
majority of peoples with disability save for some powered vehicles. The Council has had due regard to this and it is considered that this accommodates the majority of users with these protected characteristics and would not result in discrimination. Equality would be advanced between those with the protected characteristics and those who do not. Additionally, good relations would be fostered by providing this broad range of access.

It is noted that residents' comments have raised the opinion that the majority of horse riders are female, and the majority of cyclists are male. By excluding cyclists and horse riders from the site, this results in discrimination against both females and males – being the dominant groups within each sport. This is unsubstantiated and nothing has been forwarded to suggest that this is based in fact. It is therefore not considered that this results in discrimination to or fails to advance equality between males and females in the context of them being protected characteristics.

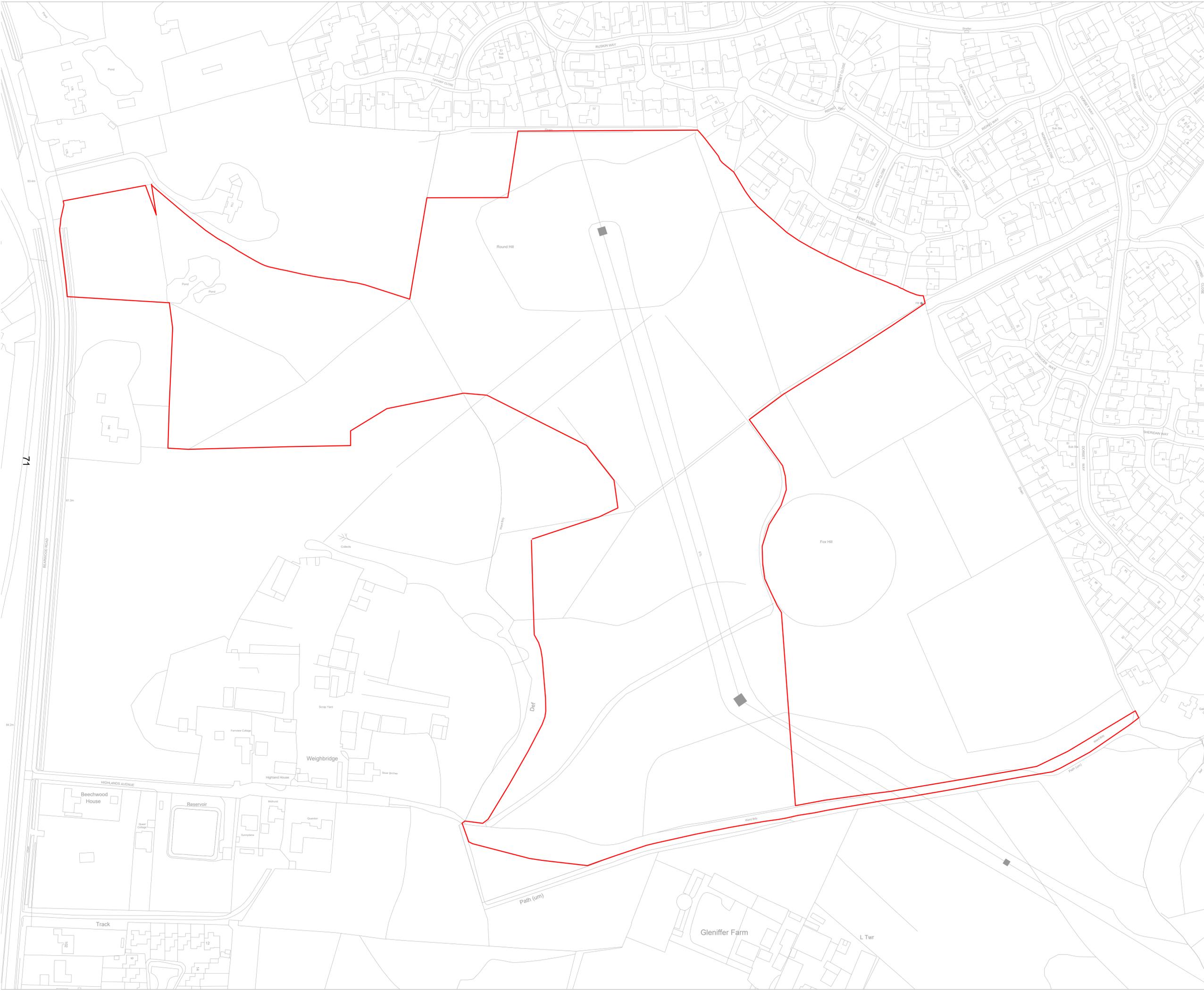
Aside from the above, there is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development

NOTES:
 Based upon the Ordnance Survey map with permission of The Controller of Her Majesty's Stationery Office. © Crown Copyright.
 Aspect Landscape Planning Ltd, West Court, Hardwick Business Park, Noral Way, Banbury OX16 2AF.
 Licence 100045345
 Copyright reserved

Scale 1:1250 @ A1



KEY:
 Site Boundary



REV	DATE	NOTE	Drawn/Chk'd

aspect landscape planning

TITLE
**Bearwood SANG, Wokingham
 Site Location Plan**

CLIENT
Pike wood SANG Limited

SCALE	DATE	DRAWN	CHKD
1:1250@A1	DEC 2020	BS	CJ
DRAWING NUMBER	REVISION		
6768.ASP2			

This page is intentionally left blank

- KEY:
- Site Boundary
 - Proposed Trees
 - Proposed Hedge
 - Proposed Wildflower EW1 Woodland Mixture
 - Proposed 2m wide, minimum 2.3km length Hoggin Footpath. Existing Footpath Links to be Utilised where Appropriate (for details see drawing 6768/ASP.HWD)
 - Proposed Rumble Strips in Hoggin Path
 - Existing Informal Footpaths
 - Public Right of Way WOKI FP11
 - Proposed Timber Post and Rail Stock Proof Fence Perimeter Fencing to be Proposed and Upgraded (specific boundaries annotated on plan)
 - Proposed 2m Composite Material Footbridge. Short Span Footbridge with 3 Post and Rail balustrade and non-slip strips. Exact Length/Span of Bridge to be Determined on Site.



Proposed Debris Dams (locations on plan are indicative and subject to agreement with WBC)

Proposed Bench. Hatton Rustic 4 Slat Seat. BX17 4055. Size 2000mm x 580mm Mounted onto 3m x 2m Hoggin Hard Standing Area

Proposed Derby Standard Litter Bin Moss Green. BX45G 2550.

Wayfinding Signage - 800mm high timber post with chamfered top and coloured arrows to aid wayfinding and illustrating alternative walking routes

Proposed Information Boards (to be specified by others)

Proposed Maintenance Access Gate (for details please drawing No: 6768/ASP.HWD)

Proposed Pedestrian Style Access Gate (for details please drawing No: 6768/ASP.HWD)

Proposed Log Bike Rack (or similar)

10m Offset Either Side of Proposed SANG Route. Exact Route to be Determined by Arboricultural Survey and Existing Vegetation Clearance.

Existing Watercourse

Contours

BMX Track

Electrical Pylons

Existing Pond

Note:
 For Arboricultural details regarding the existing trees 10m either side of the proposed SANG footpath, please refer to Aspect Arboriculture Survey.

For Ecology details please refer to EPR information.

For Access and car park details please refer to i-Transport information.

FOR PLANT SCHEDULE REFERENCE TO DRAWING 6768.PSCP.03

REV	DATE	NOTE	BS	CJ
F	19.04.2021	Additional hoggin surface proposed as per LPA comments 2 - internal routes and southern PFRoW	BS	CJ
E	09.12.2020	Updated car park layout added	BS	CJ
D	02.12.2020	Updated to LPA comments 18/11/2020 / team comments	BS	CJ
C	13.11.2020	Updated to LPA comments 9/11/2020	BS	CJ
B	03.11.2020	Updated to team comments	BS	CJ
A	28.10.2020	Updated to team comments	BS	CJ

For Entrance and Car Park Area Details, refer to Drawing 6768.PSCP.03

All footpath construction to be DDA compliant and where gradients on the ground are beyond maximum slopes for wheelchair access the ground shall be made up to allow for new shallower gradient to be achieved.

Existing close board fence & chain link fencing along northern boundary associated with rear of properties to be retained

New timber Post & Rail fence proposed around car park area.

Existing northern boundary treatment to remain with the addition of sections of upgraded fencing as required to restrict private access into the SANG, as requested by WBC. Exact locations and extent to be confirmed on site.

Slightly raised Rumble Strips approximately 10m from access points into the SANG. This will denote approach to an access point to either put a dog on a lead & general way marking.

New timber post & rail fence and Mixed Native Hedge proposed to define south western & western SANG boundary & to separate BMX track

No access to BMX track area from SANG

Footbridge with balustrade. 2m width, exact length and span to be determined on site.

Existing clearing to provide opportunities for picnics.

Approximate route of 20m wide corridor along footpath route to be cleared of Rhododendron species on a phased programme.

Existing open eastern boundary retained for access to adjacent WBC land

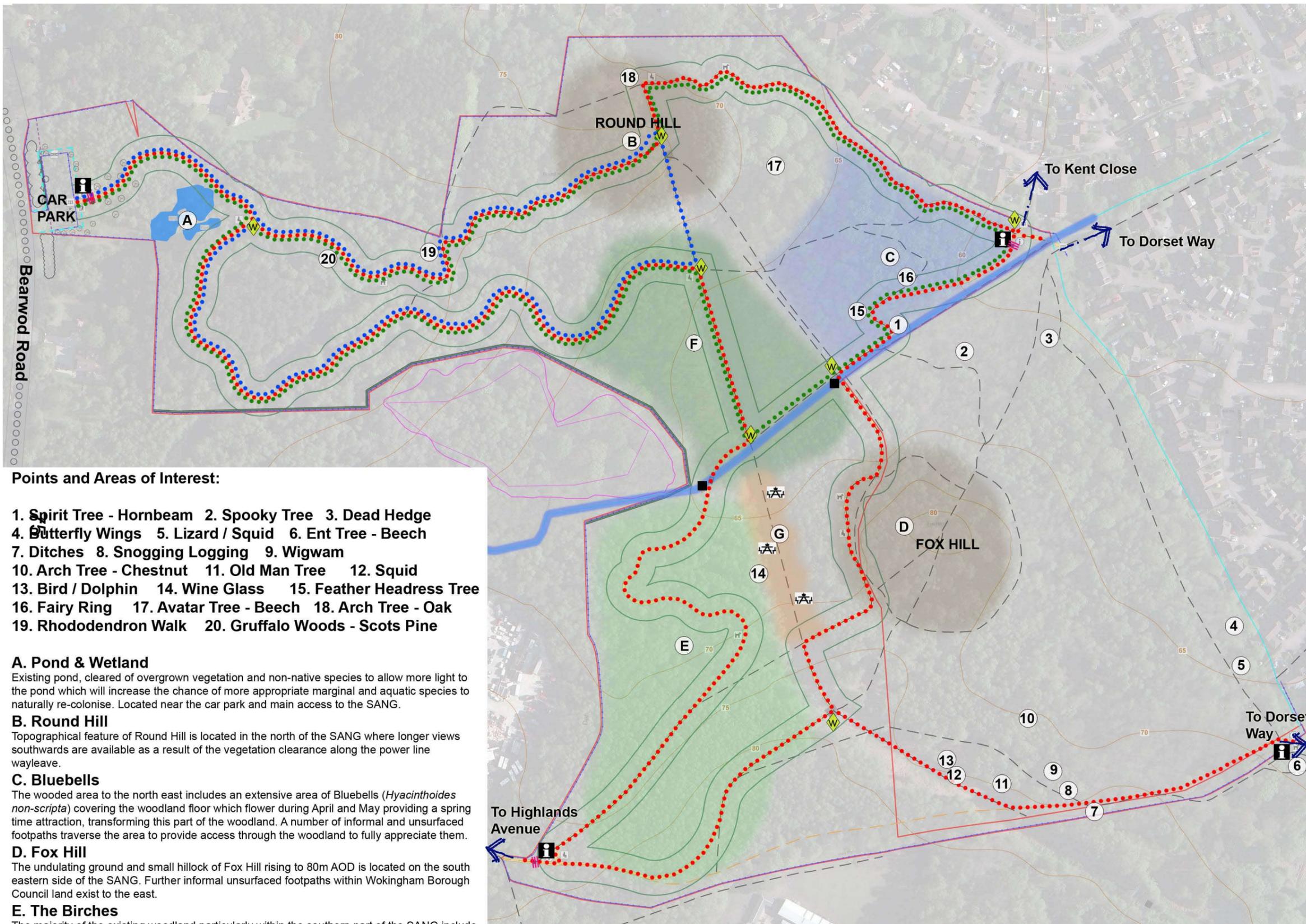
Existing fence along southern side of path retained

Existing footpaths provide pedestrian access link to the existing public right of way.



Site Aerial View (not to scale)

This page is intentionally left blank



NOTES:
 Based upon the Ordnance Survey map with permission of The Controller of Her Majesty's Stationery Office, © Crown Copyright.
 Aspect Landscape Planning Ltd, West Court, Hardwick Business Park, Noral Way, Banbury OX16 2AF.
 Licence 100045345
 Copyright reserved

- Key:**
- SANG Site Boundary
 - Car Park
 - Watercourse
 - Ponds
 - Existing Unsurfaced Paths
 - Pedestrian Access Points
 - Information Board Locations with Map & Walking Routes
 - Wayfinding Signage Location - Timber Post with Colour Coded Arrows for Walking Routes
 - Debris Dam & Footbridge over Watercourse
 - Picnic Tables
 - Rumble Strip to Indicate User is Approaching SANG Entrance/Exit
- Walking Routes:**
- Red Long Route (Approx. 2.3km)
 - Green Medium Route (Approx. 1.5km)
 - Blue Short Route (Approx. 1km)

Points and Areas of Interest:

1. Spirit Tree - Hornbeam
2. Spooky Tree
3. Dead Hedge
4. Butterfly Wings
5. Lizard / Squid
6. Ent Tree - Beech
7. Ditches
8. Snogging Logging
9. Wigwam
10. Arch Tree - Chestnut
11. Old Man Tree
12. Squid
13. Bird / Dolphin
14. Wine Glass
15. Feather Headress Tree
16. Fairy Ring
17. Avatar Tree - Beech
18. Arch Tree - Oak
19. Rhododendron Walk
20. Gruffalo Woods - Scots Pine

- A. Pond & Wetland**
 Existing pond, cleared of overgrown vegetation and non-native species to allow more light to the pond which will increase the chance of more appropriate marginal and aquatic species to naturally re-colonise. Located near the car park and main access to the SANG.
- B. Round Hill**
 Topographical feature of Round Hill is located in the north of the SANG where longer views southwards are available as a result of the vegetation clearance along the power line wayleave.
- C. Bluebells**
 The wooded area to the north east includes an extensive area of Bluebells (*Hyacinthoides non-scripta*) covering the woodland floor which flower during April and May providing a spring time attraction, transforming this part of the woodland. A number of informal and unsurfaced footpaths traverse the area to provide access through the woodland to fully appreciate them.
- D. Fox Hill**
 The undulating ground and small hillock of Fox Hill rising to 80m AOD is located on the south eastern side of the SANG. Further informal unsurfaced footpaths within Wokingham Borough Council land exist to the east.
- E. The Birches**
 The majority of the existing woodland particularly within the southern part of the SANG include the naturally colonising pioneer species Silver Birch and Downy Birch trees which favour acidic soils. Birch woodlands support a diverse invertebrate fauna and fungal flora and provides an important habitat for birds.
- F. Sweet Chestnut Wood**
 This area of the wood is dominated by the deciduous Sweet Chestnut tree (*Castanea sativa*) known for its roasted Christmas treat. Whilst not a UK native species, the Sweet Chestnut was thought to have been introduced to the British Isles by the Romans and is commonly found in woodlands across the country. The flowers produced provide an important source of nectar and pollen for bees a other insects and squirrels eat the nuts. Historically this area of Sweet Chestnuts have previously been coppiced which is noticeable today.
- G. Recreational Picnic Area**
 Space in the centre of the SANG includes a number of picnic tables and more open area.

REV	DATE	NOTE	DRAWN	CHK'D
REVISIONS				



TITLE
 Bearwood SANG, Wokingham
 Wayfinding Strategy

CLIENT
 Pike Wood SANG Limited

SCALE	DATE	DRAWN	CHK'D
1:2,500 @ A3	NOV 2020	BS	CJ
DRAWING NUMBER		REVISION	
6768 / SK001 / WS			



Sweet Chestnut Wood



Example SANG Information Board at Entrances



Bluebell Walk



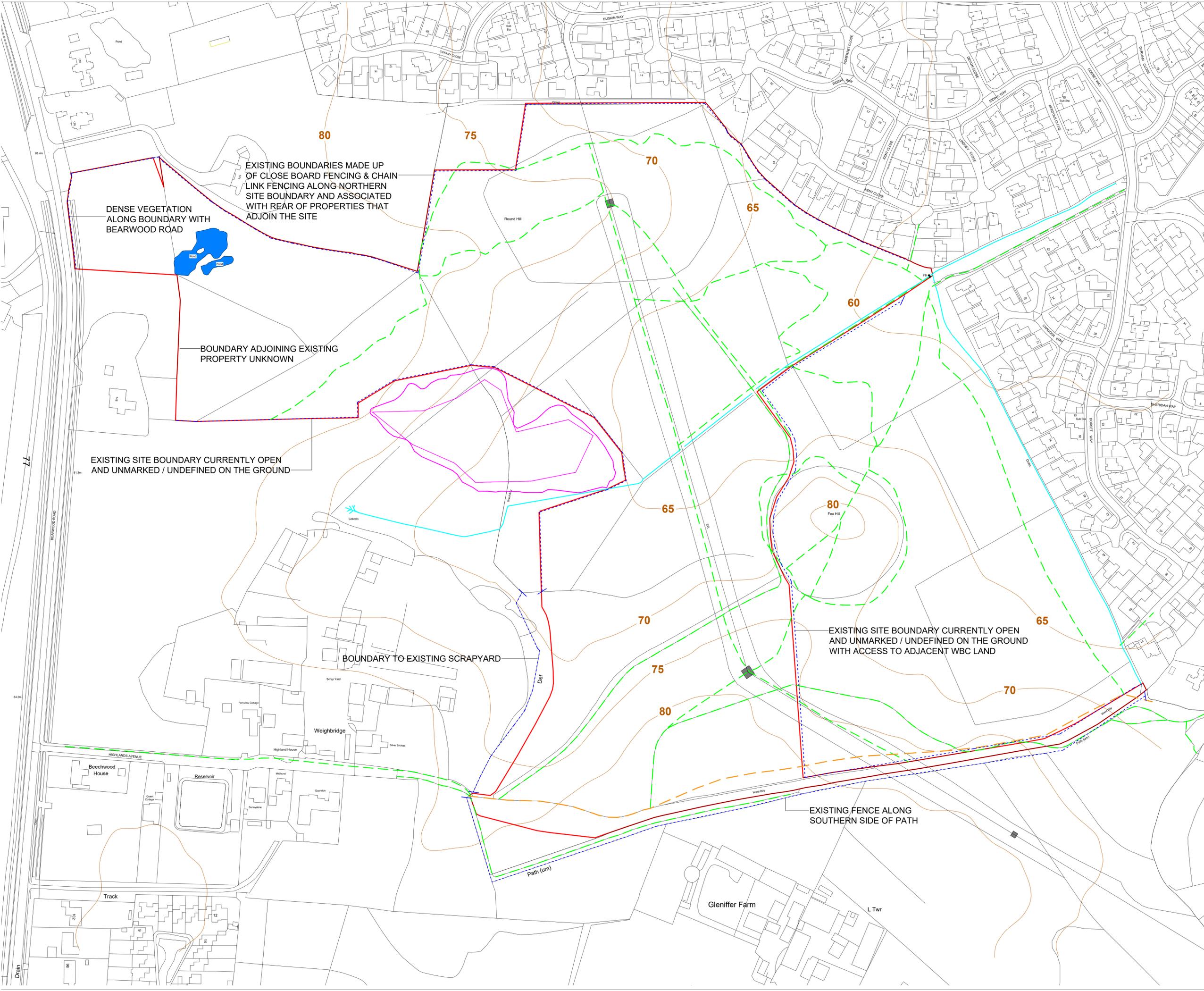
Wayfinding Timber Post Sign



Watercourse Debris Dam

This page is intentionally left blank

- KEY:
- - - Site Boundary
 - Contours
 - Existing Watercourse
 - Existing Informal Footpaths
 - Public Right of Way WOKI FP11
 - BMX Track
 - Electrical Pylons
 - Existing Pond
 - Existing Boundary Treatments (specific boundaries annotated on plan)



C	02.12.2020	Redline amended	BS	CJ
B	03.11.2020	Updated to team comments	BS	CJ
A	28.10.2020	Boundary updated to omit BMX track	BS	CJ
REV	DATE	NOTE	Drawn	CHK'd

aspect landscape planning

TITLE
 Bearwood SANG, Wokingham
 Existing Site Plan

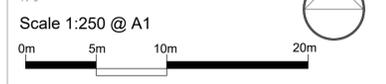
CLIENT
 Pike wood SANG Limited

SCALE	DATE	DRAWN	CHKD
1:1250@A1	AUG 2020	SB	BS
DRAWING NUMBER	REVISION		
6768.ES.01	C		

This page is intentionally left blank

83.4m

NOTES:
Based upon the Ordnance Survey map with permission of The Controller of Her Majesty's Stationery Office. © Crown Copyright.
Aspect Landscape Planning Ltd, West Court, Hardwick Business Park, Noral Way, Banbury OX16 2AF.
Licence 100045345
Copyright reserved



- KEY:
- Site Boundary
 - Proposed Trees
 - Proposed Hedge
 - Proposed Wildflower EW1 Woodland Mixture
 - Proposed 2m wide, minimum 2.3km length Hoggin Footpath. Existing Footpath Links to be Utilised where Appropriate (for details see drawing 6768/ASP.HWD)
 - Proposed Rumble Strips in Hoggin Path
 - Car Park- Cellular Fill on Hardcore Base (refer to i-Transport information for details)
 - Approximate area of vegetation removed for car park and access construction
 - Kerb Line Defining Edge of Car Park
 - Proposed Timber Post and Rail Stock Proof Fence
 - Perimeter Fencing to be Proposed and Upgraded (specific boundaries annotated on plan)

- Proposed Bench. Hatton Rustic 4 Slat Seat. BX17 4055. Size 2000mm x 580mm Mounted onto 3m x 2m Hoggin Hard Standing Area



- Proposed Derby Standard Litter Bin Moss Green. BX45G 2550.



- Proposed Dog Waste Bin, 60Litre Red. BX45G 2591-PM



- Wayfinding Signage - 800mm high timber post with chamfered top and coloured arrows to aid wayfinding and illustrating alternative walking routes



- Proposed Information Boards (to be specified by others)
- Proposed Maintenance Access Gate (for details please drawing No: 6768/ASP.HWD)

- Proposed Log Bike Rack (or similar)



- 10m Offset Either Side of Proposed SANG Route. Exact Route to be Determined by Arboricultural Survey and Existing Vegetation Clearance.

- Contours
- Existing Pond

Note:
For Arboricultural details regarding the existing trees 10m either side of the proposed SANG footpath, please refer to Aspect Arboriculture Survey.

For Ecology details please refer to EPR information.

For Access and car park details please refer to i-Transport information.

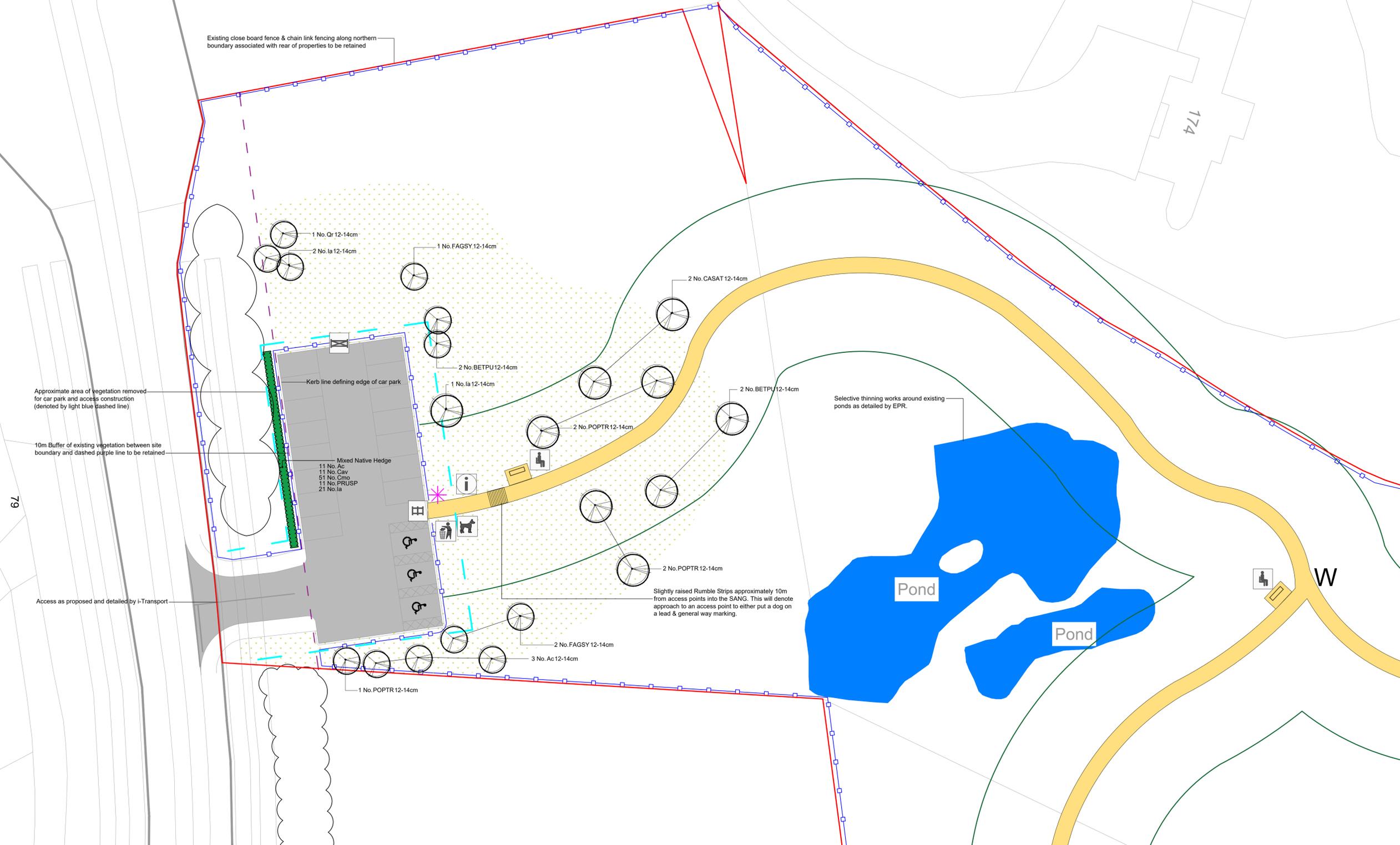
REV	DATE	NOTE	BS	CJ
E	09.12.2020	Updated car park layout added	BS	CJ
D	02.12.2020	Updated to LPA comments 18/11/2020 / team comments	BS	CJ
C	23.11.2020	Updated to LPA comments 18/11/2020	BS	CJ
B	03.11.2020	Updated to team comments	BS	CJ
A	28.10.2020	Updated to team comments	BS	CJ
REV	DATE	NOTE	BS	CJ

aspect landscape planning

TITLE
Bearwood SANG, Wokingham
Proposed SANG Car Park Detail Plan

CLIENT
Pike wood SANG Limited

SCALE	DATE	DRAWN	CHKD
1:250@A1	AUG 2020	BS	CJ
DRAWING NUMBER	REVISION		
6768.PSCP.03	E		



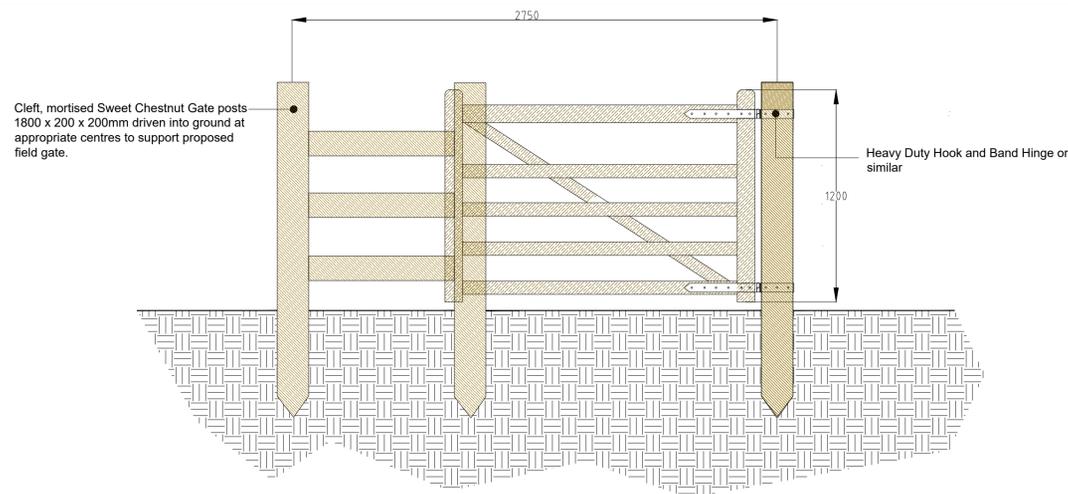
Planting Schedule					
Trees					
Number	Abbreviation	Species	Specification	Girth	Height
3 No.	Ac	Acer campestre	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
4 No.	BETPU	Betula pubescens	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
2 No.	CASAT	Castanea sativa	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
3 No.	FAGSY	Fagus sylvatica	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
3 No.	la	Ilex aquifolium	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
5 No.	POPTR	Populus tremula	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
1 No.	Qr	Quercus robur	Heavy Standard :Clear Stem min. 200 :RB 12-14cm	3.5-4.0m	
Total :21 No.					

Native Hedge Mix					
Number	Abbreviation	Species	Specification	Height	Density %
198 No.	Ac	Acer campestre	BR :Double staggered row	60-80cm	4/m 10%
198 No.	Cav	Corylus avellana	BR :Double staggered row	60-80cm	4/m 10%
985 No.	Cmo	Crataegus monogyna	BR :Double staggered row	60-80cm	4/m 50%
395 No.	la	Ilex aquifolium	C :Leader with laterals : :1+1	60-80cm	4/m 20%
198 No.	PRUSP	Prunus spinosa	BR :Double staggered row	60-80cm	4/m 10%
Total :1974 No.					

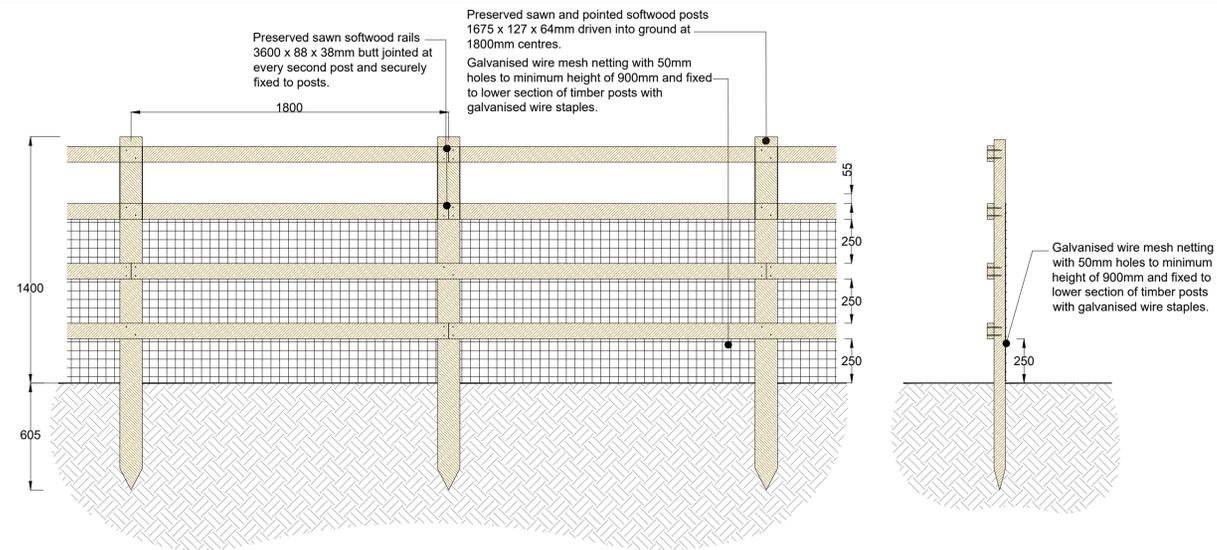
Wildflower Grass Mix
Emorsgate EW1 Woodland Mixture. Sown at 40kg/ha

PLANTING NOTES
All plants to be supplied in accordance with the HTA 'National Plant Specification' and from a HTA certified nursery. All plants and trees to be planted in accordance with BS3938 and BS8545. Delivery and handling of all plant material to be in accordance with BS4428:CLICPSE Code of Practice for 'Handling and Establishing Landscape Plants' Parts I, II and III and BS8545.
Planting Pit and Trench Preparation
Tree pits in soft landscape to be excavated to 1mx1mx1m depth prior to topsoiling and all shrub planting areas excavated to 450mm depth. All proposed hedge planting trenches to be excavated to 600mm depth. Unless otherwise specified, all tree pits in hard landscape to be 2mx2mx1m, backfilled with compacted Urban Tree Soil.
The preparation of planting pits, beds or trenches shall comply with the appropriate British Standards, namely BS4043, BS4428, BS5837 and BS8545.
Excavation of planting pits, beds or trenches shall not take place when the ground is frozen or waterlogged such that damage may occur to the structure of the soil. All excavated areas to be backfilled with either site won topsoil or imported topsoil to be BS3882-General purpose grade. All topsoiled areas to be clear of rocks and rubble larger than 50mm diameter and any other debris that may interfere with the establishment of plants. The Contractor shall break up and cultivate at the base of the trenches or planting pits. The sides of the trenches or planting pits shall be loosened with a fork or other similar implement. All stones and the like over 75mm in any dimension, deleterious matter, weeds and weed roots brought to the surface by any cultivation or excavation shall be removed off site. The Contractor shall remove off site the excavated subsoil material when preparing planting pits. The imported topsoil should make up any deficiencies caused by the removal of the subsoil material. Trenches and pits shall have the topsoil and any subsoil material thoroughly broken up and mixed prior to backfilling.
All trees shall be supplied root balled, unless otherwise stated. Root balled trees shall be well grown, healthy and with a compact, contained rootball. They shall be nursery grown and have been regularly watered. Prior to planting, all plant material shall be stored and sorted at in accordance with best practice.
Planting
All plants shall be planted in a random fashion avoiding formal regimented lines at densities indicated in the schedule, unless otherwise specified. Unless otherwise specified, all hedgerows shall be planted in a double staggered rows and hedgerow mixes shall be planted in groups of 7, 9 & 15 at densities indicated on the schedule. Ornamental shrub planting mixes shall be planted in groups of 5, 7 & 11 and native shrub planting mixes shall be planted in groups of 9, 13 & 15. The selection, procurement, handling, storage and planting operations of all proposed trees shall be in accordance with BS8545:2014 - Trees: from nursery to independence in the landscape, recommendations.
Planting and associated operations shall comply with BS4043, BS4428, BS5837 and BS8545. Unless otherwise stated planting shall be carried out during the period of 1 Nov to 31 March when the ground is not frozen or water logged. If planting is required outside this period agreement shall be sought and all bare root plants shall be substituted with container grown stock.
Provide Treebio Biodegradable Spiral Guards (Green-tech Product code: 160PS1031-PRO) or similar to young sapling trees, whips and feather planting, ensuring that the main or terminal bud is protruding out above the top of the spirals.
Watering
All plants shall be watered in to field capacity immediately after planting and mulched with 50mm depth of medium grade crushed mulch. The Contractor shall water the trees, shrubs and hedges once planted so that the entire tree pit or planted area is moistened to field capacity, i.e. "the amount of water retained by previously saturated soil once full drainage has ceased". Watering to field capacity shall continue frequently and on a regular basis as considered necessary by the landscape contractor and as necessary to ensure the successful establishment and continued thriving of all planting. Additional watering shall be undertaken during summer months and/or periods of drought. Post planting management and maintenance specifically for new tree planting shall include ongoing irrigation and formative pruning as outlined in BS8545. The period over which regular irrigation required for transplanted trees is likely to be at least two full growing seasons to ensure successful establishment. As the root system develops the frequency of irrigation can be reduced.
Staking
All trees within soft landscape areas to be double staked with cross bar and tied, using 1.5m long, 75mm diameter rounded tree stakes 75mm brace, rubber tie and spacer block. Stakes not to extend more than 650mm above ground level. All trees within hardstanding/highways visibility splays to be clear stem to 1.5m high unless otherwise specified. Trees within hardstanding / specific pits to be underground guyed unless otherwise specified.
Root Barriers
Root barriers (ReFoot 1000 or equivalent) to be included adjacent to buildings and services where necessary. Landscape contractor shall check all planting operations comply with appropriate standards and that in the absence of detailed surveys, any necessary underground investigations are undertaken to ensure there are no conflicts with existing or proposed utilities, services or foundations.
PROTECTION OF EXISTING VEGETATION TO BE RETAINED
Existing trees to be retained shall be protected in accordance with BS8837, from commencement to completion of all works on plan.
N.B. All planting proposals including tree planting have been developed in order to create a high quality environment and gain planning consent for the development. All tree species have been reviewed in line with NHC guidance (2017) and in the absence of any building foundation depths or detailed soil analysis information for the site. Where possible only low and moderate water demand species are proposed in close proximity to new buildings. A number of varied cultivars of these species as well as ornamental species that have a smaller overall mature height (which are not currently assessed within NHC guidance 2017) are proposed to provide variety in the scheme and engineer's should consider these locations & species. Where necessary new building foundation depths shall be designed to accommodate the approved tree species, site specific soil shrinkage and tree water demand in line with NHC standards 2017 (Chapter 4.2 - Building Near Trees). Planting plans have been prepared for planning purposes and in the absence of fully detailed ground investigations, geological or hydrological surveys and planting design or species choice may be subject to change - suitability should be confirmed on site by the landscape contractor. Detailed site specific soil analysis and suitable site drainage should be checked by landscape contractor to ensure planting can be implemented in accordance with approved drawings prior to implementation.

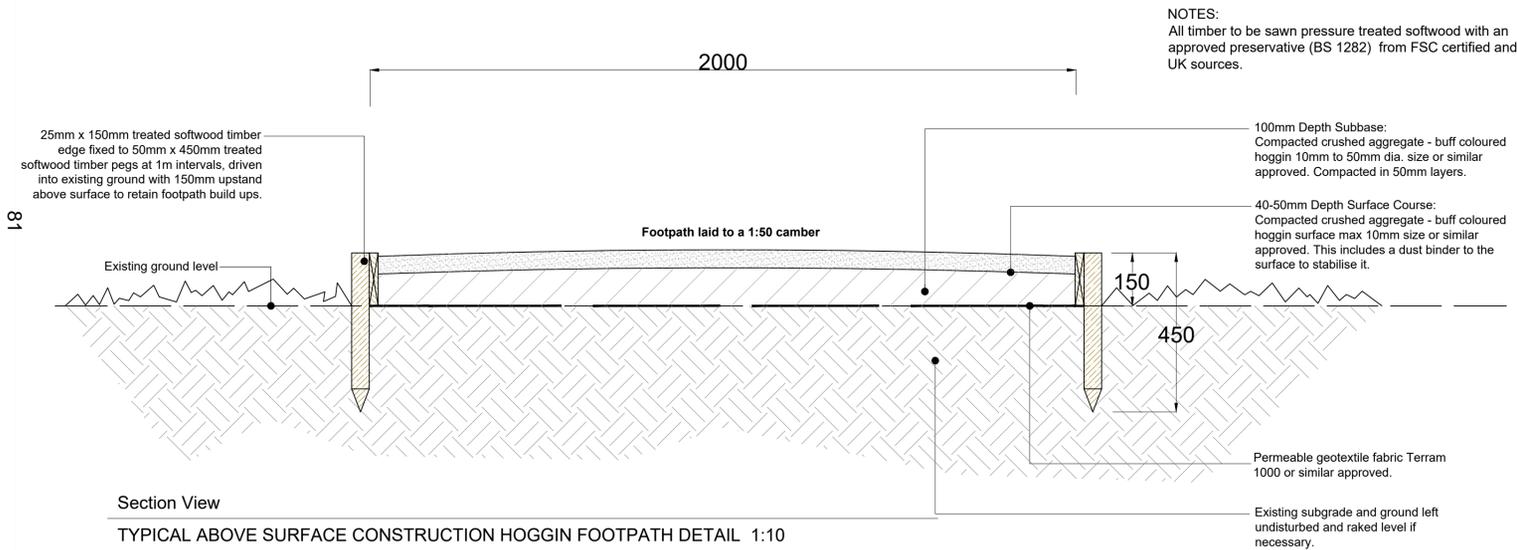
This page is intentionally left blank



Part Elevation
WOODEN OXFORD - MEDIUM MOBILITY KISSING GATE FOR PEDESTRIAN ACCESS POINTS 1:20



Part Elevation
POST AND RAIL TIMBER FENCE DETAIL WITH WIRE MESH NETTING 1:20



NOTES:
All timber to be sawn pressure treated softwood with an approved preservative (BS 1282) from FSC certified and UK sources.

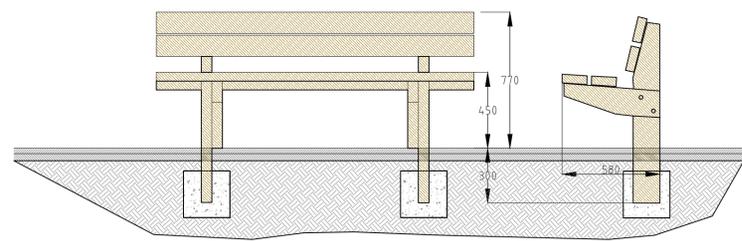
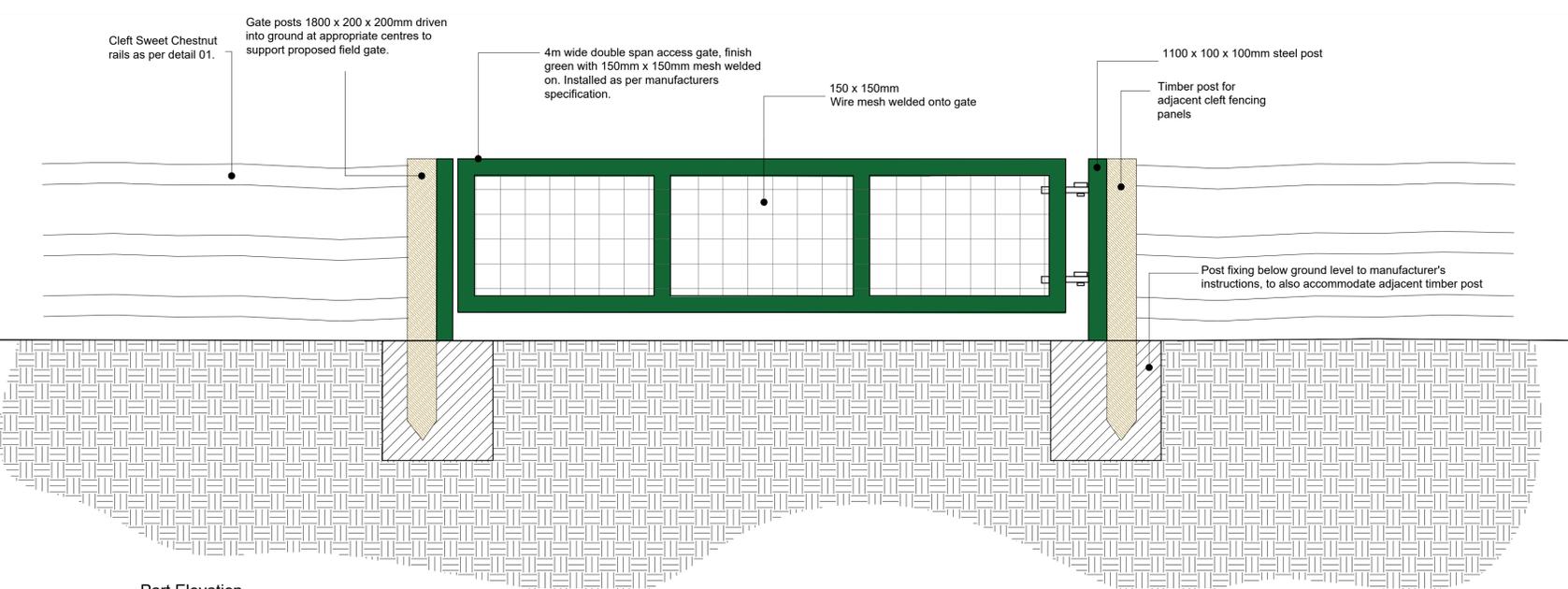
N.B.
EXACT ROUTE OF FOOTPATH TO BE DETERMINED ON SITE. FOOTPATH CONSTRUCTION TO BE ENTIRELY ABOVE SURFACE CONSTRUCTION & DDA COMPLIANT.

GRADIENTS & CORNERS OF PROPOSED FOOTPATH ROUTE TO BE REVIEWED & DETERMINED ON SITE.

WHERE GRADIENTS ON THE GROUND ARE BEYOND MAXIMUM SLOPES FOR WHEELCHAIR USE, THE GROUND SHALL BE MADE UP TO ALLOW FOR SHALLOWER GRADIENT TO BE ACHIEVED.

ANY SHARP CORNERS OF FOOTPATH ROUTE SHOULD BE AVOIDED & A GENTLE CURVE CREATED

Section View
TYPICAL ABOVE SURFACE CONSTRUCTION HOGGIN FOOTPATH DETAIL 1:10



Elevation View
HATTON RUSTIC 4 SLAT TIMBER SEAT 1:20

Part Elevation
DRB PS1 4M SPAN GATE 1:20 (proposed for car park entrance)

B	2020.11.13	Updated to LPA comments 9/11/2020	BS	CJ
A	2020.11.03	Ashphalt car park surface detail removed	BS	CJ
REV	DATE	NOTE	Drawn	Chk'd

aspect landscape planning

TITLE
Land at Bearwood Road, Wokingham
Hard Works Details

CLIENT
Pike wood SANG Limited

SCALE	DATE	DRAWN	CHK'D
As per drawing	AUG 2020	BS	CJ
DRAWING NUMBER	REVISION		
6768/ASP.HWD	B		

This page is intentionally left blank

PLANNING REF : 203539
PROPERTY ADDRESS : Arborfield Green Community Centre
: Arborfield, Reading
: RG2 9ND
SUBMITTED BY : Barkham Parish Council
DATE SUBMITTED : 11/02/2021

COMMENTS:

Although Barkham Parish Council (BPC) is aware that many residents have responded favourably to this planning application but by no means all BPC feels there are many aspects of the proposal that are not appropriate and objects for the following reasons:

1.Ancient Woodland

Section 6.69 of the Planning Statement states:

As set out within the submitted Outline Landscape and Ecology Management Plan, the woodland present is not considered to be Ancient Woodland.

Fox Hill is listed in Natural Englands Ancient Woodland Inventory under ID Number 1505144 where it is designated as Ancient and SemiNatural Woods and described as Ancient Woodland. The three surrounding areas are also categorised in this way under ID Numbers 1505134, 1505142 and 1505143.

It is NOT appropriate for someone contracted by the applicant to unilaterally downgrade this designation in this way. Every attempt should be made to nurture and preserve the area.

Section 175 (c) of the National Planning Policy Framework (2019) states:

Development resulting in the loss or deterioration of irreplaceable habitats (such as ancient woodlands and ancient or veteran trees) should be refused, unless there are wholly exceptional reasons and a suitable compensation strategy exists.

As demonstrated by the applicant and discussed in Section 3 below, there is no demand for this additional SANG capacity so there can be no wholly exceptional reasons.

2.Biodiversity

From many comments made by the residents, it seems they have been enjoying this area for very many years and this is reflected in the vibrant Friends of Fox Hill website.

The wildlife includes roaming deer and badgers. The latter are protected by law and it is believed there is a set in the vicinity which would be disturbed by this application. It is perverse that the proposal seeks to convert this NATURAL woodland into a SEMIFORMAL COUNTRY PARK which will have a detrimental effect on habitats and biodiversity. The provision of car parking and other facilities would further detract from the natural ambience. Most country and nature lovers would prefer this location to be retained as a natural area especially when there is no need for additional SANG capacity at this time.

3.Demand for Additional SANG

Capacity

Section 1.3 of the Planning Statement states:

At present, new residential developments in the local area which necessitate the proposed SANG are not identified; as such, the Site will function as a country park until it is required as a SANG.

Section 4.12 goes on to state:

The site has the potential to provide sufficient SANG capacity to facilitate approximately 769 dwellings.. for development falling within the 400m to 5km catchment (area of the TBHSPA).

BPC is unaware of any proposed developments in the area that would require this additional SANG capacity and the ongoing large development sites have integrated SANGs. The Local Plan Update is now not expected to be resubmitted for public consultation until the end of 2021. There is NO demand for additional SANG capacity so this application is therefore premature.

4.Sustainability

The longterm sustainability of these proposals must be questioned. There appears to be no ongoing business plan as to how the operation of this country park will be managed and funded to facilitate maintenance of the promised facilities and undertake the woodland management promised.

5.Pollution

BPC does not believe that the question of ground contamination has been satisfactorily addressed. The proposed site is adjacent to an industrial complex and potential for runoff is a particular concern both historically and in the future.

The continuing noise problem from the A1 industrial site is not conducive to maintaining the peace and tranquillity of the area and, hence, its suitability as a SANG. The owners of the A1 site were served an abatement order by WBC for statutory nuisance in April 2020 and we understand that 12 weeks has elapsed since compliance was due. This application should not be considered further until the work has been done and shown to be effective.

6.Archaeology

Locals have reported the presence of an early settlement in the area and a feature SW of Fox Hill can be seen on a Lidar image of the area. Presence of an Iron Age Site is confirmed on the AR CHI UK site referenced below. Consideration of the archaeology of the area must be included in any such application.

7.Greenway

Were the application to be approved, the current route of Footpath 1 which runs through the proposed SANG boundary should be preserved. If this becomes the chosen route for the proposed Arborfield Cross to Wokingham Greenway, then these proposals should not be allowed to have any negative impact.

BPC understands that the site is now for sale under the agency of Simmons and Sons. This throws into further doubt many of the points raised above as there is no guarantee any future owner will assume the required undertakings.

Please refuse this planning application.

Reference:

<https://www.archiuk.com/cgi-bin/archi/newsearchengine.pl?searchlocation=SU%2079%2068&searchtype=architownsearch&pwd=freesearch@freesearch.com&TownName=WOKINGHAM&county=Wokingham&searchrange=10000&period=&fontsize=&placename=Woose%20Hill&info2search4=architownsearch&keywords=ironageceltic>

This page is intentionally left blank

PLANNING REF : 203539
PROPERTY ADDRESS : Town Hall Market Place
: Wokingham
: RG40 1AS
SUBMITTED BY : The Wokingham Town Council P&T Committee
DATE SUBMITTED : 05/02/2021

COMMENTS:

The Committee would like to see this area remain as a green space.

The changes to the Car Park are welcomed however there are several concerns regarding this application.

?Footpath should be 11m away from neighbouring house gardens for privacy.

?Concerns regarding fire service access.

?To be sustainable this needs to have a maintenance plan.

?Concerns regarding noise from the A1 business premises.

?Should be in keeping with TB21 Landscape character.

This page is intentionally left blank

Agenda Item 6.

Application Number	Expiry Date	Parish	Ward
202065	11 June 2021	Wokingham	Emmbrook;

Applicant	McCarthy & Stone
Site Address	54 – 58 Reading Road, Wokingham, RG41 1EH
Proposal	Full application for the proposed erection of 34 no. retirement living apartments including communal facilities and associated car parking and landscaping, following demolition of existing 3 no. dwellings.
Type	Full
PS Category	1
Officer	Graham Vaughan
Reason for determination by committee	Major application

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 June 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The site is located within Wokingham and, in policy terms, within a major development location. Currently the site is occupied by three dwellings with associated gardens and parking. An area of overgrown grassland exists to the rear which adjoins the Wokingham train station car park. Mature trees exist on and around the site which help filter views.</p> <p>The scheme is for the erection a three-storey apartment building for the elderly as part of a standard product by the applicant, McCarthy & Stone. It would have a large, single footprint but its form would be broken up using architectural features. Access would be provided by an entrance and exit access to Reading Road, and this would link to a parking area to the rear of the site. Additionally, the area around the building would be grassed with some areas of patio for ground floor flats. A garden area is proposed adjacent to the car park.</p> <p>In terms of the principle of development, the scheme would provide for a recognised need with regards to housing for a particular group. It is noted that this can also help to ‘free-up’ existing housing stock. As noted above, the scheme would have a large single footprint, and this results in a significant increase in mass and built form on the site. However, when taking into context the surrounding area, this is not considered to be harmful. It is noted that there would be some harm to the setting of the adjacent listed building; St. Paul’s Church, but this would be ‘less than substantial’. Indeed, in other respects including residential amenity, highways, drainage, environmental health, trees and landscape, the scheme would have a positive impact. For viability reasons, the affordable housing contribution is reduced but this is considered a minor negative impact.</p> <p>Overall, the positive impacts of the scheme are considered to outweigh the minor negative impacts and therefore, subject to the conditions set out below and the signing of a legal agreement, the scheme is recommended for approval.</p>

PLANNING STATUS

Water Utility Consultation Zones
Contaminated Land Consultation Zone
Green Routes and Riverside Paths
Heathrow Aerodrome Consultation Zone
Affordable Housing Thresholds
Bat Roost Habitat Suitability
Borough Parishes
Scale and Location of Development Proposals
Site Allocations
District Town and Local Centres
Major Transport Schemes
Public Open Space
Tree Preservation Orders
Borough Wards
Sites of Urban Landscape Value
Landscape Character Assessment Area
Local Plan Update Submitted Sites
SSSI Impact Risk Zones
Thames Basin Heaths SPA Mitigation Zones
Green Routes and Riverside Paths Consultation Zone

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A. Completion of a legal agreement to secure an off-site contribution towards the provision of affordable housing and either a contribution towards the Council's employment skills plan or the provision of a site-specific employment skills plan.

If the Agreement is not completed within 3 months of the date of this resolution, Planning Permission will be refused unless the Operational Manager for Development Management in consultation with the Chairman of the Planning Committee agree to a later date.

B. Conditions and informatives:

Conditions

1. Timescale - The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details - This permission is in respect of the submitted application plans and drawings numbered '19 - 2923 - LP01', '19 - J2923 - 200 Rev B', '19 - J2923 - 201 Rev B', '19 - J2923 - 202 Rev B', '19 - 2923 - 203.1 Rev C', '19 - 2923 - 203 Rev C', '19 - 2923 - 204 Rev B', '19 - 2923 - 204.1 Rev B', '19 - 2923 - 205 Rev A', '19 - 2923 - 206 Rev A', '19 - J2923 - 207', '19-2923-208 Rev B', '01 Site Survey', 'PP/4001/Wokingham/F1' and 'PP/4001/Wokingham/F1' received by the local planning authority on 7 September 2020. The development shall be carried out in accordance

with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.
Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. External materials - Prior to the commencement of above ground works, samples and details of the materials to be used in the construction of the external surfaces of the building/s shall have first been submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the so-approved details.

Reason: To ensure that the external appearance of the building is satisfactory. Relevant policy: Core Strategy policies CP1 and CP3.

4. Archaeological Potential - Prior to the commencement of development, other than demolition to ground level, the applicant, their agents or successors in title, shall implement a programme of field evaluation in accordance with a written scheme of investigation (WSI) which has been submitted to and approved in writing by the local planning authority. The results of the evaluation shall inform the preparation of a mitigation strategy which will be submitted to and approved in writing by the local planning authority. The mitigation strategy shall provide for:

a) a programme of site investigation and recording, or alternative mitigation, within any areas of archaeological interest identified. Development will not commence within the area of archaeological interest until the site investigation has been satisfactorily completed; and,

b) a programme of post-investigation assessment, analysis, publication, dissemination and archiving. This part of the condition shall not be discharged until these elements of the programme have been fulfilled in accordance with the programme set out in the WSI.

Reason: The site lies in an area of potential for medieval remains. The potential impacts can be mitigated by a programme of archaeological work to record and advance our understanding of any heritage assets to be lost in accordance with national and local planning policy. Relevant policy: National Planning Policy Framework Section 16 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB25.

5. Bat Licence - No works, including demolition of buildings or removal of standing deadwood trees, shall commence until a licence for development works affecting bats has been obtained from the Statutory Nature Conservation Organisation (Natural England) and a copy (or an email from Natural England that the site has been registered under the bat mitigation class licence) has been submitted to the local planning authority. Thereafter mitigation measures approved in the licence shall be maintained in accordance with the approved details. Should conditions at the site for bats change and / or the applicant conclude that a licence for development works affecting bats is not required the applicant is to submit a report to the council detailing the reasons for this assessment along with any remaining mitigation and enhancement strategy and this report is to be approved in writing by the local planning authority prior to commencement of works.

Reason: To ensure that bats, a protected species and material consideration, are not adversely affected by the development. Relevant policy: Core Strategy policy CP7 and MDD Local Plan policy TB23.

6. Prior to commencement of the development a detailed Biodiversity Net Gain Strategy shall be submitted to and approved in writing by the local planning authority. The Strategy shall include:

- (i) a biodiversity net gain calculator using the latest Defra metric and based on the detailed soft and hard landscape plans; and
- (ii) details of any off-site offset scheme required to achieve a minimum 10% net gain over the baseline in all categories of the calculator.

The approved strategy be implemented in full in the course of the development unless otherwise approved in writing by the local planning authority.

Reason: To ensure that the development results in a net gain for biodiversity, as appropriate under Section 15 of the NPPF.

7. Reptile Strategy - Prior to commencement of the development hereby approved, a strategy for reptile mitigation and Stag beetle habitat enhancement shall be submitted to and approved in writing by the local authority. This strategy shall be prepared by a suitably qualified ecologist and appropriate to the local ecological context. Once approved the strategy shall be implemented in full unless otherwise agreed by the local authority in writing.

Reason: To ensure that the proposal is in accordance with Section 41 NERC Act re. UK Biodiversity Action Plan Priority Species (Species of Principal Importance), and complies with Planning Policies for Wildlife including CP7 of the Wokingham Borough Core Strategy (2010), and the National Planning Policy Framework.

8. Construction method statement - No development shall take place, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

Reason: In the interests of highway safety & convenience and neighbour amenities.

Relevant policy: Core Strategy policies CP3 & CP6.

9. Ground and building levels - No development shall take place until a measured survey of the site and a plan prepared to scale of not less than 1:500 showing details of existing and proposed finished ground levels (in relation to a fixed datum point) and finished roof levels shall be submitted to and approved in writing by the local planning authority, and the approved scheme shall be fully implemented prior to the occupation of the building(s).

Reason: In order to ensure a satisfactory form of development relative to surrounding buildings and landscape. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB21.

10. Details of boundary walls and fences - Prior to the commencement of above ground works, details of all boundary treatment(s) shall first be submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented

prior to the first occupation of the development or phased as agreed in writing by the local planning authority. The scheme shall be maintained in the approved form for so long as the development remains on the site.

Reason: In the interests of amenity and highway safety. Relevant policy: Core Strategy policies CP1, CP3 and CP6.

11. Landscaping - Prior to the commencement of above ground works, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished floor levels or contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable. All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development).

12. Protection of trees - a) No development or other operation shall commence on site until an Arboricultural Method Statement and Scheme of Works which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).

b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.

c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.

d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works

commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

13. Landscape management - Prior to the commencement of above ground works, a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

14. External lighting - Prior to the commencement of above ground works, details of any proposed external lighting scheme shall be submitted to and approved in writing by the local planning authority. The scheme shall set out the steps that will be taken to ensure that external lighting does not cause a nuisance to local residents including future residents of the site and shall include details of location, direction and level of illumination.

Reason: To protect the amenity of local residents. Relevant policy: Core Strategy policy CP3.

15. Cycle parking details required - Prior to the commencement of above ground works details of secure and covered bicycle storage/ parking facilities for the occupants of [and visitors to] the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before occupation of the development hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

16. Heritage works - Notwithstanding the details submitted prior to the commencement of above ground works, details for the proposed landscaping and tree planting adjacent to the boundary to the churchyard are to be submitted and approved in writing by the local planning authority. Landscaping and tree planting beside the boundary wall to the churchyard shall then be undertaken in accordance with the details as approved.

Reason: In the interests of visual amenity and to safeguard the setting of heritage asset. Relevant policy: National Planning Policy Framework Section 16 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB24.

17. Travel Plan - Prior to the occupation of the development a Travel Plan shall be submitted to and approved in writing by the local planning authority. The travel plan shall include a programme of implementation and proposals to promote alternative forms of transport

to and from the site, other than by the private car and provide for periodic review. The travel plan shall be fully implemented, maintained and reviewed as so-approved.

Reason: To encourage the use of all travel modes. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policy CP6.

18. Age restriction - The apartments hereby approved shall only be occupied by persons over the age of 55 and by any wife, husband or partner of such a person unless otherwise agreed in writing with the Local Planning Authority.

Reason: In the interests of highway safety. Relevant policy: Core Strategy policy CP7.

19. Parking and turning space to be provided - No part of any building(s) hereby permitted shall be occupied or used until the vehicle parking and turning space has been provided in accordance with the approved plans. The vehicle parking and turning space shall be retained and maintained in accordance with the approved details and the parking space shall remain available for the parking of vehicles at all times and the turning space shall not be used for any other purpose other than vehicle turning.

Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

20. Mobile Buggy Parking - No building shall be occupied until secure and covered parking for mobility buggies has been provided in accordance with the approved drawing(s)/details. The mobility buggy parking/ storage shall be permanently so retained for the parking of mobility buggies and used for no other purpose.

Reason: In the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

21. Access surfacing - No building shall be occupied until the vehicular access has been surfaced with a permeable and bonded material across the entire width of the access for a distance of 10 metres measured from the carriageway edge.

Reason: To avoid spillage of loose material onto the highway, in the interests of road safety. Relevant policy: Core Strategy policy CP6.

22. Access to be provided - No building shall be occupied until the access has been constructed in accordance with the approved plans. The works are to include dropped kerbs, tactile paving and the stopping up and reinstatement of the footways where redundant residential accesses exist before the development.

Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.

23. Prior to occupation of the development hereby approved, a CCTV scheme shall be submitted to and approved in writing by the Local Planning Authority. Details shall include the type of camera / quality of image (which should be identification, recognition and observational quality) and location of the cameras. The camera system shall be installed as per the approved details and maintained as such during the lifetime of the development.

Reason: To create safe and accessible environments where crime, disorder, and the fear of crime are reduced. Relevant policy: Core Strategy policy CP1.

24. Drainage Strategy - The development hereby approved shall be carried out in accordance with the details as agreed in 'Drainage Strategy Report, Infrastructure Design Limited, IDL/1011/DS/01 25 March 2020 and plan SE-2661-03-DE-151 Maintenance Strategy for Drainage' received by the Local Planning Authority on 7 September 2020 and 28 October 2020. The measures contained within the plan and

report shall be implemented in accordance with the approved details unless otherwise approved in writing by the local planning authority.

Reason: to prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

25. Energy Assessment - The development hereby approved shall be carried out in accordance with the details as agreed in 'Energy and Sustainability Assessment, Focus Consultants February 2020' received by the Local Planning Authority on 7 September 2020. The measures contained within the report shall be implemented in accordance with the approved details unless otherwise approved in writing by the local planning authority.

Reason: To ensure developments contribute to sustainable development. Relevant policy: NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 & the Sustainable Design and Construction Supplementary Planning Document.

26. Noise Impact - The development hereby approved shall be carried out in accordance with the details as agreed in 'Noise Impact Assessment, 24 Acoustics, R8447-1 Rev 0, 31 March 2020' received by the Local Planning Authority on 7 September 2020. The measures contained within the survey shall be implemented in accordance with the approved details unless otherwise approved in writing by the local planning authority.

Reason: To protect the amenity of future residents. Relevant policy: Core Strategy policy CP3.

27. Construction Method Statement - The development hereby approved shall be carried out in accordance with the details as agreed in 'Construction Method Statement, McCarthy & Stone, 28 August 2020' received by the Local Planning Authority on 7 September 2020 so far as they relate to the measures to minimise the environmental impacts of construction and demolition. The measures contained within the report shall be implemented in accordance with the approved details unless otherwise approved in writing by the local planning authority.

Reason: To protect the amenity of future residents. Relevant policy: Core Strategy policy CP3.

28. Retention of trees and shrubs - No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

29. Removal of demolished materials - All materials arising from the demolition of buildings on the site shall be permanently removed from the site.

Reason: In the interests of the amenity of the area. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB21.

30. Hours of work - No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 08:00 and 18:00 Monday to Friday and 08:00 to 13:00 Saturdays and at no time on Sundays or Bank or National Holidays.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

31. Contamination - In the event that contamination is found at any time during site clearance, groundwork and construction, the discovery shall be reported within two weeks to the local planning authority and a full contamination risk assessment shall be carried out (with a 'remediation method statement' if required) and submitted to the local planning authority within eight weeks of the discovery. Works thereafter shall be carried out in accordance with the approved 'remediation method statement' and a final validation report shall be submitted to and approved in writing by the local planning authority prior to occupation of the development hereby approved.

Reason: To protect future occupiers and users of the site from the harmful effects of contamination. Relevant policy: Core Strategy policy CP1.

32. Contamination - In the event that no evidence of contamination is found during the construction of the development a statement to that effect shall be submitted to and approved in writing by the local planning authority prior to occupation of the development hereby approved.

Reason: To protect future occupiers and users of the site from the harmful effects of contamination. Relevant policy: Core Strategy policy CP1.

Informatives

1. A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991.

2. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. Should you require further information please refer to the Thames Water website.

3. With regard to waste services, the applicant is advised to consider the details on the Council's website.

4. The applicant is advised that Royal Berkshire Fire and Rescue Service have provided details of site specific matters that could be used to help ensure an effective response to an emergency.

5. The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried

out only in accordance with those details. If this is not clear please contact the case officer to discuss.

6. This permission should be read in conjunction with the legal agreement under section 106 of the Town and Country Planning Act dated [INSERT], the obligations in which relate to this development.

7. The development hereby permitted is liable to pay the Community Infrastructure Levy. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. A revised Liability Notice will be issued if this amount changes. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see the Council's website.

8. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:

a) addressing the evolving planning policy context; b) a full pre-application process was undertaken by the applicant; and, c) planning issues relating to ecology, highway safety, tree and landscape matters.

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

9. Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact the Highway Authority on tel.: 0118 9746000.

10. The developer's attention is drawn to the fact that this permission does not authorise the physical construction of the proposed off-site highway works and site access connections to the public highway. A separate legal agreement made with the Council under s.278 of the Highways Act 1980 is required. No work within or affecting the public highway shall commence until the agreement has been completed and the Council, as local highway authority, has approved all construction and installation details together with a programme of works.

11. The requisite Travel plan would need to comply with the latest national and local guidance:

1) NPPF Section 9 (Sustainable Transport)

2) The Essential Guide to Travel Planning (DfT, March 2008) 3) Delivering Travel Plans Through the Planning Process (DfT, April 2009)

4) A Guide on Travel Plans for Developers (DfT)

5) Making Residential Travel Plans Work (DfT, June 2007) All accessible at:
<http://www.dft.gov.uk/pgr/sustainable/travelplans/>
<https://www.gov.uk/government/policies/improving-local-transport>

Also:

WBC Transport Plan 3 and Active Travel Plan 2011 – 2026

WBC Workplace Travel Plan Guidance and Residential Travel Plan Guidance

Documents, covering workplace travel plans and residential travel plans provide local guidance and are available on the Borough's website.

PLANNING HISTORY		
Application Number	Proposal	Decision
F/2005/4471 (58 Reading Road)	Proposed erection of a two and a half storey building to provide 5 flats and parking. Demolition of existing dwelling.	Conditional approval 7 July 2006
CLE/2010/205 7 (58 Reading Road)	Application for a Certificate of Proposed Lawfulness to establish that the development approved in planning permission F/2005/4471 was commenced within 3 years of the date of the approval in accordance with condition 1 of that permission and that continuing development in respect of the permission is lawful.	Approval 29 October 2010
F/2009/1180 (52 Reading Road)	Proposed change of use from residential to office accommodation with associated community support facilities plus temporary siting of modular unit plus provision of extension to dropped kerb to allow vehicular access	Conditional approval 19 August 2009
163444 (52 Reading Road)	Full application for the proposed erection of supported affordable housing consisting 7no bedsitting rooms, 2no single person flats and supporting ancillary accommodation, car parking, motorcycle parking, covered cycle storage and garden, following demolition of existing building.	Conditional approval 1 February 2017
180320 (52 Reading Road)	Change of use of land from landscaped amenity open space to private amenity space to create a larger garden for supported housing	Conditional approval 12 April 2018
F/2012/1444 (Station Link Road)	Demolition of numbers 48 and 50 Reading Road and construction of a single carriageway link road of approximately 336 metres in length connecting the A321 Wellington Road and the A329 Reading Road (part one-way and part two-way operation); provision of a new combined footway and cycleway along the link road, a transport interchange at Wokingham Railway Station, four new traffic signal controlled junctions and associated alterations, landscaping, two noise barriers and provision of new pedestrian crossing facilities.	Conditional approval 1 February 2013

RAIL/2011/2049 (Wokingham Train Station)	Application by Network Rail for approval of details for the redevelopment of Wokingham Station under the National Station Improvement Programme (NSIP), to include demolition of existing station building and adjacent staff accommodation buildings.	Conditional approval 21 December 2011
--	--	---------------------------------------

SUMMARY INFORMATION	
For Residential	
Site Area	0.45 hectares
Existing units	3
Proposed units	34
Existing density – dwellings/hectare	6.6
Proposed density - dwellings/hectare	75.5
Number of affordable units proposed	Off-site contribution of £400,000
Previous land use	Residential
Proposed Public Open Space	Not required
Existing parking spaces	Minimum of 6
Proposed parking spaces	32

CONSULTATION RESPONSES	
Berks, Bucks and Oxon Wildlife Trust	No comments received
Berkshire Archaeology	No objection subject to condition (4)
Crime Prevention Design Officer	No objection subject to conditions (14, 23)
National Grid	No comments received
Royal Berkshire Fire and Rescue	No objection
Southern Gas Networks	No objection subject to informative
SEE Power Distribution	No comments received
Thames Water	No objection subject to informative (1 – 3)
NHS Wokingham Clinical Commissioning Group	No comments received
Historic England	No objection
WBC Cleaner & Greener (Waste Services)	No comments received
WBC Drainage	No objection
WBC Ecology	No objection subject to conditions (5 – 7)
WBC Environmental Health	No objection subject to conditions (27, 31, 32)
WBC Economic Prosperity and Place (Community Infrastructure)	No objection subject to legal agreement
WBC Green Infrastructure	No objection
WBC Growth & Delivery (Planning Policy)	No objection
WBC Heritage	No objection subject to conditions (16)
WBC Highways	No objection subject to conditions (8, 15, 17 – 22, 27)
WBC Tree & Landscape	No objection subject to conditions (9 – 13)
WBC Property Services	No comments received

REPRESENTATIONS
Town/Parish Council: No objection

Local Members: One letter of comment from Councillor Rachel Bishop-Firth stating support of smaller homes for older residents but requests consideration of traffic joining Reading Road.

Neighbours: One letter of comment received stating no objection to the plan and the developer should work closely with the adjacent Church community to mitigate the impact of building work.

APPLICANTS POINTS

- There is an accepted need for this type of housing and it would 'free-up' existing housing stock.
- Significant consideration has gone into the design of the scheme and its impact on the adjacent listed building.
- The scheme has been considered through a Road Safety Audit in terms of the impact on Reading Road traffic flow.

PLANNING POLICY

National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP17	Housing Delivery
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB05	Housing Mix

	TB08	Open Space, Sport and Recreational Facilities Standards
	TB09	Residential Accommodation for Vulnerable Groups
	TB12	Employment Skills Plan
	TB21	Landscape Character
	TB22	Sites of Urban Landscape Value
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
	TB25	Archaeology
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide
		National Internal Space Standards
		CIL Guidance + 123 List
		Affordable Housing Supplementary Planning Document
		Sustainable Design and Construction Supplementary Planning Document

PLANNING ISSUES

Description of Development:

1. The proposal is for the erection of a 34 unit retirement living apartment scheme with associated communal facilities, parking and landscaping and would follow the demolition of properties 54 – 58 Reading Road. The scheme would be intended for older persons with an emphasis on providing independent living rather than with immediate medical care on site. As such, the scheme would involve level access to all apartments, lifts to the first floor and above, communal areas including residents' lounge, refuse room and a mobility scooter store as well as an on-site house manager.
2. Due to the shape of the proposal site, the building has been designed in a 'T' shape with the main body of the building addressing Reading Road and a further element projecting into the site. At its widest, the building would be 35.5 metres and at its longest the building would be 50.3 metres. It would be three storeys and the highest ridge point would be 9.5 metres. In respect of the existing three dwellings on the site, the front elevation would have projecting elements. A separate entrance and exit to Reading Road as well as some parking spaces would be provided to the front of the site. The access road would continue along the south eastern edge of the site to a larger parking area as well as the communal garden area.
3. For clarity, the scheme was considered at the pre-application stage which has helped inform the design of proposal as well as specific consultee matters.

Principle of Development:

4. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan Policy CC01 states that planning applications that accord with the policies in the Development

Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.

5. The site is within a major development location and therefore development is acceptable in principle in these areas as outlined through policies CP9 of the Core Strategy and CC02 of the MDD Local Plan. The scheme would not lead to a net loss of C3 use type housing and therefore no conflict with policy CP3 of the Core Strategy arises.
6. As set out in the submissions, there is an identified need for the type of housing proposed: C3 use but with communal facilities on site for elderly accommodation. Whilst the policy response raises a query about the type of housing proposed, from a planning perspective it is recognised as 'sheltered accommodation'. This is accepted as typically being "*purpose-built flats or bungalows with limited communal facilities such as a lounge, laundry room and guest room. It does not generally provide care services, but provides some support to enable residents to live independently. This can include 24 hour on-site assistance (alarm) and a warden or house manager*" as set out in the National Planning Practice Guidance (paragraph 10).
7. The need for this type of housing is recognised as a national issue in the NPPF and also at the local level through the Housing Market Assessment (HMA) which projects the growth of an elderly population in the next 20 years. Whilst it is accepted the most up-to-date information is shown through the Local Housing Needs Assessment (LHNA) (January 2020), it nonetheless identifies a need for this type of housing as found in the HMA. The submissions also make the point that the provision of this type of housing helps 'free up' existing housing stock which is considered beneficial to housing land supply factors. Furthermore, policy CP2 of the Core Strategy states, "to ensure that new development contributes to the provision of sustainable and inclusive communities (including the provision of community facilities) to meet long term needs, planning permission will be granted for proposals that address the requirements of: an ageing population, particularly in terms of housing, health and wellbeing". Policy TB09 of the MDD also supports, in principle, schemes that provide for the elderly through purpose built accommodation. The issues as set out are accepted and it is considered no conflict with the principle of development arises.
8. For confirmation, whilst the site comprises part of the allocation for works to Wokingham Train Station through policy SAL09, no conflict arises regarding the site being available for transport related schemes. It is considered the station rail link project has been completed and the site would not be required to contribute towards this. On the basis of the above, no conflict in terms of the principle of development arises.

Character of the Area:

9. *Existing:* Development along Reading Road has been established for some time with St. Paul's Church being a defining point of the development of Wokingham. The existing residential properties at the site were clearly later additions and were part of a line of dwellings that extended to what is now the Wade Centre for the elderly. This particular element was altered through the construction of Station Approach which necessitated the removal of some of these dwellings. Additionally, the car park deck structure and the train station redevelopment itself

are examples of modern-day changes nearby the site. On the opposite side of the site, Beeches Manor, a two storey, single footprint building providing extra-care services for the elderly was constructed within the last 10 years. In looking at the grain of development in the area, it can be seen that there are examples of buildings/structures with a large footprint that are set within a large site. St Paul's Church is the closest example but the car park decking, the train station, Beeches Manor, The Holt School, Imperial Court and even the Council offices themselves are relevant examples.

10. It is important to note that the adjoining church is Grade II* listed and set within a designated Site of Urban Landscape Value. Reading Road is also a Green Route and therefore these planning protections require that any scheme is considerate towards them.
11. *Layout:* The proposal would include the main building to the north eastern part of the site which would essentially occupy the footprint of the current dwellings and their respective amenity space. The access road would be located at the south eastern edge and the car parking/garden area to the south. Whilst the irregularity of the shape of the site is acknowledged, it is considered this ultimately would utilise the site effectively by providing a building and the necessary ancillary aspects. It is also acknowledged that the scheme would result in a single footprint on the site which would be different from the three separate existing dwellings. Nonetheless, as set out above, there are already examples around the site where this occurs and, as such, no conflict with the established character of the area would occur.
12. *Scale and Massing:* The proposed building has a single footprint due to the applicant's desire to provide a certain product; specifically, a building containing multiple apartments that have step-free access and share a communal area. Ultimately this results in a large scale building with considerable more bulk and mass than the current three dwellings on the site. In terms of the height of the scheme, the submitted street scene plan indicates the ridge height would only be marginally higher than the existing buildings. Nonetheless, this would be visible for a much greater section of Reading Road than current i.e. the built form is not broken up by pitched roofs and instead is noticeable across the whole elevation. Adding to this, the projection of the building into the site and the visibility of the side elevation, particularly in views from and around the church, would create a sense of built form greater than at present.
13. In response to concerns raised at the pre-application stage, the applicant has amended the design of the building further. In particular, this has involved shifting the rear projection of the building further to the south east to help prevent views of the side elevation from Reading Road. An additional 'step' of the building has also been designed along the north western elevation to again reduce the amount of built form that would be visible. To help visualise the scheme in context, the applicant has also provided visual representations of the scheme. These were achieved by digitally inserting a 3D projection of the scheme into a picture of the site taken at different viewpoints. They indicate that, clearly the scheme would introduce a large building into the street scene, and this would be most noticeable from the station car parking deck and the junction of Reading Road with Station Approach, however existing vegetation would help filter views into the site. Additionally, the scheme would not appear substantially higher than

the immediate neighbouring building, 52 Reading Road. They also show that from Station Approach in particular, the existing vegetation prevents any substantial view into the site. Indeed, the submitted Landscape Visual Impact Assessment identifies the two major visual effects would be from Reading Road adjacent to the site (viewpoint 5) and from the adjacent churchyard (viewpoint 3). The Tree and Landscape Officer agrees that the impact from these viewpoints would be medium as *“although the proposed development will be prominent it is not considered to be uncharacteristic when considered against the receiving landscape”*.

14. *Materials and Design Features:* Due to the three-storey scale of the proposal, it is acknowledged there is a sense of verticality to the building. To help break this up, the scheme has stepped eaves heights and, particularly at the front, projecting gables to give a sense of the three properties that currently exist on the site. Additionally, material changes have been organised on a horizontal basis to give greater articulation to the building and compliment the stepping of the elevations. The materials proposed are a mix of surrounding materials found in the street scene however it is noted that the use of brick facades with brick detailing is the most common feature and therefore the scheme exemplifies this. Overall, the materials and design features used allow for an active frontage on all sides of the building.
15. *Landscaping:* The scheme would require the removal of four trees (two that have died and two to be replaced with better specimen trees) and, through a landscaping scheme, it is expected that these would be replaced and additional trees planted. Although precise numbers would be agreed at a conditions stage, a Landscaping Strategy suggests that approximately 40 trees would be planted and the Tree and Landscape Officer supports this. Specifically, the use of landscaping would help mitigate the increase in built form and enhance the Green Route. It is noted that the provision of a boundary hedgerow with specimen trees along the Churchyard boundary would help reduce the dominance of the building. On the basis of the above, the Tree and Landscape Officer does not object to the proposal, subject to conditions (9 – 13).
16. *Conclusion:* The proposed scheme would result in a large building where currently three dwellings of typical two storey scale exist. Whilst there would be a noticeable change in the immediate vicinity of the site, the overall landscape is one of built form and indeed one that contains buildings with a single large footprint. The use of appropriate materials, the breaking up of the form of the building through design features and the enhancements to existing landscaping would ensure that the scheme would assimilate into the character of the area. As such, the scheme is considered appropriate in its context which takes account of the Site of Urban Landscape Value, the Grade II* listed church and the Green Route. Therefore the scheme is in accordance with policies CP1 and CP3 of the Core Strategy and policies CC03, TB21, TB22 and TB24 of the MDD Local Plan.

Listed Building:

17. The Church of St. Paul is located adjacent to and north west of the site and is Grade II* listed. At its closest point, it is 29 metres from the site boundary and, as such, there is no harm to the historic fabric of the building. Nonetheless, there could be impact upon the setting of the building. The Heritage Officer notes that setting is defined in the NPPF as being “the surroundings in which a heritage

asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral”.

18. The proposal would result in the building being marginally further away than the current dwelling however the scale of the building would be substantially increased and, ultimately, would be more noticeable in short range views from and around the church. It is noted that, in the context of the listed building, the main change has been the development around the 1930s of the series of detached dwellings along Reading Road. On this basis therefore it is acknowledged that the scheme would introduce built form in the vicinity of the church which would not be in keeping with the historic ‘lack of development’ and therefore harm arises. The scale of this harm however is not considered to be substantial given the distance from the building and the wider context of the development of Wokingham town. Indeed, the Heritage Officer agrees with the conclusion of the Heritage Statement that the scheme ranges between ‘minor adverse’ to ‘no impact’ on heritage assets around the site. As such, in terms of the NPPF impact, the scheme results in “less than substantial harm” and this needs to be weighed against the benefits of the scheme. It should be noted that Historic England have raised no objection to the proposal.

19. On a separate issue, the Heritage Officer has raised concern regarding the impact of excavation and planting along the boundary wall with the adjacent church yard due to the potential for the wall to be damaged. Nonetheless, they consider that subject to further details sought by condition (16), this issue could be overcome.

Amenity of Neighbouring Occupiers:

20. The only immediate neighbouring property would be 52 Reading Road. For clarity, this is a supported affordable housing property i.e., a quasi-residential use. Due to the separation distance to this property, no concerns are raised with regard to any overbearing or loss of light impact. It is noted however that the proposal would include windows facing this site along the south eastern elevation which would provide opportunities for overlooking to the amenity space. The Borough Design Guide advises that proposals above two storeys should maintain a separation distance of at least 15 metres in terms of flank to rear relationships. It is considered this is the most appropriate assessment to make and the proposal would accord with this. As such, no conflict with policy CP3 of the Core Strategy arises.

Amenity of Future Occupiers:

21. *Internal Space:* Internal space standards are set nationally and based on the number of bedrooms. The submitted plans indicate that the scheme would exceed the minimum amount of floor space required and therefore no objection is raised with regard to this. It should also be noted that the scheme includes a communal lounge within the building which adds to the overall provision.

22. *Amenity Space:* The provision of amenity space would be through two distinct areas; a mix of soft and hard landscaped areas immediately around the building comprising of approximately 670m²; and a more informal area adjacent to the car park, comprising of approximately 400m² with potential seating areas. It is

considered this would be appropriate for the scale of development and is in accordance with advice set out in the Borough Design Guide that flat proposals allow for shared amenity space.

Highway Access and Parking Provision:

23. *Highway Safety:* The site is accessed from Reading Road, a major road within Wokingham which experiences a high level of traffic. It is proposed for the site to be accessed using an in/out access arrangement and therefore a Stage 1 Road Safety Audit has been undertaken to accompany a Transport Statement with the application.
24. The access is in close proximity to the Station Approach/Reading Road junction and also a number of right turn lanes on Reading Road. The trip generation of the scheme is stated as being an increase above the existing of one trip in the AM period, two trips in the PM period and 12 trips across a 12-hour period. The Transport Statement considers this to be negligible in terms of the impact on the highway network. The number of rights turns from Reading Road was also surveyed and this concluded that the number for the bowling club were infrequent but the youth centre higher than expected. Nonetheless, the survey results demonstrate that that the existing right turn lanes would have minimal conflict with the proposed development on right turn lanes and the proposed development site.
25. The Highways Officer has considered the submitted information and agrees with the recommendations of the Road Safety Audit. As such, subject to conditions (17, 19, 21, 22), no conflict with policy CP6 of the Core Strategy is considered to arise.
26. *Accessibility:* The site is located in a major development location and a short distance from Wokingham town centre. It is also in close proximity to Wokingham Railway station, bus stops for local services and accessed from a major road within the town. In this respect, it is considered that the site is accessible by a range of transport modes as well as being within walking distance of day-to-day shopping needs. As such, the scheme complies with policy CP6 of the Core Strategy.
27. *Parking Provision:* The scheme proposes a total of 32 unallocated parking spaces with four provided near the front of the site and the remaining 28 (including two disabled spaces) at the rear of the site. The adopted parking standards indicate that a C3 use type of this scale would require a total of 38 unallocated parking spaces and therefore there is a shortfall of six spaces. Notwithstanding this, the applicant has provided information regarding car ownership for similar sized schemes across the country and, ultimately, the demand for parking spaces is lower due to the typical person living on the site. The Highways Officer is satisfied that a lower parking provision would not cause undue parking issues around the site on the basis of this information however, they recommend that a condition (18) is applied to ensure the age of occupants is maintained as per the additional information.
28. It is acknowledged that the scheme may require a manager/member of staff to be on site and this would necessitate a parking space. However, this could be

accommodated within the provision noted above and, as such, no objection is raised.

29. *Cycle Provision:* As noted above, the applicant has provided information regarding the demand for cycle parking which would equate to one space required for the development. This, along with visitor cycle parking is proposed to be provided within the mobility buggy store on the site. Again, the Highways Officer is satisfied with this subject to a condition (15).

30. *Waste collection:* It is considered that a waste vehicle could successfully enter and exit the site with the proposed arrangement, and this would allow for the safe collection of waste.

Crime Prevention:

31. The Crime Prevention Design Advisor has considered the scheme and noted areas that could be improved to deter criminal activity and also provide safety for residents. It is noted however that elements of this are not material considerations to the application including, for example, the provision (or lack of) a 'trades' entrance button. Nonetheless, discussions with the applicant have occurred and specific matters are addressed below.

32. With regard to the provision of solid panel fencing along the boundary with the church, it is considered this would lead to harm in terms of views in and out of the site and therefore further harm to the setting of the listed building. It is proposed to provide defensible planting along the boundary which would deter movement and be in line with 'Secured by Design' guidance. This is considered an appropriate balance to make.

33. An external lighting strategy has not been submitted and this has been highlighted by the Crime Prevention Design Advisor. Nonetheless, this can be secured by condition (14). Similarly, a CCTV strategy could be secured through a condition (23). Aspects such as postal deliveries and access controls in terms of a reception desk would be resolved through the applicant's typical approach i.e., this is not a material planning consideration. Clarification on the use of emergency exits has been provided and this is considered satisfactory.

Flooding and Drainage:

34. As the proposal is a major development and the site is subject to pluvial flooding then a Flood Risk Assessment is required. At the pre-application stage, the Drainage Officer raised queries on the surface water drainage system being designed not to flood any part of the site in a 1 in 30 year return period design storm; any overland flows being intercepted before it enters the proposed site; and, consent being obtained from the relevant Water Company if the proposal is to discharge surface water runoff into a surface water sewer or combined sewer. The submitted flood risk assessment provides sufficient information on these matters for the drainage officer to be satisfied that, subject to a condition (24) requiring the drainage strategy to be carried out, there would not be a harmful impact in terms of flooding or drainage in relation to the proposal. On this basis, no conflict with policy CC09 or CC10 of the MDD Local Plan arises.

Landscape and Trees:

35. A Tree Survey and Impact Assessment with accompanying Tree Constraints Plan and Tree protection Plan have been provided to support the application. These note that there a number of trees within the site protected by TPO 1147/2006 & 1116/2006 however a number of the TPO oaks are now dead and will need to be removed. The Tree and Landscape Officer has considered the submitted information and notes that the most significant trees are being retained as well as landscape mitigation occurring for the increase in built form on the site particularly through a boundary hedgerow with specimen trees along the north western boundary. In addition, the Tree and Landscape Officer notes that some further details regarding trees including the replacement of the dead oak trees, the replacement and therefore enhancement of two category C trees and clarification on species set out in the landscape strategy would be required however this can be resolved through the use of conditions (9 – 13). As such, with regard to specific tree matters, subject to conditions, the scheme accords with policy CC03 of the MDD Local Plan.

Environmental Health:

36. At the pre-application stage, the Environmental Health Officer considered the main issues were potentially contaminated land and the mitigation of noise from nearby traffic. As such, a Geotechnical Study and Investigation Report and a Noise Impact Assessment have been submitted.

37. *Potentially Contaminated Land:* The Geotechnical report states that following investigation, no substantially harmful contamination was found at the site and therefore the risk to the end users of the site (in this case a residential use) would be very low. The report considers that certain building techniques may be required in terms of the foundations but this should be achievable. The Environmental Health Officer has considered this report and agrees with its recommendations subject to a condition (31, 32).

38. *Noise Impact:* The Noise Impact Assessment states that the ambient noise levels for day and night times were established and then applied to the proposed development. On the basis of using sufficient glazing and ventilation systems, the assessment concludes that the level of noise heard internally would be within the maximum level expected of such areas. The Environmental Health Officer has considered this report and agrees with its recommendations subject to a condition (26).

Ecology:

39. The site contains habitats suitable for protected and priority species including bats, reptiles and hedgehogs. Given the potential impact on these species, a Preliminary Ecological Appraisal and Preliminary Roost Inspection has been submitted. The appraisal notes that no harmful impact would occur with regard to protected sites, protected habitats (ancient woodland, hedgerows or habitats of principal importance) and biodiversity opportunity areas. With regard to species, it considers no harmful impacts would occur in terms of amphibians, badgers and nesting birds (subject to not carrying out clearing works within nesting season). In terms of reptiles and bats, the survey considers the site could support suitable habitats for these and therefore additional survey work is required to ascertain if they are present on the site.

40. With regard to reptiles, the survey found evidence of them on site and proposes to translocate them elsewhere. The Ecology Officer notes that the survey was conducted appropriately but the information on translocation is not detailed. Nonetheless, they consider a condition (7) requiring further information would overcome this. With regard to bats the survey found evidence of a roost on the site; in deadwood and also one of the buildings. On this basis, the Ecology Officer considers a derogation licence from Natural England is required prior to work taking place on site which, again, could be controlled through a condition (5).

41. The NPPF aims to secure net gain in terms of biodiversity and, in this instance, the Ecology Officer advised a net gain calculator be used to ascertain this. The calculator indicates that through the proposed landscaping scheme and the translocation of reptiles off the site, a 10% increase could be achieved. However, further details of this (including the strategy for translocation) would be required by condition (6). As such, subject to these conditions, it is considered the scheme accords with policies CP7 of the Core Strategy and TB23 of the MDD Local Plan.

Sustainable Design and Construction:

42. As per policy CC05 of the MDD Local Plan, the scheme meets the threshold to demonstrate that a minimum 10% reduction in carbon emissions through renewable energy or low carbon technology can be achieved. The submitted Energy and Sustainability Statement sets out that the construction of the building would be in excess of building regulation requirements, and this will help ensure the scheme minimises its energy impact. It also states that a solar PV array would be installed to the roof of the building which would be of sufficient size to achieve the 10% reduction stated on policy. On this basis, and subject to a condition (25) requiring the scheme to be in accordance with the submitted information, no conflict with policy CC04 arises.

Infrastructure:

43. As the proposal is for residential use, it would be liable for the Community Infrastructure Levy. This would be at a rate of £150 per square metre as it is acknowledged the proposal is for 'sheltered housing' as set out in the CIL charging schedule. On this basis, no conflict with policy CP4 of the Core Strategy arises.

Affordable Housing:

44. The proposal exceeds the threshold for providing affordable housing as per planning policy CP5 of the Core Strategy. A minimum of 30% of the total number of units (net) should be provided as affordable housing which equates to 9.3 units. In this instance, the Council considers it appropriate to require a commuted sum in-lieu of an affordable housing provision which would be £649,792.

45. A viability assessment relating to the provision of affordable housing in-lieu has been submitted. This stated that an offer of £5,620 could be made with the potential to increase this by £215,674 if it remains lawful to change ground rents. For clarity, there is current uncertainty as to whether the Government will alter rules applying to the charging of ground rents. Considering the reduced amount proposed, the assessment was independently verified by the Council's viability consultant. The consultant concluded that several factors within the assessment could be reviewed and discussed these with the applicant. It was agreed that

there was some flexibility within calculating the overall value of the site and therefore a revised offer has been made. The applicant has put forward an offer of £400,000 and this would be regardless of the change in ground rents. The viability consultant has agreed this is a reasonable offer in light of the areas that were discussed, and, on this basis, it was considered appropriate to take the application forward. For confirmation this is approximately 62% of the policy compliant amount. In terms of the overall scheme, this would be the equivalent of approximately 19% of the units being provided as affordable.

46. Whilst the new offer is below the policy compliant amount, it should be noted it is an established planning principle that developments should be determined in accordance with the development plan unless material considerations indicate otherwise. The issue of viability is an established material consideration, and, in this instance, it has been satisfactorily demonstrated that the scheme would be unviable if the policy compliant amount was sought. As such, subject to the signing of a legal agreement to secure the agreed amount, the scheme, whilst not fully accord with policy CP5 of the Core Strategy, would nonetheless be acceptable in this regard.

Special Protection Area:

47. Although the site is within 7km of the Thames Basin Heaths Special Protection Area, the proposal does not meet the threshold require to mitigate impact upon this. As such, no contribution is required in this respect and no conflict with policy CP8 of the Core Strategy arises.

Employment Skills Plan:

48. The proposal is considered as a major development and, as per policy TB12 of the MDD Local Plan, an employment skills plan is required demonstrating how employment opportunities for local business could be provided. It is considered that either a plan detailing this, or a contribution to the Council's employment skills plan could be made but either option could be secured through a legal agreement. For clarification, the Community Infrastructure team have confirmed the contribution would be £3,750. Subject to the signing of a legal agreement, the scheme complies with policy TB12.

Archaeology:

49. In policy terms, the site is considered to have low archaeological potential given the amount of change seen in the area however, an Archaeological Assessment has been submitted. This considers that there is moderate potential for evidence dating back to the medieval period on the site but low potential for any other periods. On this basis, any further investigation could be controlled by condition. The Council's archaeologist has considered the report and agrees with the conclusions. As such, subject to a condition (4) requiring a written scheme of investigation to be agreed with the LPA, the scheme accords with policy TB25 of the MDD Local Plan.

Planning Balance:

50. The proposal would increase the provision of a particular type of housing. The need for this provision is recognised in local and national planning policy and, as such, there is accordance with the development plan in this respect. In order to achieve this, it is noted that the scheme would be large in scale and form and particularly noticeable in the immediate vicinity of the site. Nonetheless, the use

of architectural features, the breaking up of this form and certain materials would ensure that the impact on the character of the area would not be harmful and therefore the scheme is in accordance with the development plan in this respect.

51. The scheme would however have a marginal negative impact upon the setting of the adjacent listed building due to the scale and mass of the building albeit there would be no loss of historic fabric. In this sense there is a minor conflict with the NPPF and policy TB24 of the MDD Local Plan which weighs against the proposal. However, this harm has been identified as less than substantial.

52. In other aspects, the scheme would be acceptable in terms of the amenity of neighbouring residents and future residents. There would be enhancement of trees and landscaping on the site and no harm would occur with regard to environmental health, ecology or archaeological matters. Subject to conditions, the proposal would not detrimentally impact in terms of highway and drainage matters.

53. As a result of a viability report, it is noted that the provision of an affordable housing contribution is reduced. Nonetheless, viability is a material consideration, and this can be a reason to overcome conflict with the development plan. As such, this is considered a minor negative element of the scheme.

54. Overall therefore, there are clear positive impacts of the scheme, not least the provision of suitable accommodation for the elderly. When this is weighed against the minor negative aspects of the impact upon the setting of a listed building and the low provision of affordable housing contribution, it is considered that the public benefits of the scheme outweigh the harm. On this basis, the scheme is recommended for approval.

The Public Sector Equality Duty (Equality Act 2010)

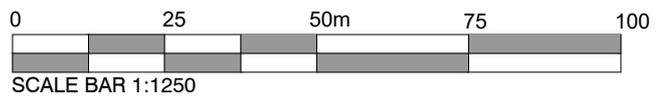
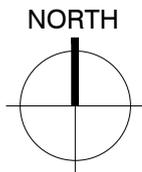
In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. The scheme would impact upon older persons however, there is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.

CONCLUSION

Subject to the conditions set out in the report and the signing of a legal agreement, the scheme is, considered to accord with local and national planning policy and is therefore recommended for approval.



Ordnance Survey (c) Crown Copyright 2020. All rights reserved. Licence number 100022432



ASCOT DESIGN
Timeless architecture

Ascot Design Ltd,
Berkshire House
39-51 High Street,
Ascot,
Berkshire,
SL5 7HY
Tel: 01344 299330
Fax: 01344 299331
Email: info@ascotdesign.com
www.ascotdesign.com

The copyright in this document and design is confidential to and the property of Ascot Design Limited

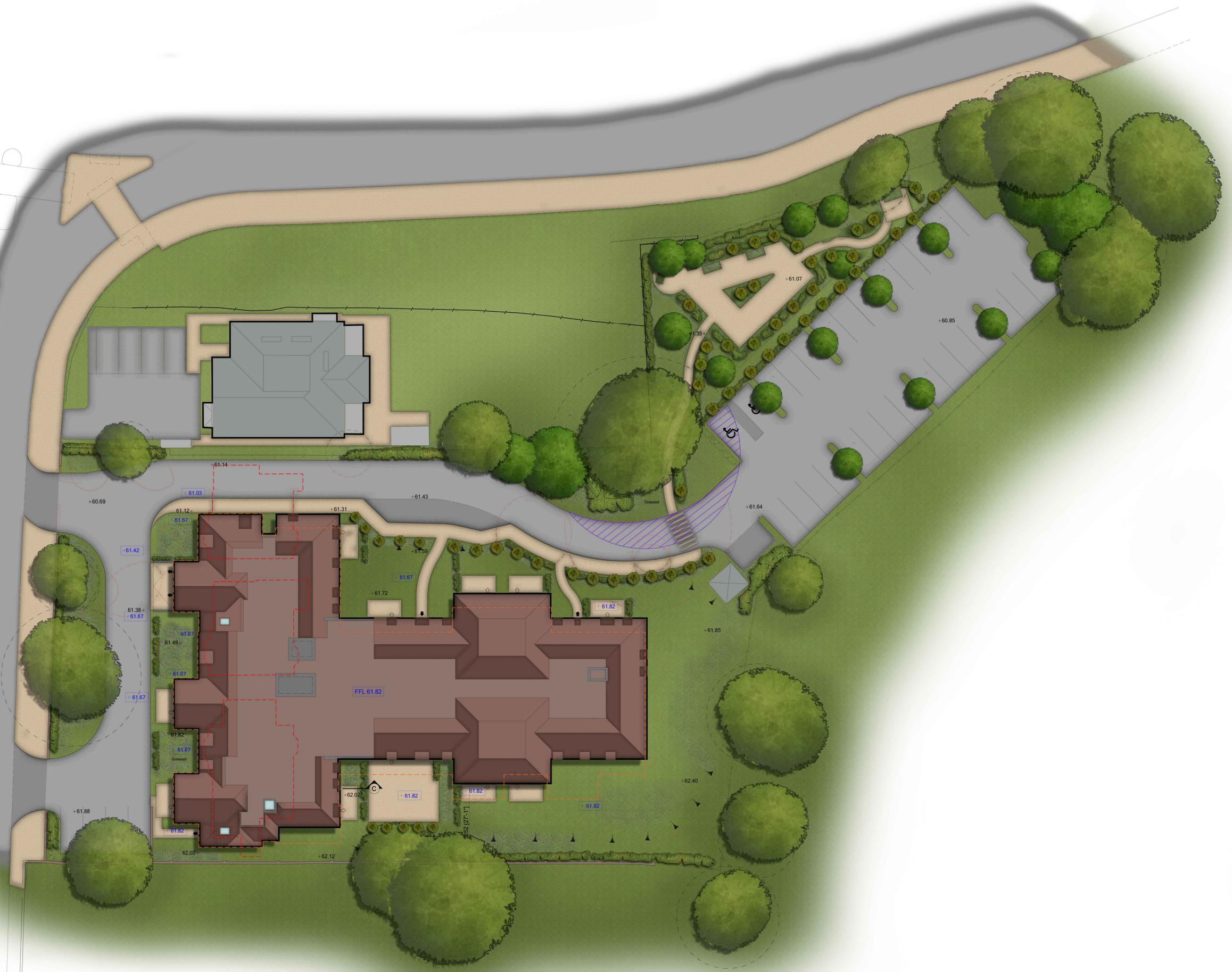
Status	PLANNING APPLICATION
Client	MCARTHY AND STONE
Project Title	54-58 READING ROAD WOKINGHAM, RG41 1EH

Rev.	Date	Detail
DRAWING Title		
LOCATION PLAN		
Scale:	Date	Drawn
1:1250@A4	OCT 19'	SC
DRAWING No.		Rev.
19 - 2923 - LP01		-

This page is intentionally left blank

This page is intentionally left blank

117



- KEY**
-  EXISTING TREES TO BE RETAINED
 -  PROPOSED TREES
 -  TREES TO BE REMOVED
 -  PROPOSED INDICATIVE PLANTING
 -  EXISTING PLANTING
 -  -00.00 EXISTING SPOT SITE LEVELS
 -  +00.00 PROPOSED SPOT SITE LEVELS
 -  FFL 00.00 PROPOSED HOUSE FINISHED FLOOR LEVELS (SUBJECT TO CONFIRMATION AT WORKING DRAWINGS STAGE)
 -  - - - DASHED LINE INDICATES BUILDING TO BE DEMOLISHED
 -  BANK
 -  A ASECTIONLINE
 -  - - - OUTLINE OF BUILDING AT INITIAL PRE APPLICATION STAGE

Rev.	B	Date	13.11.20	Detail	General comment amends
------	---	------	----------	--------	------------------------

PLANNING APPLICATION



ASCOT DESIGN
Timeless architecture

Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY
Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com
www.ascotdesign.com

Client
MCCARTHY & STONE

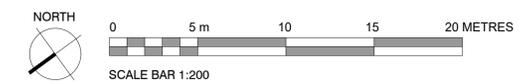
Project Title
**54-58 READING ROAD,
WOKINGHAM, RG41 1EH**

Drawing Title
**PROPOSED COLOURED
SITE PLAN**

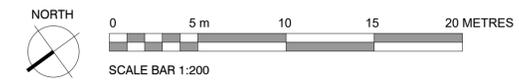
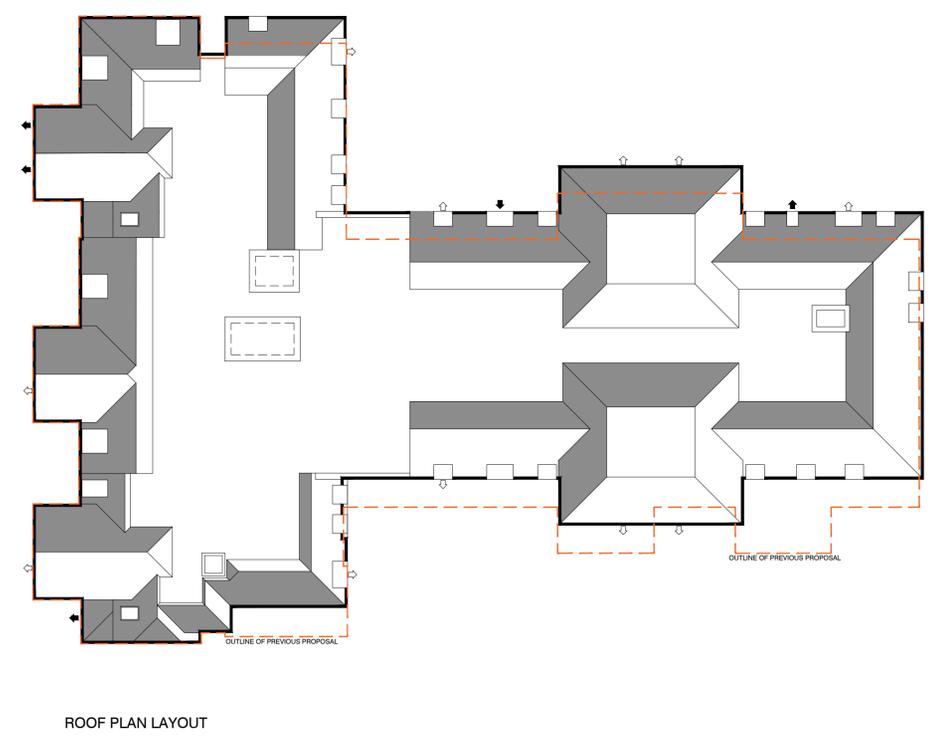
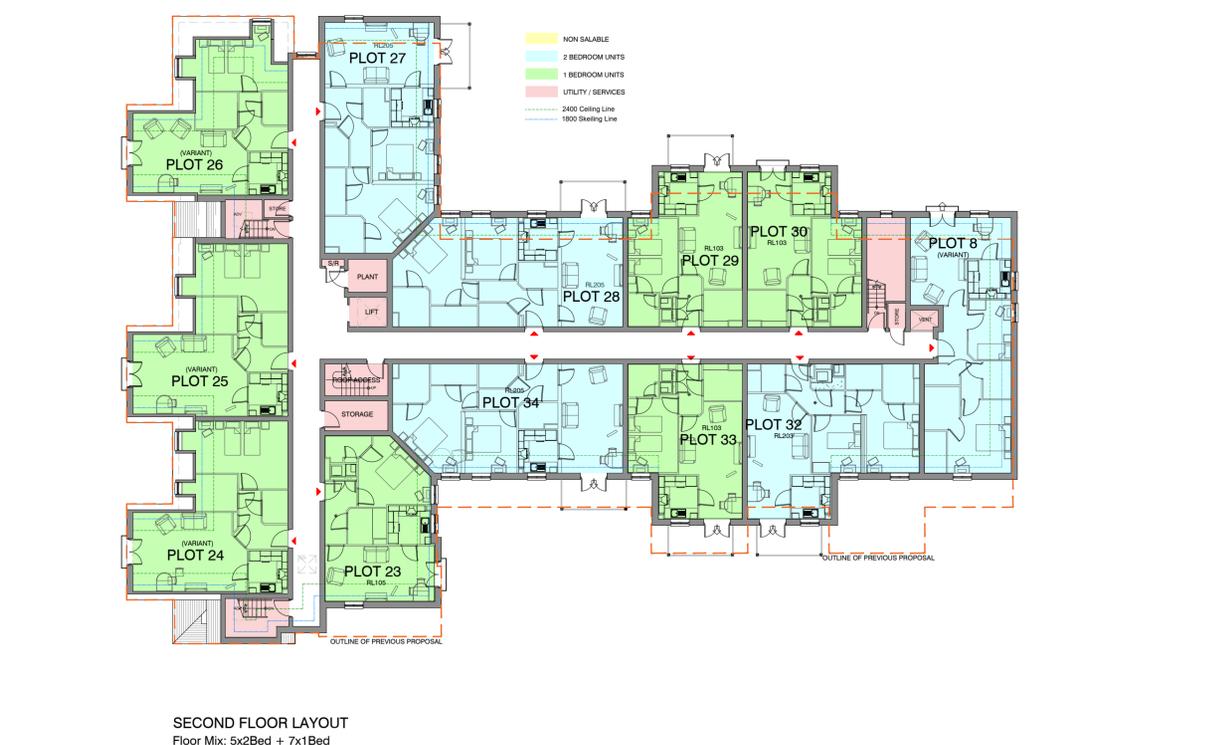
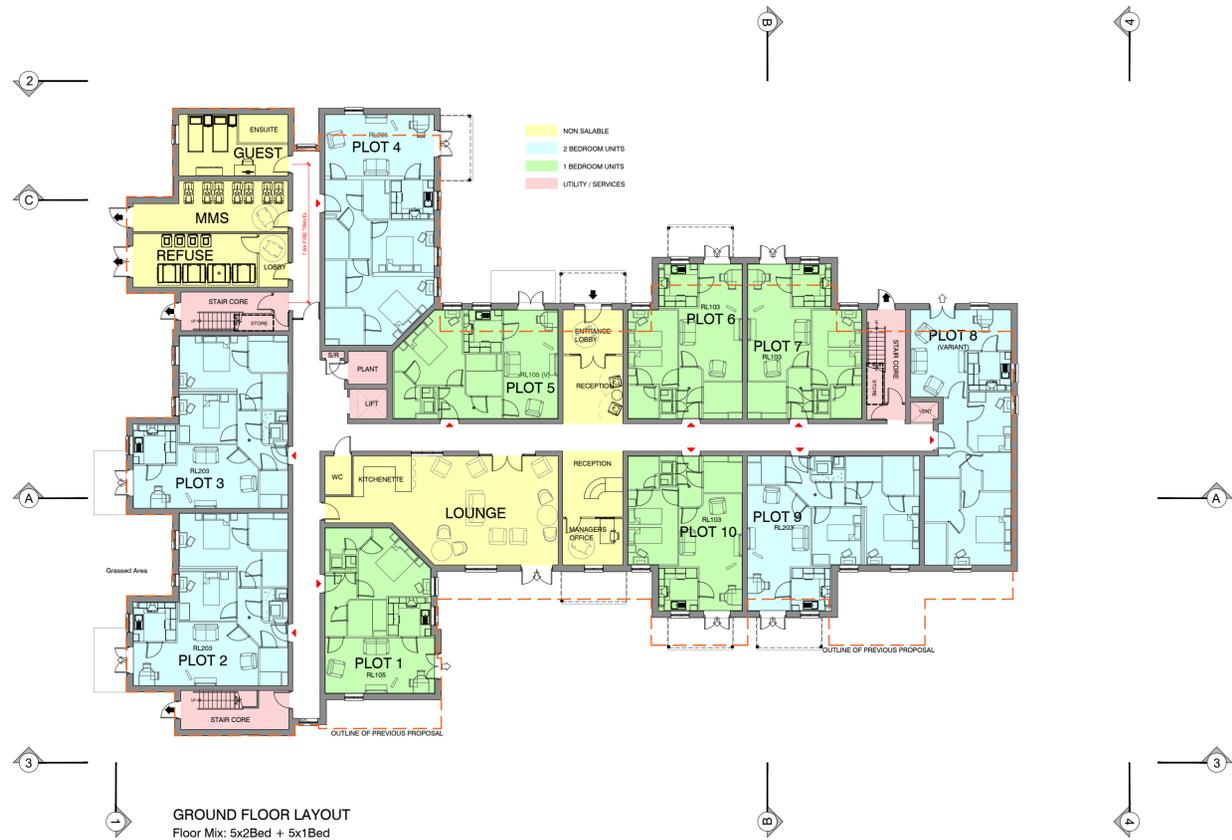
Scale	1:200 @ A1	Date	FEB 20'	Drawn	SC
-------	------------	------	---------	-------	----

Drawing No.	19 - J2923 - 201	Rev.	B
-------------	------------------	------	---

The copyright in this document and design is confidential to and the property of Ascot Design Limited



This page is intentionally left blank



Rev:	B	Date:	13.10.20	Detail:	Front roof form amends
Status:	PLANNING APPLICATION				
 ASCOT DESIGN Timeless architecture <small>Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com www.ascotdesign.com</small>					
Client:	McCARTHY & STONE				
Project Title:	54-58 READING ROAD, WOKINGHAM, RG41 1EH				
Drawing Title:	PROPOSED GROUND, FIRST & SECOND FLOOR PLANS				
Scale:	1:200 @ A1	Date:	FEB 20'	Drawn:	SC
Drawing No.:	19 - J2923 - 202			Rev.:	B
The copyright in this document and design is confidential to and the property of Ascot Design Limited					

This page is intentionally left blank



Side Elevation (2) WITH FOREGROUND CONTEXT
View from Church Boundary
North West Elevation

121

'Proposed Scheme'

SITE BOUNDARY

SITE BOUNDARY



Front Elevation (1) WITH FOREGROUND CONTEXT
Street Scene (Indicative Ground Line)
North East Elevation

'No.52'

'Access Road'

'Proposed Scheme'

'Access'

'Church'

SITE BOUNDARY

SITE BOUNDARY



SCALE 1:100

Rev. C Date 10.11.20 Detail Front Elevation Changes

Status PLANNING APPLICATION



ASCOT DESIGN
Timeless architecture

Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY
Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com
www.ascotdesign.com

Client McCARTHY & STONE

Project Title 54-58 READING ROAD,
WOKINGHAM, RG41 1EH

Drawing Title PROPOSED ELEVATIONS
WITH CONTEXT (SHEET1)

Scale 1:100 @ A1 Date FEB 20' Drawn SC

Drawing No. 19 - 2923 - 203.1 Rev. C

The copyright in this document and design is confidential to and the property of Ascot Design Limited

This page is intentionally left blank



Side Elevation (3) WITH FOREGROUND CONTEXT
 View from Access Road (Indicative Ground Line)
 South East Elevation



123
SITE BOUNDARY

'Proposed Scheme'

'Access Road'

SITE BOUNDARY



Rear Elevation (4) WITH FOREGROUND CONTEXT
 View From Car Park
 South West Elevation

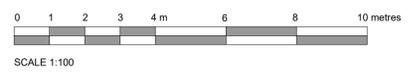


'No.60'
SITE BOUNDARY

'Proposed Scheme'

'No.52' / 'Car Park To Rear'

SITE BOUNDARY



Rev	B	Date	10.11.20	Detail	Front Elevation Changes
-----	---	------	----------	--------	-------------------------

Status: **PLANNING APPLICATION**

ASCOT DESIGN
 Timeless architecture

Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY
 Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com
 www.ascotdesign.com

Client: **MCCARTHY & STONE**

Project Title: **54-58 READING ROAD,
 WOKINGHAM, RG41 1EH**

Drawing Title: **PROPOSED ELEVATIONS
 WITH CONTEXT**

Scale	1:100 @ A1	Date	FEB 20'	Drawn	SC
-------	------------	------	---------	-------	----

Drawing No.	19 - 2923 - 204.1	Rev.	B
-------------	-------------------	------	---

The copyright in this document and design is confidential to and the property of Ascot Design Limited

This page is intentionally left blank



Side Elevation (2)
View from Church Boundary
North West Elevation

'Proposed Scheme'

SITE BOUNDARY

SITE BOUNDARY



Front Elevation (1)
Street Scene (Indicative Ground Line)
North East Elevation

'No.52'

'Access Road'

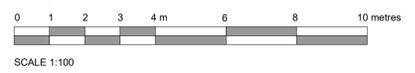
'Proposed Scheme'

'Access'

'Church'

SITE BOUNDARY

SITE BOUNDARY



Rev. C	Date 10.11.20	Detail Front Elevation Changes
Status PLANNING APPLICATION		
 ASCOT DESIGN Timeless architecture <small>Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com www.ascotdesign.com</small>		
Client MCCARTHY & STONE		
Project Title 54-58 READING ROAD, WOKINGHAM, RG41 1EH		
Drawing Title PROPOSED ELEVATIONS FRONT AND SIDE (SHEET 1)		
Scale 1:100 @ A1	Date FEB 20'	Drawn SC
Drawing No. 19 - 2923 - 203	Rev. C	
<small>The copyright in this document and design is confidential to and the property of Ascot Design Limited</small>		

This page is intentionally left blank



Side Elevation (3)
View from Access Road (Indicative Ground Line)
South East Elevation

127

SITE BOUNDARY

'Proposed Scheme'

'Access Road'

SITE BOUNDARY



Rear Elevation (4)
View From Car Park
South West Elevation

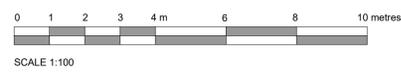
'No.60'

SITE BOUNDARY

'Proposed Scheme'

'No.52' / 'Car Park To Rear'

SITE BOUNDARY



Rev	B	Date	10.11.20	Detail	Front Elevation Changes
-----	---	------	----------	--------	-------------------------

Status
PLANNING APPLICATION



ASCOT DESIGN
Timeless architecture

Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY
Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com
www.ascotdesign.com

Client
MCCARTHY & STONE

Project Title
**54-58 READING ROAD,
WOKINGHAM, RG41 1EH**

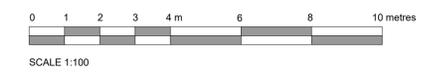
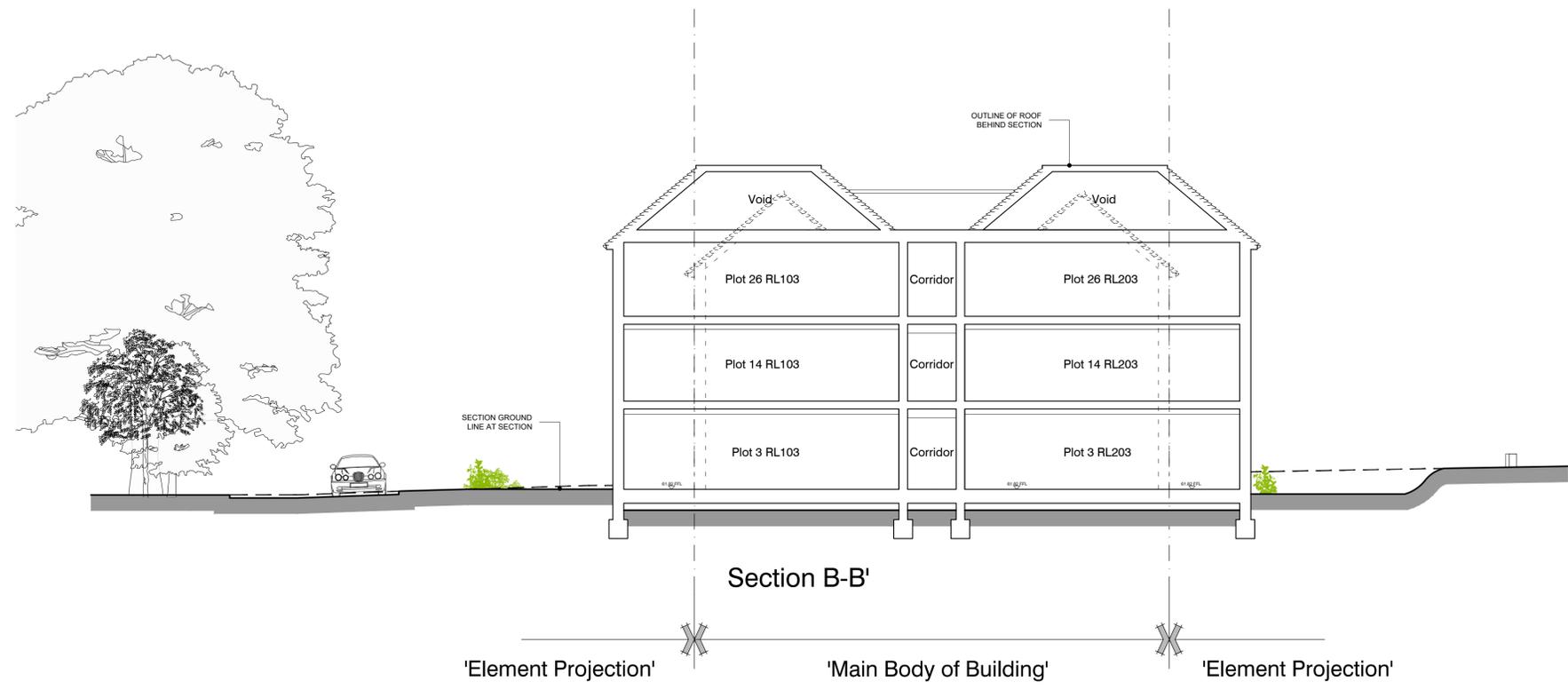
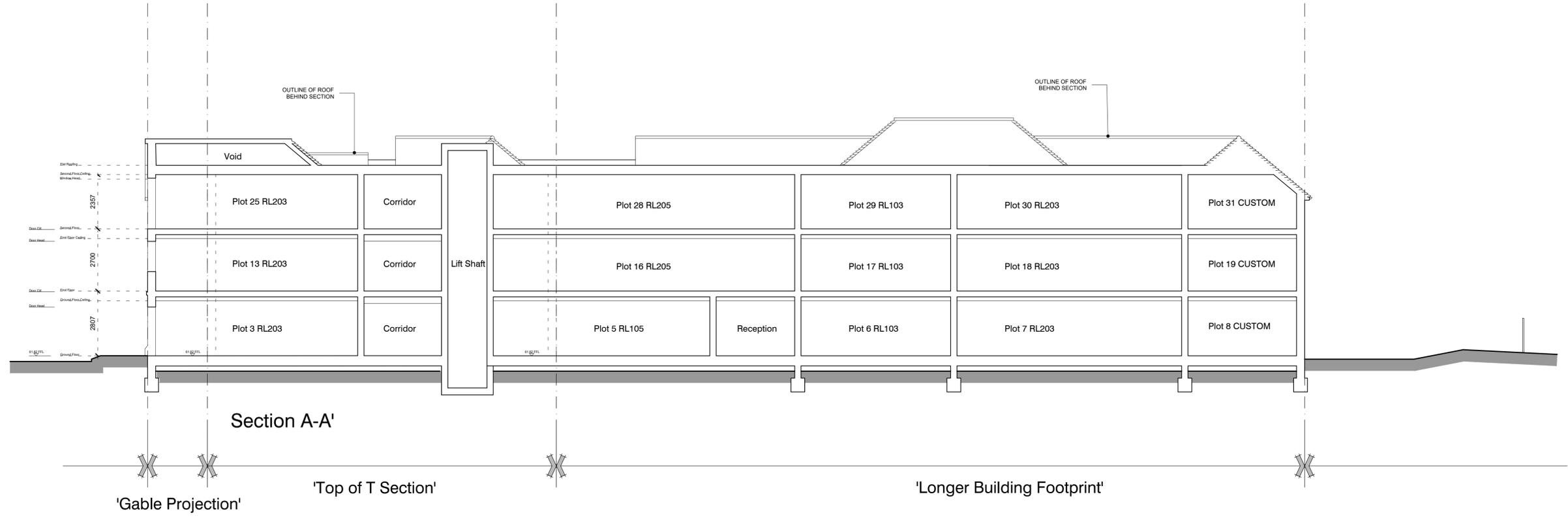
Drawing Title
**PROPOSED ELEVATIONS
REAR AND SIDE (SHEET 2)**

Scale	1:100 @ A1	Date	FEB 20'	Drawn	SC
-------	------------	------	---------	-------	----

Drawing No.	19 - 2923 - 204	Rev.	B
-------------	-----------------	------	---

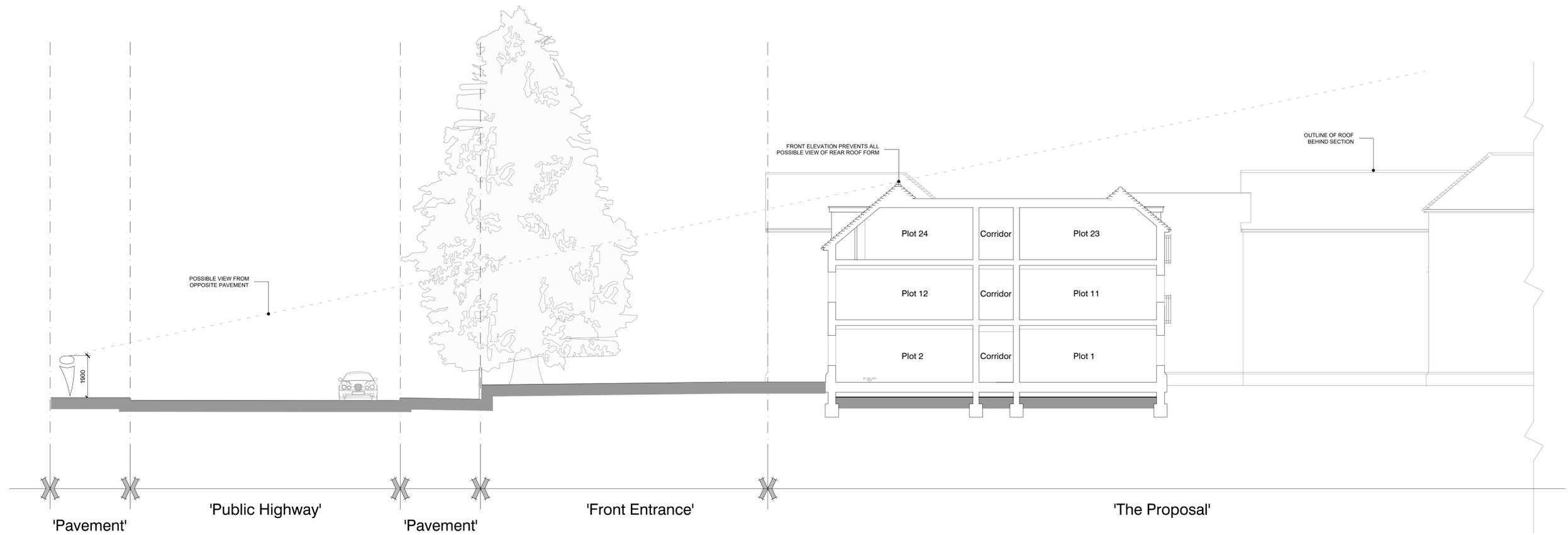
The copyright in this document and design is confidential to and the property of Ascot Design Limited

This page is intentionally left blank



Rev: A	Date: 10.11.20	Detail: Front Elevation Changes
Status: PLANNING APPLICATION		
 ASCOT DESIGN Timeless architecture <small>Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com www.ascotdesign.com</small>		
Client: MCCARTHY & STONE		
Project Title: 54-58 READING ROAD, WOKINGHAM, RG41 1EH		
Drawing Title: PROPOSED SECTIONS A & B		
Scale: 1:100 @ A1	Date: FEB 20'	Drawn: SC
Drawing No: 19 - 2923 - 205	Rev: A	
The copyright in this document and design is confidential to and the property of Ascot Design Limited		

This page is intentionally left blank



Rev.	A	Date	10.11.20	Detail	Front Gable Changes
------	---	------	----------	--------	---------------------

Status **PLANNING APPLICATION**



ASCOT DESIGN
Timeless architecture

Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY
Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com
www.ascotdesign.com

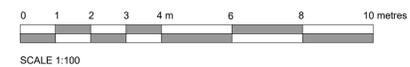
Client **MCCARTHY & STONE**

Project Title **54-58 READING ROAD,
WOKINGHAM, RG41 1EH**

Drawing Title **PROPOSED STREET SECTION**

Scale	1:100 @ A1	Date	FEB 20'	Drawn	SC
-------	------------	------	---------	-------	----

Drawing No.	19 - 2923 - 206	Rev.	A
-------------	-----------------	------	---



This page is intentionally left blank



WITH FOREGROUND CONTEXT
Proposed Street Scene
 North East Elevation

'No.52'



'Access Road'

SITE BOUNDARY

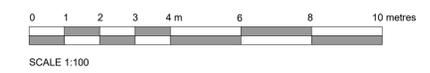
'Proposed Scheme'



SITE BOUNDARY



Existing Street Scene
 North East Elevation



Rev	B	Date	10.11.20	Detail	Front elevation amended
-----	---	------	----------	--------	-------------------------

Status **PLANNING APPLICATION**



ASCOT DESIGN
 Timeless architecture

Ascot Design Ltd, Berkshire House 39-51 High Street, Ascot, Berkshire, SL5 7HY
 Tel: 01344 299330 Fax: 01344 299331 Email: info@ascotdesign.com
 www.ascotdesign.com

Client **McCARTHY & STONE**

Project Title **54-58 READING ROAD,
 WOKINGHAM, RG41 1EH**

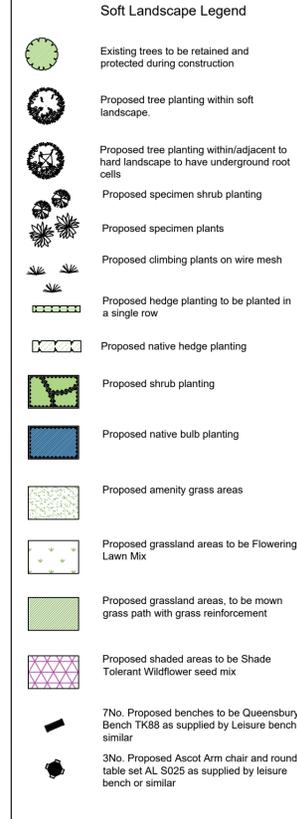
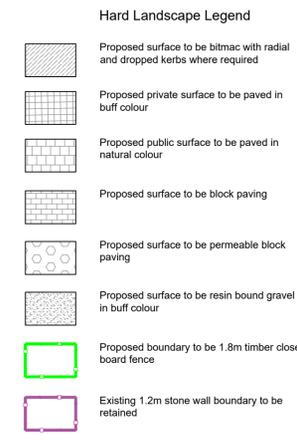
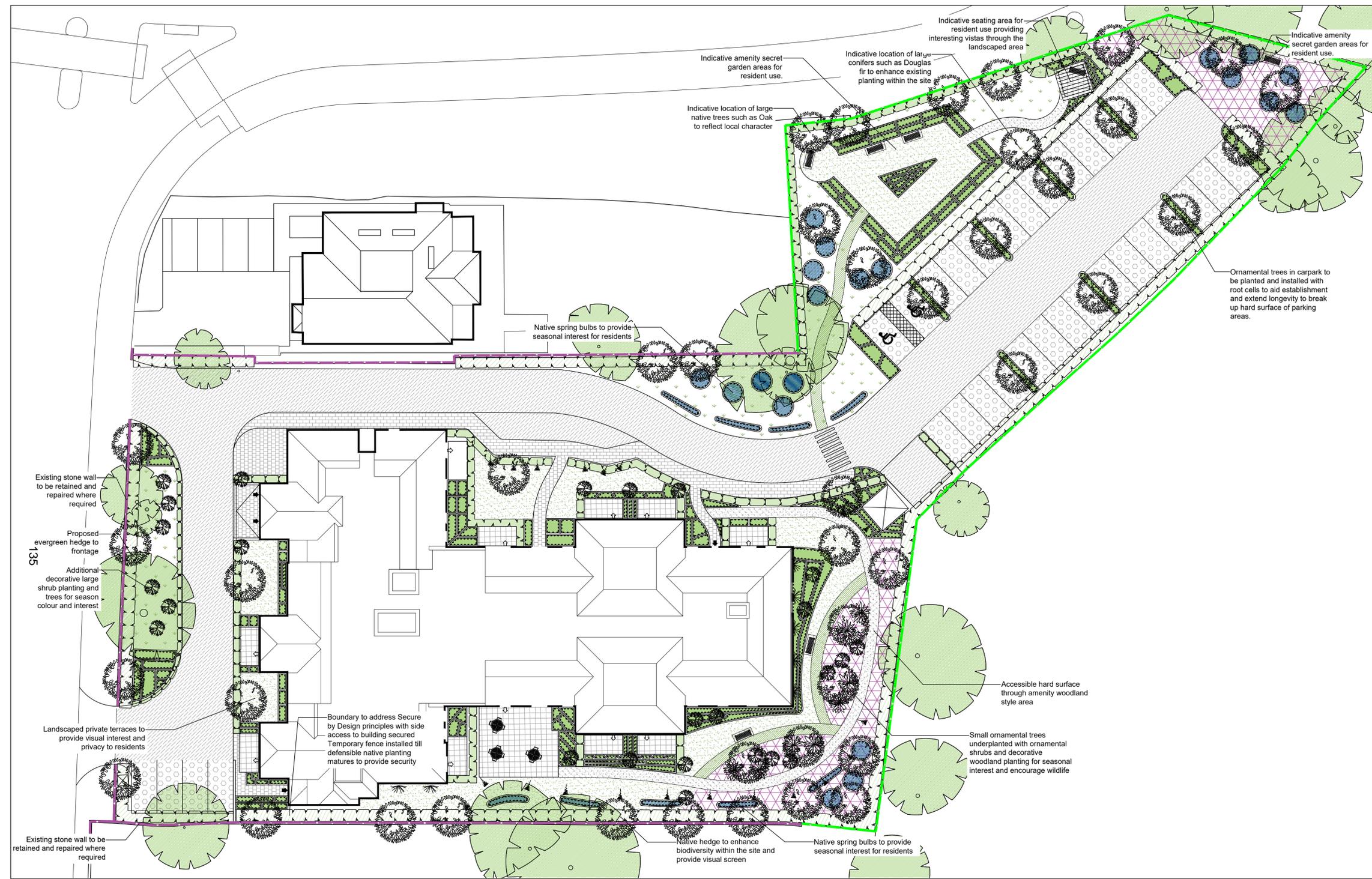
Drawing Title **STREET SCENE
 COMPARISON**

Scale	1:100 @ A1	Date	FEB 20'	Drawn	SC
-------	------------	------	---------	-------	----

Drawing No.	19-2923-208	Rev.	B
-------------	-------------	------	---

The copyright in this document and design is confidential to and the property of Ascot Design Limited

This page is intentionally left blank



Indicative Planting Schedule

Species Name
Amelanchier 'Ballarina'
Salix caprea
Salix virens
Retula pubescens
Alnus glutinosa 'Imperialis'
Prunus 'Accolade'
Scorbus ana 'Lutescens'
Acet. calamagrostis
Quercus robur
Acet. pinnatifida 'Crimson Sentry'
Prunus serrula tibetica
Retula villosa jacquemontii
Viburnum litchinoides
Prunus 'Umineko'
Scorbus vilmosa
Flaxus baccata

Species Name
Phamaecyparis law. 'Pembury Blue'
Pinus sylvestris
Pseudotsuga menziesii

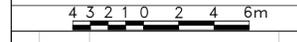
Species Name
Berberis darwinii
Berberis thunbergii 'Bagatelle'
Brachyglottis 'Sunshine'
Calluna vulgaris
Coronilla alba 'Worcester Gold'
Cornus sanguinea
Cornus alba 'Sibirica Variegata'
Cornus sanguinea
Cornus sanguinea 'Midwinter Fire'
Cornus avellana
Cornus coccinea 'Rubrifolia'
Coronilla 'Sora Beauty'
Forsythia ovata
Fuchsia 'Mrs Popple'
Genista vicia
Hebe 'Coral Pink'
Hebe 'Marjorie'
Hebe 'Nicola's Bush'
Hebe 'White Gem'
Hebe 'Franciscana blue gem'
Hedera 'Erecta'
Hydrangea moderatum
tax coccinea
Lavandula angustifolia 'Munstead'
Lavandula angustifolia 'Twickel Purple'
Lastrum ovalifolium
Phormium 'Sage'
Prunus 'Surdovner'
Pieris 'Forest Flame'
Prunus japonica 'Christmas Cheer'
Rosa 'Ballarina'
Rhododendron officinale 'Miss Jessop's Upright'
Sarcococca 'Black Beauty'
Sarcococca hookeriana
Skimmia japonica 'Fraser's'
Skimmia japonica 'Fruita Alba'
Skimmia japonica 'Fruita Alba'
Skimmia japonica 'Veitchii'
Skimmia japonica 'revesiana'
Sorbus japonica 'Snowmound'
Symphoricarpos chenaultii 'Hancock'
Symna meyeri 'Palibin'
Ulex europaeus
Viburnum davidii
Viburnum lentana
Viburnum opulus

Species Name
Astrantia major 'Ruby Wedding'
Geranium cordifolia 'Purpurea'
Geranium purpurascens
Geranium oxianum 'Old Rose'
Impatiens nivalis
Linum catharticum 'Herrenhausen'
Panicum 'Historia Superba'
Scilla 'Herbstfreude'

Species Name
Galanthus nivalis
Hyacinthoides non-scripta

Species Name
Dryopteris affinis

Species Name
Carax comans 'Bronze Perfection'
Miscanthus sinensis
Miscanthus sinensis 'Kleine Fontaine'
Miscanthus sinensis 'Morning Light'
Stipa gigantea



Rev	Date	Details	Drawn
A	12.11.20	LPA comments	JNT

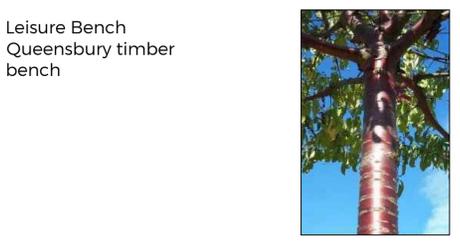


HEAD OFFICE
 Rodbourne Rail Business Centre, Grange Lane,
 Malmesbury, SN16 0ES
 Tel: 01666 825646
 Courtyard House, Mill Lane, Godalming, GU7 1EY
 Tel: 01483 425714
 Suite 6, Crescent House, Yonge Close, Eastleigh, SO50 9SX
 Tel: 02382 026300
 email: mail@acdenv.co.uk
 www.acdenvironmental.co.uk



scheme: Reading Road (54-58),
 Wokingham
 client: McCarthy & Stone
 drawing: Stage 3 Landscape Strategy
 date: Feb. 2020
 scale: 1:200@A1
 drawing no: MCS22788-10A
 drawn: JNT checked: JS

Copyright of ACD Environmental Ltd. All rights described in Chapter IV of the Copyright, Designs and Patents Act 1988 have been generally asserted. 2011. Copyright of this plan remains with ACD until all fees have been paid in full.



Marshalls conservation textured paving in buff or similar for private areas

Leisure Bench Queensbury timber bench

Prunus serrula tibetica chosen for year round interest

Forsythia ovata chosen for seasonal colour

Cornus sibirica alba chosen for year round interest and attracts wildlife

Marshalls conservation textured paving in natural or similar for public areas

Leisure Bench Ascot arm chair and round table set

Alnus glutinosa 'Imperialis' chosen for interesting foliage

Lavandula 'Twickel Purple' chosen for year round colour and attracts wildlife

Cotinus coggrygia 'Rubrifolius' chosen for seasonal colour.

This page is intentionally left blank

54-58 READING ROAD,
WOKINGHAM, BERKSHIRE, UK

ACCURATE IMAGE PRODUCTION DOCUMENT

VR 12046
Issue D

METHOD STATEMENT

ACCURATE IMAGE PRODUCTION PROCEDURE

Our Method Statement follows "Guidelines for Landscape & Visual Impact Assessment, 3rd edition 2013" and Advice Notes from 'Photography and Photomontage in Landscape and Visual Impact Assessment', published by The Landscape Institute and IEMA and the 'London View Management Framework (LVMF) Supplementary Planning Guidance (2012)', produced by Greater London Authority.

GENERAL NOTE

Production notes specific to each viewpoint are included on the viewpoint record sheets.

VIEWPOINT LOCATION

Establish and agree each viewpoint location, Field of View (FOV) and optical axis with all relevant parties.

Aim to produce high resolution imagery to match the perspective and detail of the same view on site.

Wherever possible design proposal should be close to the optical axis of the photograph to most closely match our perception of shape and to reduce parallax errors towards the edges of the background photographs.

Choose a lens FOV with a balance between detail and FOV to show the design proposal within meaningful context.

Determine whether a single frame image will be suitable or whether a panorama view will be required.

Camera lens FOV to match as closely as possible the central field of view of the human eye.

Wherever possible use a consistent FOV (and therefore Viewing Distance) between multiple viewpoints.

Shoot preliminary photographs from the agreed locations.

Record each viewpoint location on a current Ordnance Survey site plan.

Issue preliminary photographs and site plan to all relevant parties for approval and confirmation to proceed prior to commissioning professional photography and survey.

VIEWPOINT BACKGROUND PHOTOGRAPHY

Commission professional photographer with experience of photographing for accurate architectural image production.

Shoot high resolution digital background photographs from agreed locations under direction of visualisation consultant.

If necessary, obtain landowner's permission and photography licence to photograph from each viewpoint.

Set centre of camera lens to a height of 1600mm above ground level, or if this is not practical, set height as close as possible and record height of centre of lens above ground.

Set camera level on a stable tripod. Use spirit level mounted on the tripod or camera's hot-shoe to ensure optical axis of view is horizontal and in the centre of the photograph.

If vertical rise is necessary to fit the top of the existing context, avoid converging verticals by using a tilt shift lens. Indicate optical axis on final output.

Record type of camera, lens and focal length for each photograph.

Record date, time and weather conditions for each photograph.

Record camera location on a current Ordnance Survey site plan.

If possible, mark position of centre of tripod on ground with chalk or spot marker paint for land surveyor to locate.

Take concise photographic record of camera and tripod in position for each viewpoint location.

If shooting panorama view which require stitching, reduce parallax errors by using a panoramic tripod head to ensure lens nodal point is set to centre of rotation (axis) of tripod.

Record degrees of rotation for multiple photographs.

Obtain DCIM information for each photograph.

Supply final background photographs as high resolution digital image files.

Complete background photography from agreed viewpoint locations prior to commissioning professional land surveyor.

VIEWPOINT SURVEY INFORMATION

Commission professional land surveyor with experience of surveying for accurate architectural image production.

Use final background photographs to determine Surrounding Feature Survey Points necessary to achieve accurate camera match within 3D model.

Choose a suitable number of Surrounding Feature Survey Points to enable accurate camera matching.

Choose a selection of close, mid and long range Surrounding Feature Survey Points within central FOV to reduce parallax errors towards the edges of the background photographs.

If the site lacks suitable surrounding features, carefully position, photograph in location, and survey, Ranging Poles or small wooden stakes placed in ground.

Issue land surveyor with Surrounding Feature Survey Points sheet indicating survey points and camera locations requirements for each viewpoint.

Issue land surveyor with Survey Points Record sheet for each viewpoint.

Survey and supply final land survey information as Easting, Northing and Height AOD relative to National Grid coordinates and Ordnance Survey heights in text file and 3D AutoCAD digital format.

3D MODEL

Create accurate 3D model of proposals to required level of detail from accurate and final architectural 2D drawings supplied by designer.

Position 3D model within 3D site model relative to Ordnance Survey grid coordinates using accurate site plan information supplied by designer.

The position of any landscape elements have been produced as per the landscape proposals, however, heights, foliage and girth are shown indicatively as if planted for a number of years.

CAMERA MATCHING

Import background photographs into 3D model.

If required, stitch multiple photographs together to form a higher resolution composite image. Note stitch points on final output.

Import land survey Surrounding Feature Survey Points into 3D model

Position virtual camera in 3D model using land survey camera location information.

Set FOV for each virtual camera to match the recorded real-world camera SLR equivalent FOV.

Adjust virtual camera target to align 3D Surrounding Feature Survey Points with corresponding real world features visible in background photograph.

LIGHTING & MATERIALS

Using the latest 3D and rendering software, reproduce the sun's position, thus simulating shadowing onto the computer generated model of the proposed development.

The restraint with the current software is that it is difficult to accurately simulate this light interaction with the building materials. A more 'artistic' interpretation must therefore be utilised. This involves the use of reference photographs of the materials within similar environmental conditions to help fine tune the overall image.

RENDERING

A computer generated image of the proposed building or 'render' is then produced by the software. Use Photoshop, to overlay the CG image onto the base line of the photograph.

Any foreground elements in between the proposal and the camera position which may occlude the development are then montaged over the proposed development to give a true visual representation.

ACCURATE IMAGE PRODUCTION TEAM

AVR Project Lead, Photography, Camera Match, Modelling, Lighting, Image and Report Production:

Virtual Resolution
1st Floor, Unit 7 Wellington Mills
Quebec Street
Elland
HX5 9AS

Surveying:

Absolute Survey Ltd
New Causeway
Reigate
RH2 7PE

REAL WORLD SCALE (VIEWING DISTANCE)

24mm lens focal length - 73 degree FOV.
Print A3 @ 100% - Viewing Distance approx. 280mm

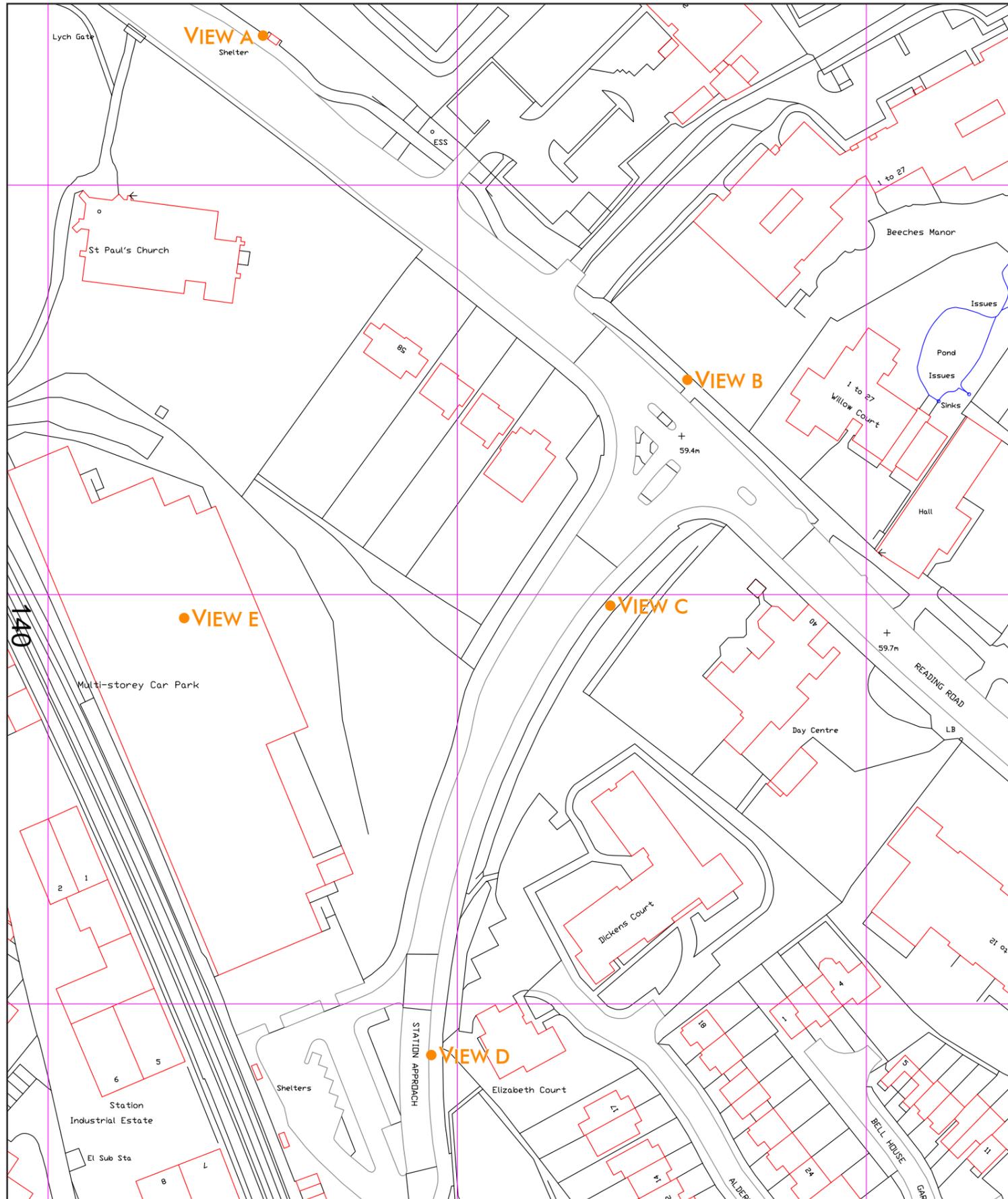
54-58 READING ROAD,
WOKINGHAM, BERKSHIRE, UK

ACCURATE IMAGE PRODUCTION DOCUMENT

CONTENTS

2	METHOD STATEMENT
4	OVERALL SITE PLAN
5	INTRODUCTION
6	VIEW A - VIEW FROM BUS STOP ON READING ROAD LOOKING SOUTHEAST.
12	VIEW B - LOOKING WEST FROM ZEBRA CROSSING AT JUNCTION OF READING ROAD AND STATION APPROACH.
18	VIEW C - LOOKING NORTHWEST FROM STATION APPROACH, ADJACENT TO #52.
24	VIEW D - NORTHERN VIEW ON STATION APPROACH, OPPOSITE CAR PARK ENTRANCE.
30	VIEW E - VIEW FROM UPPER LEVEL OF STATION CAR PARK, LOOKING NORTHEAST.

OVERALL SITE PLAN



SURVEY	DESCRIPTION
VIEW A	VIEW FROM BUS STOP ON READING ROAD LOOKING SOUTHEAST.
VIEW B	LOOKING WEST FROM ZEBRA CROSSING AT JUNCTION OF READING ROAD AND STATION APPROACH.
VIEW C	LOOKING NORTHWEST FROM STATION APPROACH, ADJACENT TO #52.
VIEW D	NORTHERN VIEW ON STATION APPROACH, OPPOSITE CAR PARK ENTRANCE.
VIEW E	VIEW FROM UPPER LEVEL OF STATION CAR PARK, LOOKING NORTHEAST.

INTRODUCTION

VIEWPOINT RECORD SHEETS

Virtual Resolution (VR) were commissioned by *McCarthy & Stone Ltd* in August 2019 to help produce dimensionally accurate photomontage images of the proposed development of 54-58 Reading Road, Wokingham, Berkshire, UK.

The photography was carried out and surveying completed in September 2019. Camera matches were then created and quantified by VR from that data. In the same month a design had been reached and CAD sent to VR for final modelling and image montage work.

All drawn and digital information regarding the building and landscape design have been supplied and completed by *Ascot Design Ltd*.

All images should be read in conjunction with the architects' plans and elevations. The views have been produced in order to illustrate the likely impact of the scheme on the wider setting of the site.

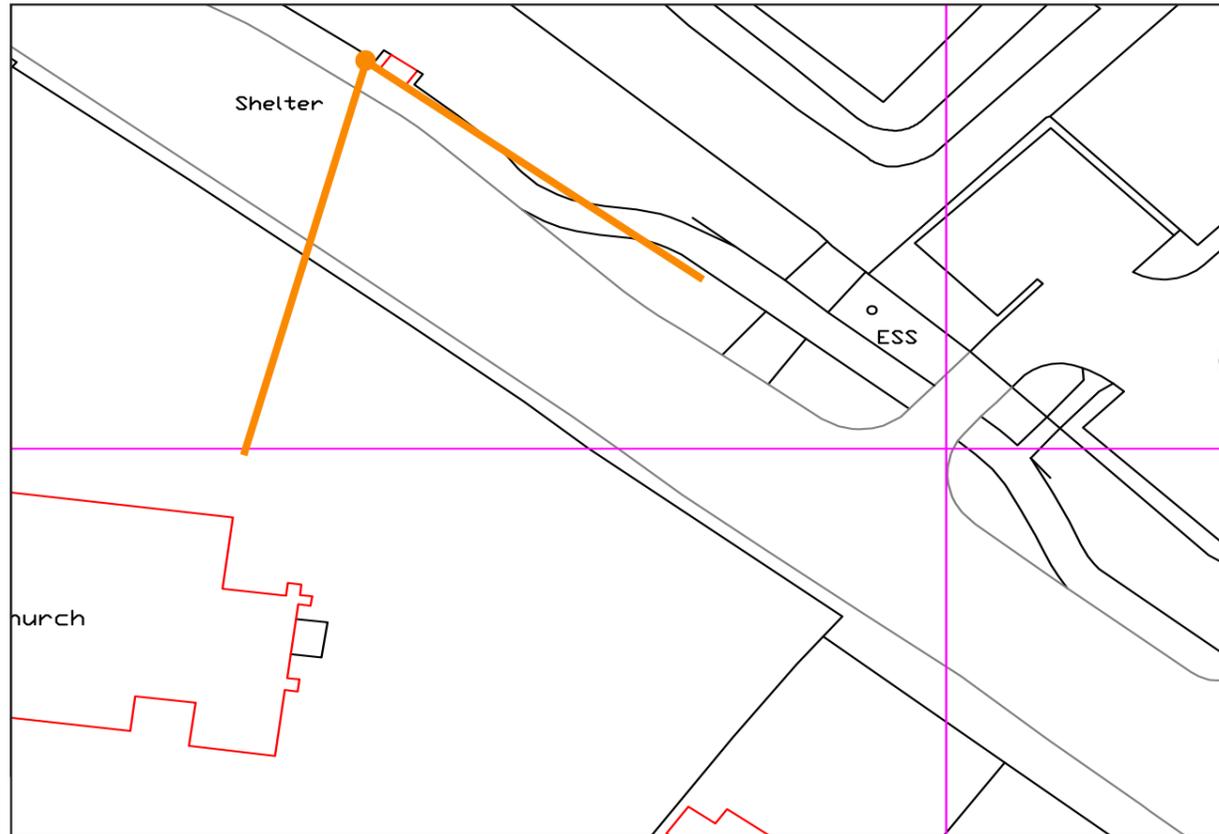
In November 2019, the building adopted re-designed elements and the images shown within were updated to reflect this new design.

February 2020, VR were commissioned to update all views to a revised 'C' building and show all views as AVR3 level.

March 2020, VR amended to latest elevations given on 18th March 2020, effecting NW and SW elevations. Tree movements have also been adjusted to the rear car parking area.

VIEW A - VIEW FROM BUS STOP ON READING ROAD LOOKING SOUTHEAST.

CAMERA RECORD & BACKGROUND & PHOTOGRAPHY



SITE PLAN SHOWING CAMERA LOCATION



BACKGROUND PHOTOGRAPH | CENTRE

CAMERA	Canon EOS 5D Mkiii	DATE & TIME	19/09/2019 08:32
LENS	Canon 24mm f1.4L Mk2	WEATHER CONDITIONS	Clear skies, Sunny, Good Visibility
LENS HEIGHT	1.600m (AOD)	NATIONAL GRID REF	480552.481 E 169036.505 N
PRODUCTION NOTES	Viewpoint marked on site with a Hilti nail and white paint marker.		

PRODUCTION DETAILS



CAMERA SURVEY MARKER



CAMERA LENS HEIGHT



CAMERA IN POSITION



CAMERA IN POSITION

SURROUNDING FEATURE SURVEY POINTS RECORD

SURVEY POINT	DESCRIPTION	EASTING (X)	NORTHING (Y)	HEIGHT AOD (Z)
Camera Point	Viewpoint Camera Position 1	480552.481	169036.505	63.194
102	Corner of top roof	480546.19	168982.48	77.19
103	Corner of roof	480546.70	168986.55	70.85
109	Corner of render	480539.97	168994.10	68.18
112	Corner of roof	480577.81	168958.27	67.46
114	Corner of chimney	480589.31	168957.74	71.05
115	Corner of chimney	480589.80	168958.41	71.04
116	Corner of traffic lights internal frame	480654.62	168950.31	63.47
117	Corner of traffic lights internal frame	480654.61	168950.32	62.40
119	Corner of sign road internal frame	480573.50	169019.09	66.20
120	Corner of sign road internal frame	480571.90	169017.35	66.21
122	Corner of road markings	480574.98	169009.68	62.36
124	Corner of grave	480586.43	168986.46	62.78
125	Corner of grave	480586.12	168986.04	62.77
128	Corner of grave	480557.15	168997.19	63.57
129	Corner of grave	480554.40	168978.21	63.54
130	Corner of road markings	480553.71	169024.45	62.90
131	Corner of road markings	480550.68	169026.59	62.96

SURROUNDING FEATURE SURVEY POINTS ON PHOTOGRAPH

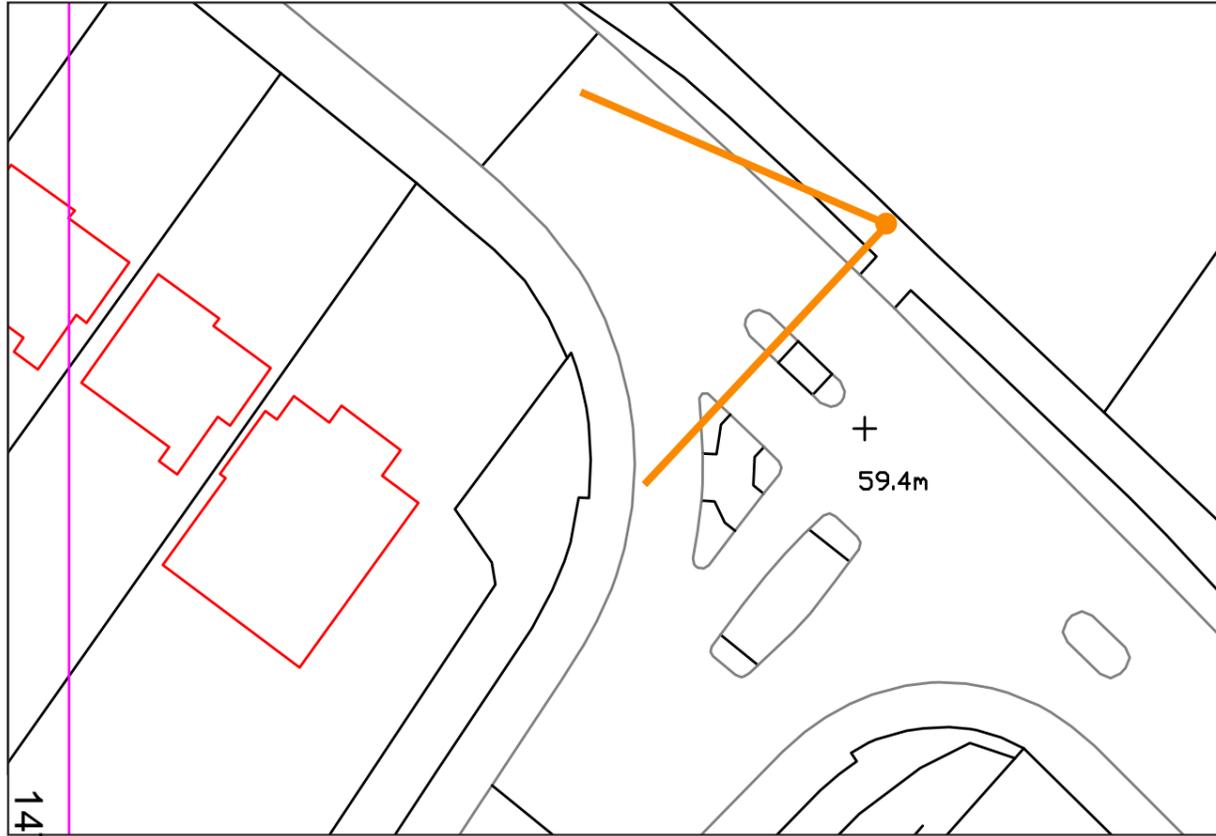






146

VIEW B - LOOKING WEST FROM CROSSING AT JUNCTION OF READING ROAD AND STATION APPROACH. CAMERA RECORD & BACKGROUND & PHOTOGRAPHY



147
SITE PLAN SHOWING CAMERA LOCATION



BACKGROUND PHOTOGRAPH | CENTRE

CAMERA	Canon EOS 5D Mkiii	DATE & TIME	19/09/2019 08:41
LENS	Canon 24mm f1.4L Mk2	WEATHER CONDITIONS	Clear skies, Sunny, Good Visibility
LENS HEIGHT	1.600m (AOD)	NATIONAL GRID REF	480656.234 E 168952.573 N
PRODUCTION NOTES	Viewpoint marked on site with a Hilti nail and white marker paint.		

PRODUCTION DETAILS



CAMERA SURVEY MARKER



CAMERA LENS HEIGHT



CAMERA IN POSITION



CAMERA IN POSITION

SURROUNDING FEATURE SURVEY POINTS RECORD

SURVEY POINT	DESCRIPTION	EASTING (X)	NORTHING (Y)	HEIGHT AOD (Z)
Camera Point	Viewpoint Camera Position 2	480656.234	168952.573	59.597
201	Corner of glass on window	480620.94	168931.06	68.98
202	Corner of glass on window	480620.11	168929.93	68.99
203	Corner of window opening	480619.31	168939.09	69.11
204	Corner of window opening	480618.78	168939.48	69.11
207	Corner of glass on window	480610.83	168945.07	66.66
208	Corner of glass on window	480609.98	168945.71	66.66
209	Corner of white on sign	480591.44	168983.86	64.84
210	Corner of white on sign	480591.26	168983.58	64.27
211	Corner of white on sign	480591.63	168984.12	64.27
212	Corner of horizontal bar on sign	480649.47	168954.94	62.28
213	Corner of horizontal bar on sign	480649.46	168954.95	62.08
214	Corner of road markings	480652.10	168951.17	59.76
215	Corner of road markings	480648.09	168946.99	59.93
216	Corner of road markings	480645.30	168949.59	59.98
217	Corner of brick wall	480625.03	168954.79	60.36
219	Corner of brick wall	480624.94	168954.64	60.83
221	Corner of arrow on sign	480635.43	168934.50	62.05

SURROUNDING FEATURE SURVEY POINTS ON PHOTOGRAPH



150

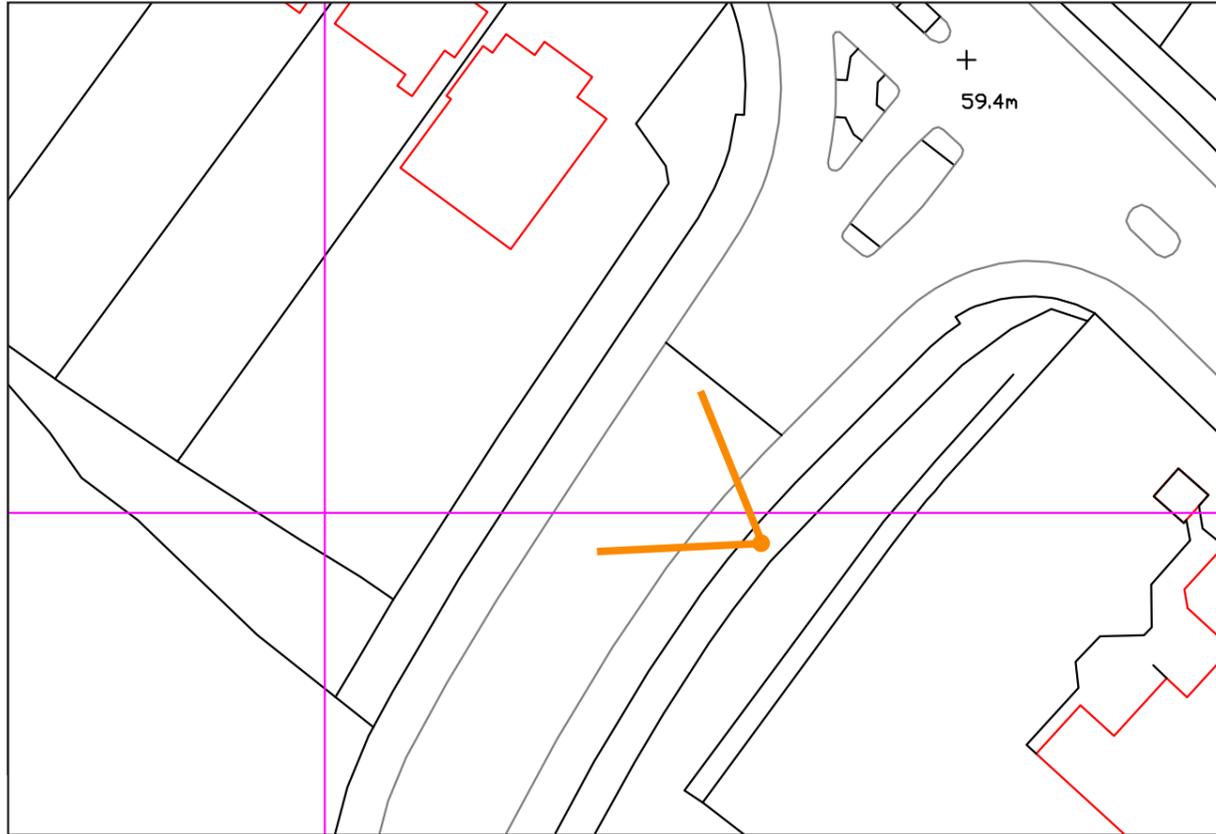




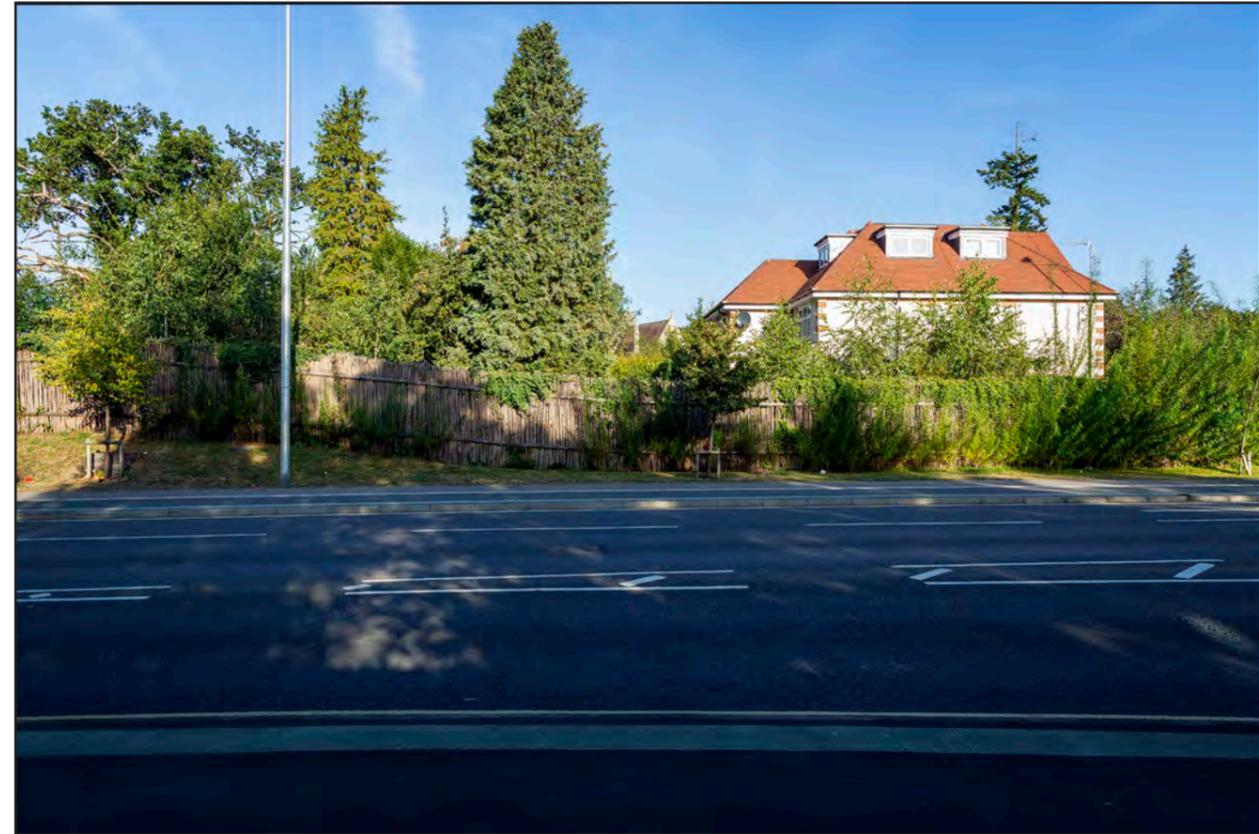
151

VIEW C - LOOKING NORTHWEST FROM STATION APPROACH, ADJACENT TO #52.

CAMERA RECORD & BACKGROUND & PHOTOGRAPHY



SITE PLAN SHOWING CAMERA LOCATION



BACKGROUND PHOTOGRAPH | CENTRE

CAMERA	Canon EOS 5D Mkiii	DATE & TIME	19/09/2019 08:46
LENS	Canon 24mm f1.4L Mk2	WEATHER CONDITIONS	Clear skies, Sunny, Good Visibility
LENS HEIGHT	1.600m (AOD)	NATIONAL GRID REF	480637.319 E 168897.475 N
PRODUCTION NOTES	Viewpoint marked on site grass with white marker paint. Applied slight camera roll correction to photograph of 0.1 degrees anti-clockwise.		

PRODUCTION DETAILS



CAMERA SURVEY MARKER



CAMERA LENS HEIGHT



CAMERA IN POSITION



CAMERA IN POSITION

SURROUNDING FEATURE SURVEY POINTS RECORD

SURVEY POINT	DESCRIPTION	EASTING (X)	NORTHING (Y)	HEIGHT AOD (Z)
Camera Point	Viewpoint Camera Position 3	480637.319	168897.475	60.005
301	Corner of window glass frame	480620.92	168931.06	68.97
302	Corner of window glass frame	480620.92	168931.06	68.38
304	Corner of window glass frame	480619.10	168928.58	68.38
307	Corner of roof	480624.65	168933.14	66.66
310	Corner of roof	480617.48	168923.32	66.63
311	Corner of dormer roof	480615.71	168926.61	69.47
312	Corner of dormer roof	480618.20	168927.10	69.50
315	Corner of render	480546.54	168983.11	75.77
316	Corner of render	480546.35	168981.74	75.78
320	Corner of timber step	480613.50	168900.49	60.39
321	Corner of timber step	480613.09	168900.75	60.74
322	Corner of timber step	480613.55	168901.68	60.83
323	Corner of top middle timber post	480621.79	168912.94	60.92
324	Corner of top middle timber post	480622.12	168913.36	60.91
325	Corner of road markings	480634.22	168908.07	59.91
328	Corner of road markings	480632.40	168905.76	59.91
329	Corner of road markings	480630.52	168903.78	59.91
330	Corner of road markings	480628.77	168901.20	59.90

SURROUNDING FEATURE SURVEY POINTS ON PHOTOGRAPH

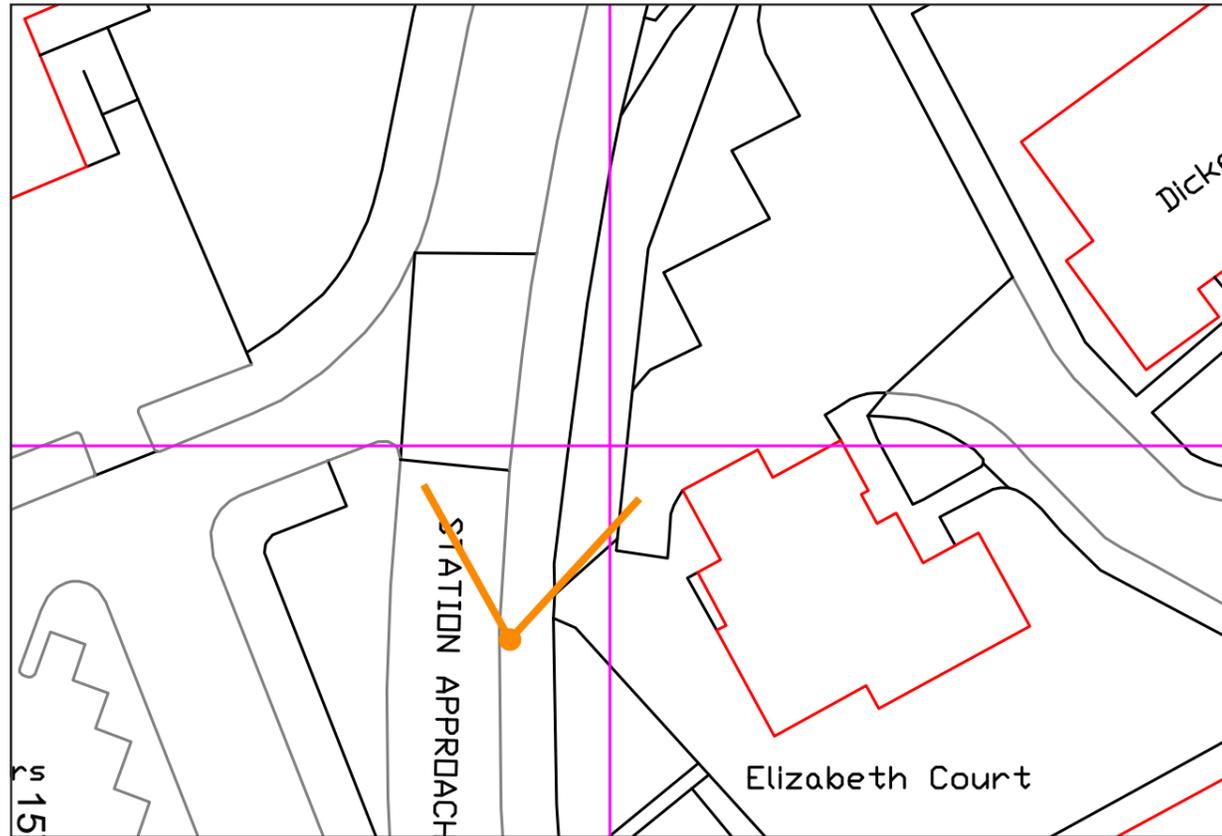


155





156



SITE PLAN SHOWING CAMERA LOCATION



BACKGROUND PHOTOGRAPH | CENTRE

CAMERA	Canon EOS 5D Mkiii	DATE & TIME	19/09/2019 14:56
LENS	Canon 24mm f1.4L Mk2	WEATHER CONDITIONS	Clear skies, Sunny, Good Visibility
LENS HEIGHT	1.600m (AOD)	NATIONAL GRID REF	480593.626 E 168787.733 N
PRODUCTION NOTES	Viewpoint marked on site with a Hilti nail and white marker paint. Applied slight camera roll correction to photograph of 0.5degrees anti-clockwise.		

PRODUCTION DETAILS



CAMERA SURVEY MARKER



CAMERA LENS HEIGHT



CAMERA IN POSITION



CAMERA IN POSITION

SURROUNDING FEATURE SURVEY POINTS RECORD

SURVEY POINT	DESCRIPTION	EASTING (X)	NORTHING (Y)	HEIGHT AOD (Z)
Camera Point	Viewpoint Camera Position 4	480593.626	168787.733	57.086
402	Corner of arrow on sign	480601.60	168876.46	62.59
405	Corner of arrow on sign	480593.41	168847.37	61.27
407	Corner of white on sign	480586.51	168822.23	60.51
408	Corner of white on sign	480586.16	168822.21	61.07
409	Corner of white on sign	480585.84	168822.26	60.49
410	Corner of metal barrier	480574.72	168831.68	60.91
411	Corner of metal barrier	480574.73	168831.68	62.24
413	Bottom corner of sign	480565.57	168828.01	62.40
414	Bottom corner of sign	480565.04	168827.77	62.40
416	Right corner of white vertical bar on sign	480600.79	168820.51	58.70
419	Corner of white road marking	480592.34	168801.90	57.14
420	Corner of white road marking	480591.84	168800.35	57.13
421	Corner of white road marking	480591.68	168801.95	57.16
422	Top corner of column beam	480566.93	168817.64	60.91

SURROUNDING FEATURE SURVEY POINTS ON PHOTOGRAPH





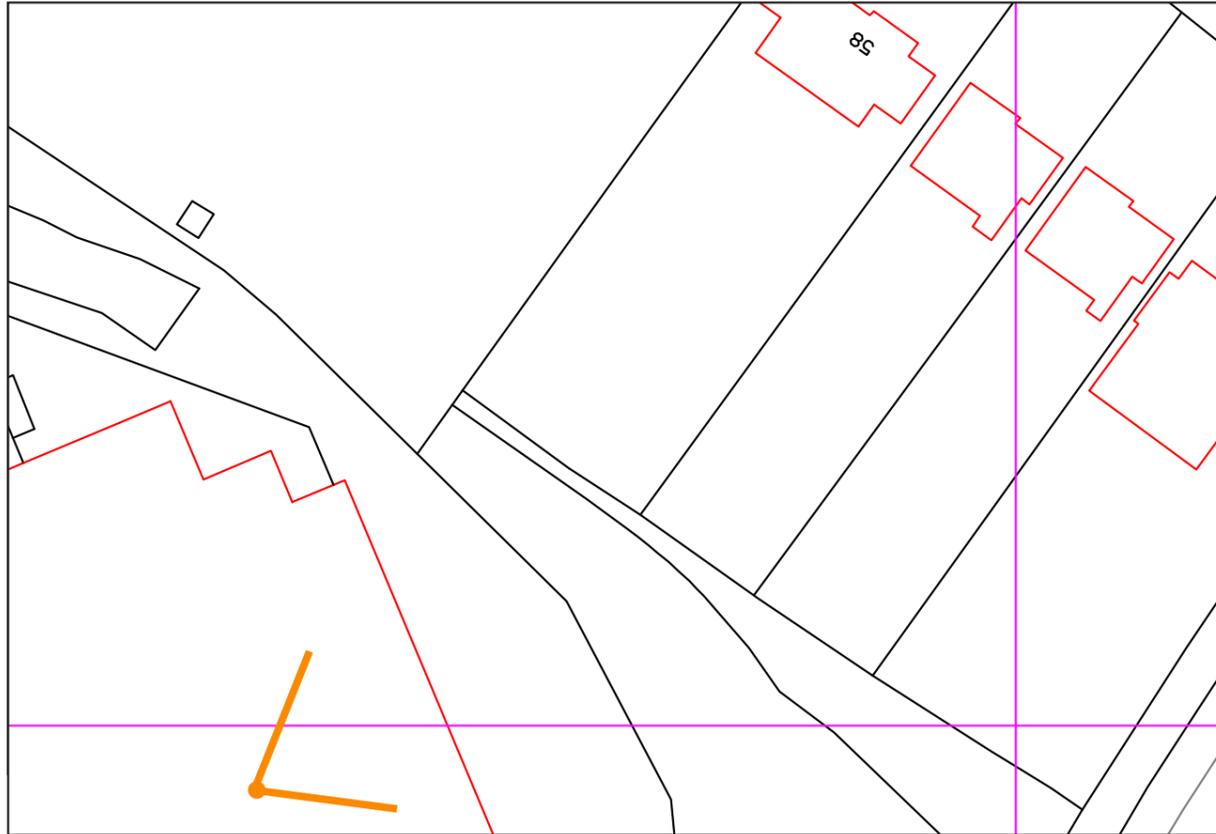
160

161



VIEW E - VIEW FROM UPPER LEVEL OF STATION CAR PARK, LOOKING NORTHEAST.

CAMERA RECORD & BACKGROUND & PHOTOGRAPHY



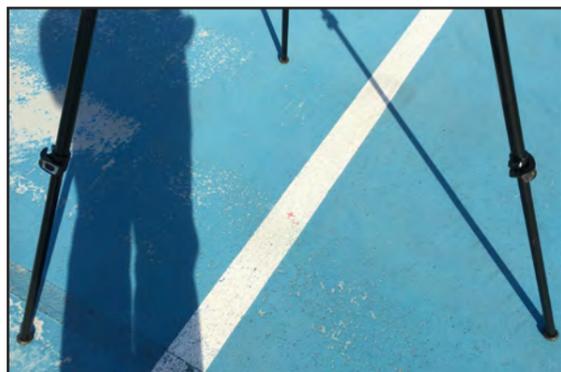
SITE PLAN SHOWING CAMERA LOCATION



BACKGROUND PHOTOGRAPH | CENTRE

CAMERA	Canon EOS 5D Mkiii	DATE & TIME	19/09/2019 09:05
LENS	Canon 24mm f1.4L Mk2	WEATHER CONDITIONS	Clear skies, Sunny, Good Visibility
LENS HEIGHT	1.600m (AOD)	NATIONAL GRID REF	480533.279 E 168894.357 N
PRODUCTION NOTES	Viewpoint marked on site with red marker. Applied slight camera roll correction to photograph of 0.2degrees anti-clockwise.		

PRODUCTION DETAILS



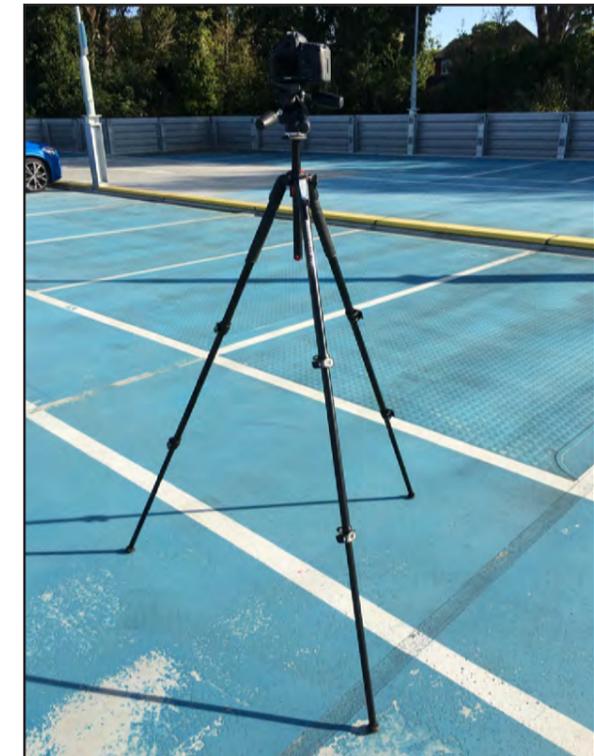
CAMERA SURVEY MARKER



CAMERA LENS HEIGHT



CAMERA IN POSITION



CAMERA IN POSITION

SURROUNDING FEATURE SURVEY POINTS RECORD

SURVEY POINT	DESCRIPTION	EASTING (X)	NORTHING (Y)	HEIGHT AOD (Z)
Camera Point	Viewpoint Camera Position 5	480533.279	168894.357	60.978
501	Corner of metal post	480543.78	168914.36	61.08
502	Corner of metal post	480543.79	168914.39	62.25
506	Corner of window glass frame	480580.36	168962.65	66.80
508	Corner of window glass frame	480579.98	168962.95	65.89
509	Corner of roof	480581.88	168963.79	69.52
511	Corner of chimney	480594.47	168953.98	70.68
512	Corner of post details	480549.49	168900.70	62.19
514	Corner of post details	480549.56	168900.56	61.39
515	Corner of post details	480552.39	168893.78	62.19
517	Corner of post details	480552.45	168893.65	61.39
520	Corner of road markings	480538.80	168896.19	60.94
521	Corner of road markings	480537.87	168898.42	60.95
522	Corner of road markings	480541.05	168900.29	60.97
523	Corner of road markings	480541.69	168898.29	60.97
524	Corner of road markings	480542.61	168897.03	60.97

SURROUNDING FEATURE SURVEY POINTS ON PHOTOGRAPH







166

CONTACT:

VIRTUAL RESOLUTION

1st Floor, Unit 7 Wellington Mills, Quebec Street, Elland, HX5 9AS
T: 01422 260 500 E: info@virtualresolution.co.uk W: www.virtualresolution.co.uk

Virtual Resolution is a trading name of VR Studios Ltd



This page is intentionally left blank

PLANNING REF : 202065
PROPERTY ADDRESS : Town Hall Market Place
: Wokingham
: RG40 1AS
SUBMITTED BY : The Wokingham Town Council P&T Committee
DATE SUBMITTED : 07/10/2020

COMMENTS:

The Committee have no objections.

The Committee were pleased to see that solar panels will be used on the roof.

There are concerns about damage to trees but welcome that the proposal is to replace removed trees with more than those removed with new planting.

This page is intentionally left blank

Agenda Item 7.

Application Number	Expiry Date	Parish	Ward
203460	17/05/2021	Wokingham	Wescott

Applicant	Ms Sarah Cleaver
Site Address	Frog Hall, Frog Hall Drive, Wokingham, RG40 2LF
Proposal	Full application for the erection of fencing and hardstanding to form a bin store to serve the existing flats (Retrospective).
Type	Full planning application
Officer	Adriana Gonzalez
Reason for determination by committee	The application has been listed by Ward Member Cllr Maria Gee on the following grounds: <ul style="list-style-type: none"> - Highway and safety grounds - Position of the bin store impeding right of way of residents

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 June 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The application is before Committee as it has been listed by the Wescott Ward Member, Councillor Maria Gee.</p> <p>The application site is within a major development location in the settlement of Wokingham and within the grounds of Frog Hall, a converted block of flats. The surrounding area is characterised by mainly two storey residential dwellings on individual plots.</p> <p>The application is for a full application for the erection of fencing and hardstanding to form a bin store area to serve the existing flats of Frog Hall. The bin store area is located within part of the private residential amenity of the block of flats, towards the front north-eastern corner of the site. The application is retrospective as the development was completed in May 2020 as stated within the application form.</p> <p>The report concludes that no part of the development results in any harmful impact on the character of the area, amenity of neighbouring occupiers, highway safety or landscaping. Paragraphs 1-26 provide further details to these material considerations. It is recommended that this application is approved as it accords with the NPPF and Wokingham Development Plan Policies.</p>

PLANNING STATUS
<ul style="list-style-type: none"> • Major Development Location – Wokingham • Tree Preservation Order – TPO-0004-1960 • Bat Roost Habitat Suitability • Aerodrome Safeguarding: Farnborough Consultation Zone 3 • Thames Basin Heaths SPA Consultation Zone – 5Km

RECOMMENDATION
<p>That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following Conditions and Informatives:</p>

Conditions:**1. Approved Plans**

This permission is in respect of the submitted application plans and drawings numbered Location Plan (Esc. 1:1250); Plan and Elevation/Sections (Esc. 1:50) received by the local planning authority on 11/12/2020, and revised Site Plan (Esc 1:500) received by the Local Planning Authority on 28/04/2021. The development shall be retained in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

Informatives:

1. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF.

PLANNING HISTORY

Application Number	Proposal	Decision
Enforcement RFS/2020/085628	Unauthorised erection of new bin store	Case Closed – Application submitted
B/29/71	Proposed conversion of existing large house into 11 self-contained flats with provision of 6 lock-up garages with 5 permanent parking spaces	Conditionally Approved 14/01/1972
B/14/71	Proposed conversion of existing large house into eleven self-contained flats	Refused 1971

SUMMARY INFORMATION**For Residential**

Site Area	0.25Ha (based on submitted red line)
Existing Use	Residential Block of Flats
Existing parking spaces	12 on-site car spaces (unaffected)

CONSULTATION RESPONSES

WBC Highways	No objections
WBC Cleaner and Greener	No objections
WBC Enforcement	No comments
Royal Berkshire Fire and Rescue	No comments

REPRESENTATIONS

Town/Parish Council:
No comments received.

Local Members:

Objection received from Councillor Maria Gee on the following grounds:

- Highway and safety grounds – the fire brigade tested access with a fire engine on the 17th March 2021 and were not able to pass the corner.
- The new position of the bin store is regarded by residents not in the flats as impeding their right of way.

Objection received from Councillor Peter Dennis on the following grounds:

- The access to the bin store area does not have a door-gate as requested by WBC Waste and Recycling guidelines. Any door would either have to open onto the shared driveway thus blocking it, or into the bin storage area reducing the amount of space for waste storage.
- Lack of door makes for an unwelcome view from neighbouring properties.
- Access point to the bin store is via the shared driveway; this represents a safety hazard, the corner is blind resulting in possible collisions.
- Access point is not accessible for wheelchair users.
- One post of the bin store effectively encroaches onto the vehicle space of the highway, thus restricting access.
- Bin store does not have a covering nor adequate shade; this represents a problem of smell from food waste rotting.

Neighbours:

Objections received from the occupants of nos. 40, 42, 44, 46 and 47a Frog Hall Drive on the following grounds:

- New bin store restricts visibility for both pedestrians and vehicles around the corner of the right of way. (see Para 17)
- New bin store physically restricts access of service, delivery and emergency vehicles. (see Para 18-19)
- The post to the right of the bin store entrance projects onto the right of way reducing its width. (see Para 17)
- Submitted drawings of the bin store are an inaccurate representation of what has been built. (Revised Site Plan received)
- Photos submitted with the application are not relevant and do not give a fair visual impression of the impact or location. (Site visit performed by Case Officer)
- Health and safety hazard due to smell, noise and vermin. (see Para 13-14)
- The relocation of the bin area was instigated by a request of the refuse collection company to allow for easier access to the bin area and to reduce the distance the bins needed to be manhandled to the collection vehicle. (not a material consideration)
- Bin store does not comply with the guidelines for Solid Waste Storage Areas. (see Para 6-9 and 13)
- There is an area on the other side of the car park more suitable for the bin store. (not a material consideration)
- Relocation resulted in the loss of trees and hedges. (see Para 22)
- Detrimental impact to visual amenity of the area. (see Para 8-10)

Eleven letters received from local residents from Frog Hall flats in support of the proposal, and also in response to the objections received (as above).

(Officer's note: the impact upon the character of the area, upon neighbouring private residential amenities, highways safety and trees and landscape will be addressed below in relevant section of this report.)

APPLICANTS POINTS

- The application has an acceptable impact on the character of the area and on neighbouring properties without any detriment to highway safety.
- The new bin store has been constructed to enable the wheelie bins to be served and emptied in a more efficient manner, making it safer for both residents and the local authority waste collection team.
- The new bin store enables easier control for any vermin issues should they arrive.
- The old bin store facility was not appropriate for storage and use due to its size.
- The new bin store complies with the Council's waste and recycling guidelines, and represents an improvement as it is of an adequate size to contain separate bins for each type of waste, enabling efficient recycling and waste disposal.

PLANNING POLICY

National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP3	General Principles for Development
	CP6	Managing Travel Demand
	CP9	Scale and Location of Development Proposals
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC07	Parking
	TB21	Landscape Character
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide – Section 4

PLANNING ISSUES

Description of Development:

1. The application involves the erection of fencing and hardstanding to form a new bin store area within the private residential garden of Frog Hall block of flats. The application is retrospective, and the development was completed in May 2020 as stated within the application form.
2. It is noted the previous bin store was located to the side/front of the host building, near one of the communal building entrances. The information submitted within the application form refers that the previous location of the bin store was not suitable for the storage and disposal of domestic waste for all the flats, being too small and too far away from the Council's collection point.

3. The new bin store is located to the frontage of the site within the front garden, towards the north-east side, with direct access from the private right of way, a single carriage way providing access to the block of flats and properties nos. 40-46 Frog Hall Drive.
4. The bin store area has an irregular footprint measuring approximately 3.5 metres in width and 5.1 metres in length. It comprises of a concrete base and a 2 metres high close board fencing enclosing the area with open access. The structure houses the wheeled bins for all types of waste and recycling pertaining to all the flats.

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories. As the site is within a major development location, the proposal is acceptable in principle.

Character of the Area:

7. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale of activity, mass, layout, built form, height, materials and character to the area in which it is located and must be of high-quality design without detriment to the amenities of adjoining land uses and occupiers. Policy CC04 of the MDD Local Plan requires adequate internal and external storage for the segregation of waste and recycling, as well as provision for green waste and composting and an appropriate area for ease of collection.
8. The Borough Design Guide SPD states that bin stores and recycling facilities should be designed to screen bins from public view, whilst being accessible for residents. Bin stores must be placed in a position that meets the Council's Highways standards. They should be close to the highway edge as possible and no greater than 10m from a point of access for the collection vehicle.
9. The bin store area comprising of hardstanding and close board fencing is located towards the north-east corner of the application site, suitably located within the private residential garden of the block of flats and without obstructing the main view from the building when approaching from main Frog Hall Drive. It is also modestly proportioned in terms of height and size whilst providing adequate space for the storage of all waste containers, and it is partially screened by adjacent vegetation which contributes to maintain the verdant character of the residential garden.

10. Furthermore, the bin store is reasonably accessible from the main road and the carry distance complies with the Council's desired guidelines for waste collection, so that it is unlikely there is an increased risk of waste being dumped informally within the street. WBC Cleaner and Greener who are responsible for waste collection within the Borough have been consulted on the application and have raised no objections.
11. In addition, the site visit confirmed that timber close boarded fence and areas of hardstanding are common features within the front of properties, usually as boundary treatment and for parking of vehicles, therefore the presence of these elements within the frontage of Frog Hall flats do not create an alien, incongruous feature within the street scene. It was also observed during the site visit a number of properties in the area where such bins were not enclosed within a dedicated storage area, which results in additional visual clutter in the street scene. As such, it is considered that the development does not result in any harmful impact to the character and appearance of the host building nor to the visual amenity of the area.

Neighbouring Amenity:

12. Policy CP3 of the Core Strategy aims to protect neighbouring amenity. The bin store area is currently located adjoining the Frog Hall's parking area with direct access from the private carriageway, at approximately 11 metres (linear) from the front elevation of the nearest neighbouring property no. 40 Frog Hall Drive across the carriageway, and with its access oblique to the main access to this neighbouring site.
13. Given the single storey and open nature of the bin store area, it is not considered it results in any adverse impacts upon the neighbouring properties – particularly no. 40 – by way of loss of light, overbearing or overshadowing, and is not significantly different to the existing relationship on site, where plot boundaries are characterised by high fencing/brickwall and overgrown hedges, including the boundaries of no. 40. Likewise, while the bin store is visible from the carriageway, due to its modest scale and nature of development (close board fencing and hardstanding) it does not represent a dominant feature in the outlook from neighbouring property no. 40 or appear oppressive, thus there is no significant adversely effect upon the living conditions of the occupiers of this property in this respect.
14. Moreover, the new bin store effectively provides ease of maintenance with a tap for regular cleaning and washing, and adequate ventilation being the area only partially enclosed on the boundaries. The site visit confirmed that the bin store is kept to the highest practical standard of hygiene with all waste contained in appropriate wheeled bins which all have close lids, and there was no evidence of vermin presence within the bin store at the time of the site visit. Whilst it is acknowledged the storage of refuse is likely to generate some odour, it is noted that the closest windows are those on the first floor of property no. 40, which are at a distance of approximately 11 metres from the storage area – which is considered to be an appropriate separation distance for waste within a residential area. As such, a reason to refuse this application on this sole ground would not be substantiated.
15. In addition, any potential noise nuisance is only temporary during waste disposal from residents and on collection day by the Local Authority, which would be the case wherever the bin store was located within the site so that there is no significant adverse impact upon the living conditions of adjoining occupiers.

Amenity Space for Current/Future Occupiers:

16. The new bin store facility occupies part of the existing private residential garden of Frog Hall flats. However, the site visit confirmed that given the modest dimensions of the structure, the remaining amenity space is still of an adequate size, capable of accommodating typical garden activities, in accordance with the Borough Design Guide. Therefore, no objections are raised in this aspect.

Access and Movement:

17. Policy CP6 of the Core Strategy seeks to manage travel demand with sustainable travel modes, appropriate vehicle parking and minimisation of traffic impacts. The bin store area is located adjacent to the parking area to the frontage of the building, without compromising its access nor the number of available parking spaces for residents of the flats or visitors. No objections are therefore raised in this respect.

18. It is noted that objections have been received relating to the location of the bin store restricting visibility for both pedestrians and vehicles around the corner of the right of way to properties nos. 40-46 Frog Hall Drive. This right of way is a single carriageway which provides access solely to these properties. A historical photo taken from Google Street View (2009) confirms that there used to be a large mature tree and substantial hedgerow right up to the edge of the carriageway. The site visit confirmed that the fencing and posts are all within the site boundary line, without obstructing the right of way nor reducing the width of the carriageway. As such, it is considered that the location of development does not represent a significant difference to the previous situation, and thus the new bin store does not result in any harmful impact in terms of highway and safety grounds, nor does it impede the right of way for residents of the properties using this access. The WBC Highways Officer has raised no objections to the development.



Photo taken from Google Map, dated April 2009



Photo from Officer's Site Visit on 13 April 2021

19. It is noted the objection received relating to the new bin store area physically restricting access of service, delivery and emergency vehicles to properties located within the right of way (i.e. nos. 40-46 Frog Hall Drive). It is also noted the listing request from Councillor Gee on the ground that on 17/03/21, the fire brigade tested access with a fire engine and were not able to pass the corner. It should be noted that this point raised by Councillor Gee is based on the comments made by local

neighbours of properties nos. 40-46 Frog Hall Drive, and has not been verified by the Royal Berkshire Fire & Rescue Services.

20. The Royal Berkshire Fire & Rescue Services have been consulted as part of this application, and whilst no response have been received to date, the below image taken by a local resident on 17/03/21 demonstrates that there is adequate space within the carriageway for a fire engine to access without being close to the new bin store area, and in any event, as previously mentioned the current bin store is set without encroaching on the highway and does not represent a material difference to the previous situation where trees and hedges were right up to the edge of the carriageway. As such, in this instance it can be said that the development does not result in any harmful impact on highway safety or the operation of emergency and rescue vehicles. The WBC Highways Officer has raised no objections on this aspect.



Photo of Fire Engine Vehicle



Photo of Delivery Van

Trees and Landscape:

21. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, promote linkages between public open space and the countryside, retain existing trees and establish appropriate landscaping, and Policy TB21 requires consideration of the landscape character.

22. The Council's inventory registers the presence of protected trees within the immediately adjacent land to the north and west of the application site (TPO-0004-1960), however these are at a sufficient distance (25 metres linear) from the new bin storage area. Given the modest scale of development, it is not considered that the erection of hardstanding and fencing has resulted in any damage upon the root protection area of these trees.

23. It is noted the objection received which relates to the relocation of the bin storage area resulting in the loss of trees and hedges. Historical photo from Google maps shows the presence of a large tree and hedge where the bin is currently located.

However, the Council's inventory does not record that this tree was protected, and it is not clear whether their removal is directly linked to the development subject of this application. The existing laurel hedge and other soft landscaping along the fencing provides for suitable screening, it softens the hard boundary fence and thus integrates more sympathetically within the verdant character of this front amenity area. As such, it is considered that the development does not have a significantly adverse impact upon trees and landscape to the extent of substantiating a reason to refuse the application on this ground.

Ecology:

24. There are no ecological issues associated with the proposal.

Thames Basin Heaths Special Protection Area:

25. Policy CP8 of the Core Strategy states that where development is likely to have an effect on the Thames Basin Heaths Special Protection Area (TBH SPA), it is required to demonstrate that adequate measures to avoid and mitigate any potential adverse effects are delivered. The subject property is located within 5km of the TBH SPA but the scope of the works is minor whereby there is no foreseeable impact upon the SPA.

The Public Sector Equality Duty (Equality Act 2010)

In determining this application, the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. The new bin store area does not have a step or platform, and is fully accessible by wheelchair users. There is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.

CONCLUSION

26. The retrospective proposal makes adequate provision for the storage and collection of domestic waste and recyclable materials, having regard to neighbouring amenity and the character and appearance of the surrounding area. The proposal does not represent a material difference to the existing situation in terms of highway access and safety. It is considered that the development accords with national and local planning policies, and it is therefore recommended for approval.

This page is intentionally left blank

H. M. LAND REGISTRY

NATIONAL GRID PLAN

SU 82 68

SECTION A

~~BERKSHIRE~~

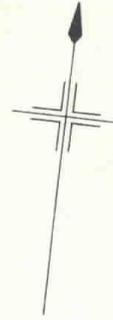
ADMINISTRATIVE AREA WOKINGHAM

Scale 1/1250

Enlarged from 1/2500

~~WOKINGHAM DISTRICT~~

WOKINGHAM WITHIN PARISH



Old Reference XXXVIII 16
Made and printed by the Director
General of the Ordnance Survey,
Chessington, Surrey. 1965 for HMLR
© Crown Copyright 1965



TITLE No. BK101041

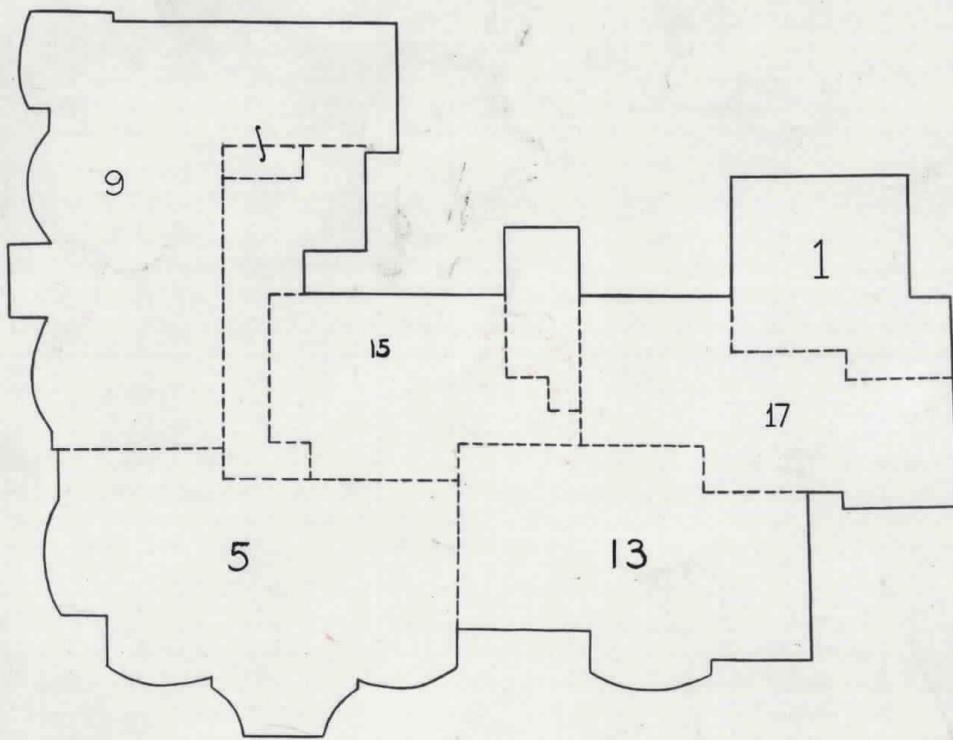
This is a copy of the title plan on 4 JAN 2019 at 18:43:00. This copy does not take account of any application made after that time even if still pending in HM Land Registry when this copy was issued.

This copy is not an 'Official Copy' of the title plan. An official copy of the title plan is admissible in evidence in a court to the same extent as the original. A person is entitled to be indemnified by the registrar if he or she suffers loss by reason of a mistake in an official copy. If you want to obtain an official copy, the HM Land Registry web site explains how to do this.

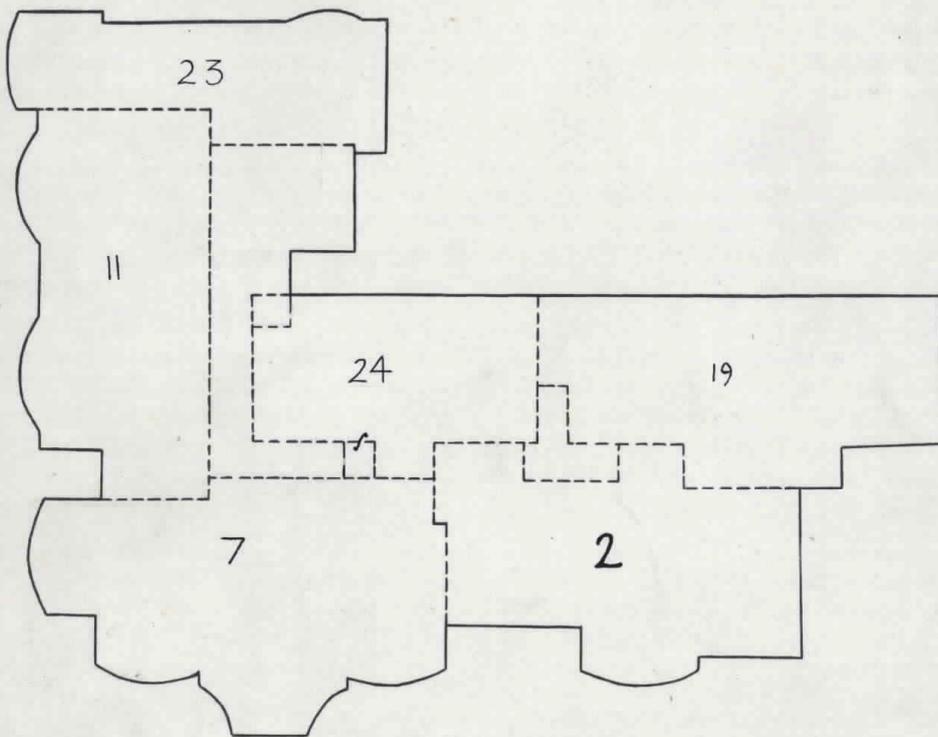
HM Land Registry endeavours to maintain high quality and scale accuracy of title plan images. The quality and accuracy of any print will depend on your printer, your computer and its print settings. This title plan shows the general position, not the exact line, of the boundaries. It may be subject to distortions in scale. Measurements scaled from this plan may not match measurements between the same points on the ground.

This title is dealt with by HM Land Registry, Gloucester Office.

Scale: 1/250

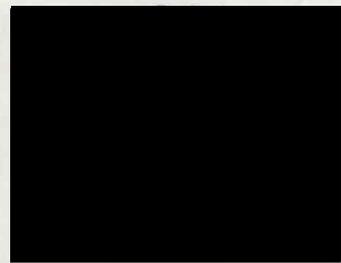


GROUND FLOOR



FIRST FLOOR

SUPPLEMENTARY PLAN
TO THE FILED PLAN



TITLE No. BK101041

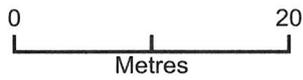
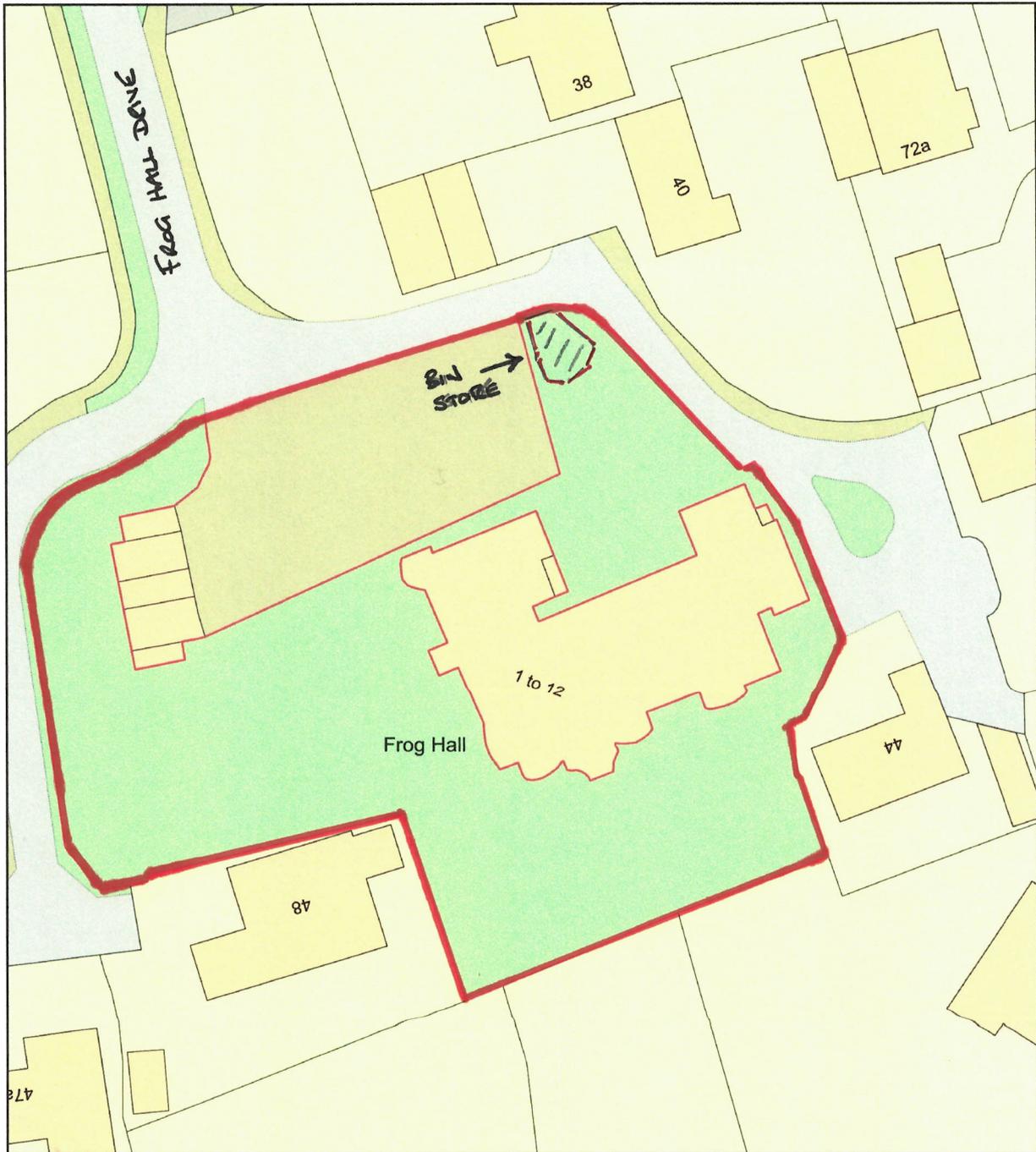
This is a copy of the title plan on 4 JAN 2019 at 18:43:00. This copy does not take account of any application made after that time even if still pending in HM Land Registry when this copy was issued.

This copy is not an 'Official Copy' of the title plan. An official copy of the title plan is admissible in evidence in a court to the same extent as the original. A person is entitled to be indemnified by the registrar if he or she suffers loss by reason of a mistake in an official copy. If you want to obtain an official copy, the HM Land Registry web site explains how to do this.

HM Land Registry endeavours to maintain high quality and scale accuracy of title plan images. The quality and accuracy of any print will depend on your printer, your computer and its print settings. This title plan shows the general position, not the exact line, of the boundaries. It may be subject to distortions in scale. Measurements scaled from this plan may not match measurements between the same points on the ground.

This title is dealt with by HM Land Registry, Gloucester Office.

Frog Hall site plan



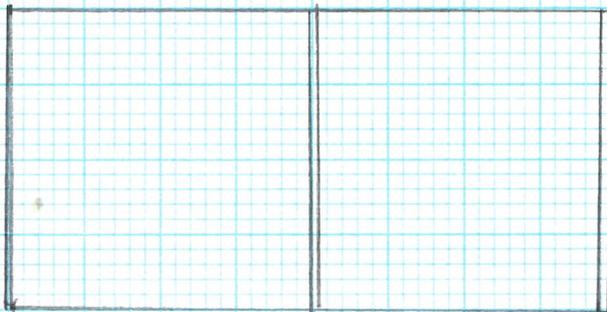
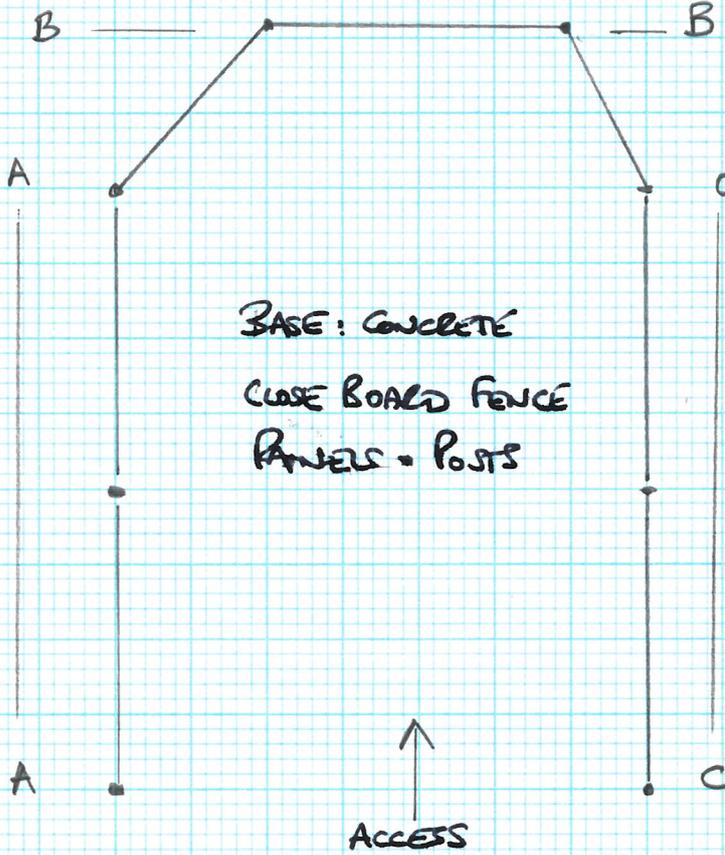
Plan Produced for: Wokingham Council
Date Produced: 14 Apr 2021
Plan Reference Number: TQRQM21104121136449
Scale: 1:500 @ A4

This page is intentionally left blank

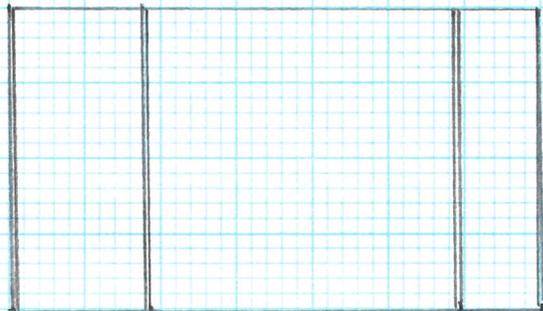
APPLICATION NUMBER : 203460

SITE ADDRESS : FROGHALL, FROGHALL DRIVE, R440 2LF

SITE PLAN:



SECTION A - A
C - C

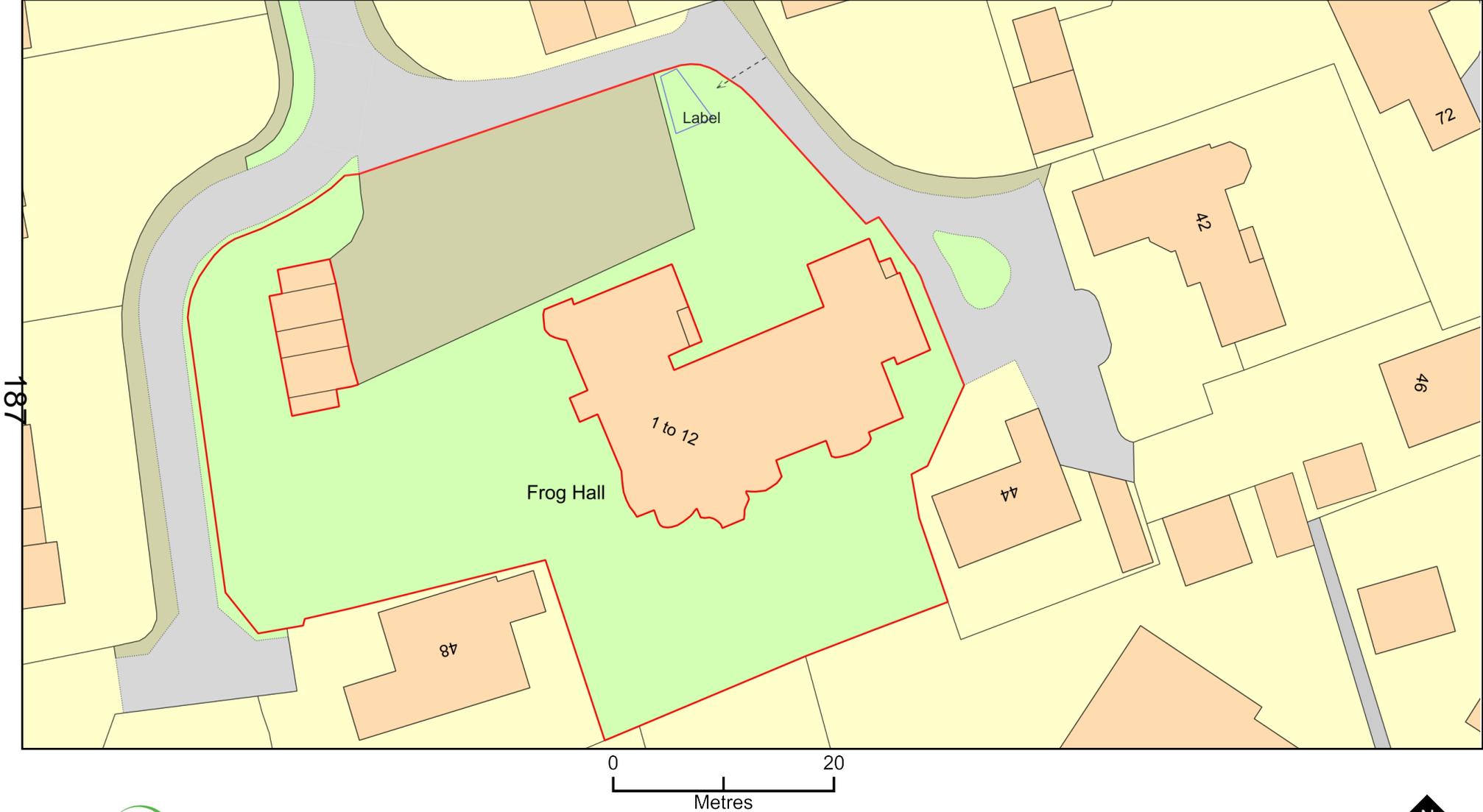


SECTION B - B

SCALE: 1:50 185° 1 - - - - 2 METRES

This page is intentionally left blank

Frog Hall Bin Store



Plan Produced for: Cleaver Property Management

Date Produced: 30 Jul 2020

Plan Reference Number: TQRQM20212150019230

Scale: 1:500 @ A4

© Crown copyright and database rights 2019 OS 100042766

This page is intentionally left blank

Agenda Item 8.

Application Number	Expiry Date	Parish	Ward
210805	23/06/2021	St Nicholas Hurst	Hurst;

Applicant	Mr Adam Hurst
Site Address	"Dolphin School", Waltham Road, Hurst, Wokingham, RG10 0FR
Proposal	Full application for the proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.
Type	Full Planning Application
Officer	Adriana Gonzalez
Reason for determination by committee	Major Application

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 June 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY

This planning application is before the planning committee as it is a major application given the site area is in excess of 1Ha (1.8 Ha).

The application proposes the erection of a two storey detached building of contemporary architectural appearance, which will comprise 2no classrooms, music room and WC facilities at ground floor, as well as a new library and art room within the first floor. A rooftop terrace will complement the proposal whilst allowing for events and viewing of team matches on the adjoining playing field.

The proposed scheme offers the opportunity for positive improvements in visual terms, whilst providing a much needed new educational unit for Dolphin School. The building's location, scale and design will respect the character and appearance of the site and countryside setting. Likewise, the proposal will be respectful to the setting of the adjoining Hinton House, a grade II*listed building, preserving the significance of this heritage asset.

Paragraphs 1-40 provide further details to these material considerations, and subject to conditions 1-9 the application is recommended for approval.

PLANNING STATUS

- Designated Countryside
- Within the Edge of Green Belt – North-East Boundary
- Listed Building Curtilage – Hinton House
- Ground Water Consultation Zone
- Bat Roost Habitat Suitability
- Green Route – Waltham Road

RECOMMENDATION

That the committee authorise the **GRANT OF PLANNING PERMISSION** subject to the following:

Conditions:

1. Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details

This permission is in respect of the submitted application plans and drawings numbered 1065-PL01; 1065-PL02 Rev A; 1065-PL03 Rev A; 1065-PL04; 1065-PL05; 1065-PL06; 1065-PL07; 1065-PL08 Rev A; 1065-PL09; 1065-PL01 Rev A (volumes); Drainage Plan 20-057-51-P1; Travel Plan (TTP Consulting Ltd, February 2021); Design And Planning Statement Including Sustainability Statement (trace architects, March 2021); Tree Survey (Arbtech, February 2021); Preliminary Ecological Appraisal and Preliminary Roost Assessment Survey (Arbtech, January 2021); Drainage Design Document Rev P1 (Creative Side Structures, February 2021) received by the local planning authority on 08/03/2021; eDNA Results Technical Report (SureScreen Scientifics, April 2021) received by the local planning authority on 26/04/2021; Arbtech TPP 01 (March, 2021); Arbtech AIA 01 (March 2021) received by the local planning authority on 10/05/2021; Construction Method Statement Rev P4; Construction Method Statement Plan 20-057-51-P2 & Revised Heritage Statement (March 2021) received by the local planning authority on 19/05/2021. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. External materials

Before the development hereby permitted is commenced, samples and details of the materials to be used in the construction of the external surfaces of the building/s shall have first been submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the so-approved details.

Reason: To ensure that the external appearance of the building is satisfactory. Relevant policy: Core Strategy policies CP1 and CP3.

4. Landscaping details

Prior to the commencement of the development, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished floor levels or contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, play equipment, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable. All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the

development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

5. Protection of trees

- a) Prior to the commencement of the development there shall be submitted to and approved in writing by the local planning authority a revised Arboricultural Method Statement and Scheme of Works, which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).
- b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence
Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

6. Landscape and Trees Management

Prior to the commencement of the development a landscape and tree management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas and trees on site, shall be submitted to and approved in writing by the local planning authority. The landscape and tree management plan shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping (including trees) hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

7. Construction Method Statement

No development shall take place, including any works of demolition, until a revised Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

Reason: In the interests of highway safety & convenience and neighbour amenities.

Relevant policy: Core Strategy policies CP3 & CP6.

8. Travel Plan as agreed

No part of the development shall be occupied until the approved travel plan (Travel Plan (TTP Consulting Ltd, February 2021) has been implemented. The travel plan shall be implemented, maintained and reviewed as approved.

Reason: To encourage the use of all travel modes. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policy CP6.

9. Ecology details as agreed

The development hereby approved shall be carried out in accordance with the ecology details and recommendations as identified in "Preliminary Ecological Appraisal and Preliminary Roost Assessment Survey, Arbtech, January 2021" received by the Local Planning Authority on 08/03/2021. The scheme shall be implemented prior to the occupation of development, in full in accordance with the approved details and for the lifetime of the development unless otherwise agreed in writing by the local planning authority.

Reason: to secure wildlife enhancements and biodiversity net gain on site within the course of the development. Relevant policy: paragraph 170 of the NPPF and TB23 of the MDD Local Plan.

Informatives:

1. The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.
2. The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material

changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

3. This permission does not convey or imply any approval or consent that may be required for the display of advertisements on the site for which a separate Advertisement Consent application may be required. You should be aware that the display of advertisements without the necessary consent is a criminal offence liable to criminal prosecution proceedings through the courts.
4. The applicant is advised that the fitting of a sprinkler system should be considered at the premises prior to the operation of the building as approved.
5. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:
 - a full pre-application process was undertaken by the applicant;
 - addressing concerns relating to trees on site;
 The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

RELEVANT PLANNING HISTORY		
Application Number	Proposal	Decision
F/2011/2040	Installation of 33 solar panels on south-west elevation of dormer roof	Approved 09/01/2012
F/2006/7432	Additional staff car parking in school grounds	Refused 16/06/2006 Appeal Allowed
VAR/2002/8005	Variation to condition 10 of 43595 to increase pupils from 250 – 275	Conditionally Approved 02/12/2004
F/2001/3254	2 storey extension – use as classroom, library, I.T. and science	Conditionally Approved 06/09/2002
971433	Proposed Variation to Conditions 12 And 13 on Approval 43595 To Allow Commencement Of Development Without provision of car parking	Approved 09/10/1997
961591	Proposed retention of temporary building for classroom	Approved 21/10/1996
961590	Proposed provision of two additional temporary buildings	Approved 21/10/1996
44580	Single storey extension and sewage treatment plant	Conditionally Approved 07/08/1995
43595	2 storey extension to form additional classrooms, staff rooms, offices above, hardstanding. Variation of condition 2 on 11451 and condition 6 on 15767 (from 120 pupils to 250 pupils)	Conditionally Approved 31/07/1995
38434	Single storey extension and retro application for further 22 parking spaces	Refused 15/01/1992

38433	Redevelop and extend ex. School	Refused 15/01/1992
25947	Extend covered way re-roof existing	Conditionally Approved
23550	Car park on part of playing field	Conditionally Approved
20772	Implementation of 15767 without compliance to condition7	Refused 08/03/1984
15767	General purpose hall	Conditionally Approved 02/07/1981
13964	General purpose hall	Refused 25/09/1980
11451	Use of School on permanent basis	Conditionally Approved 25/10/1979
11449	Retention of pavilion structure	Conditionally Approved 25/10/1979
11453	Retention of art room	Conditionally Approved 25/10/1979
09706	Use of School on permanent basis	Refused 18/01/1979
09704	Alterations to front wall and increase number of pupils from 25 – 120	Conditionally Approved 08/06/1979
09702	General purpose hall	Refused 18/01/1979
04664	Playing field	Conditionally Approved 17/06/1975

SUMMARY INFORMATION	
Site Area	1.8 Ha
Existing land use(s)	Learning & Non-Residential Institution (Use Class F1)
Proposed floorspace	268sqm (total)
Number of pupils	220 students (no increase proposed)
Number of jobs created/lost	No increase (currently 50 employees total)
Existing parking spaces	120 car spaces
Proposed parking spaces	120 car spaces

CONSULTATION RESPONSES	
WBC Environmental Health	No objection
WBC Drainage	No objection
WBC Highways	No objection subject to conditions (7, 8)
WBC Education (School Place Planning)	No comments received
WBC Economic Prosperity and Place (Community Infrastructure)	No objection
WBC Biodiversity	No comments received
WBC Tree & Landscape	No objection subject to conditions (4, 5, 6)
WBC Cleaner & Greener (Waste Services)	No comments received
WBC Heritage & Conservation	No objection subject to additional information (further details below)
WBC Health and Wellbeing	No comments received
Historic England	Stated they have no comments
Thames Water	No comments received
Royal Berkshire Fire and Rescue	No comments received
Crime Prevention Design Officer	No comments received

REPRESENTATIONS

Town/Parish Council: No objections

Local Members: No comments received

Neighbours: No comments received

APPLICANTS POINTS
<ul style="list-style-type: none"> • Application supported by a Planning Statement and Heritage Statement • The proposal will provide new educational space for the school whilst providing a bold new entrance building to improve the profile of the school and express its core values. • proposal uses modern methods of construction to create a building that is highly energy efficient, economical to build and, of great importance to a working school, quick to erect.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development-
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP9	Scale and Location of Development Proposals
	CP11	Proposals outside development limits (including countryside)
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB02	Development adjoining the Green Belt
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets

Supplementary Planning Documents (SPD)	BDG	Borough Design Guide Supplementary Planning Document Sustainable Design and Construction Supplementary Planning Document CIL Guidance
--	------------	---

PLANNING ISSUES

Description of Development:

1. The application site is located within the grounds of a private school campus, that is Dolphins School, in a countryside location within the settlement of Hurst. The school grounds comprise of approximately 1.8Ha. The school's eastern boundary adjoins the curtilage grounds of Hinton House, a Grade II* listed building since 1952. To the south and west, the site adjoins open fields whilst the Waltham Road runs along the northern boundary.
2. The proposal involves the erection of a new two storey detached building to accommodate 2no classrooms, music room and WC facilities at ground floor, as well as a new library and art room within the first floor. A rooftop terrace will complement the proposal whilst allowing for events and viewing of team matches on the adjoining playing field.
3. The new building itself would be located in front of the existing two storey building to the east of the sports field. It would have a T-shape layout given by the mixed of flat-roof, rectangular forms, and would measure approximately 23.7 metres in width, 20.3 metres in length and 6.3 metres in height. The footprint of the building would be approximately 202sqm for a total internal floor space of 268sqm (as measured off submitted plans).
4. The design is formed from two rectangular blocks at right angles linked by a staircase and walkway. The two-storey building will have a contemporary architectural appearance with simple palate of materials and flat roofs. Elevations are simple clad in cedar slats and flush full height glazing.

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. The application site is located outside the settlement boundary and in the countryside, where the restrictions of Core Strategy Policy CP11 are applicable. Policy CP11 indicates that new development should normally be located within the settlement limits; however, essential community facilities that cannot be accommodated within development limits or through the re-use/replacement of existing buildings may be appropriate.

7. The restrictive countryside policy should also be weighed against the advice contained within the NPPF. Paragraph 92b of the NPPF states that planning policies and decisions should *“take into account and support the delivery of local strategies to improve health, social and cultural well-being for all sections of the community, and deliver sufficient social, recreational and cultural facilities and services to meet the community needs.”* Paragraph 94 of the NPPF urges local planning authorities to *“give great weight to the need to create, expand or alter schools.”*
8. Policy CP2 of the Core Strategy also aims to ensure that new development contributes to the formation of sustainable and inclusive communities through the provision of community facilities (including educational buildings), and that the requirements of children are addressed.
9. It is considered that the enhancement of the school constitutes the provision of an essential community facility, and accords with Policy CP11. The new building will provide improved teaching spaces for the benefit of both children and teachers, whilst providing a positive welcome for visitors. As such, the proposal would be acceptable in principle subject to further assessment of the impact of the proposed development on the character of the area, street scene, neighbouring amenity, highway safety as well as compliance with other local and national planning policies.

Character of the Area:

10. Section 12 of the NPPF states that ‘permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions’ and ‘the Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people’.
11. Policy CP1 states that development should maintain or enhance the high quality of the environment. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and must be of high quality design. CP11 of the Core Strategy refers to maintaining the quality of the environment and restricting development within the countryside. R1 and RD1 of the Borough Design Guide SPD requires that development contribute positively towards and be compatible with the historic or underlying character and quality of the local area, and R11 requires a coherent street character, including in terms of scale, rhythm, proportion and height.
12. It is noted the site is located close to the Metropolitan Green Belt (opposite Waltham Road). Policy TB02 of the MDD Local Plan states that proposals outside the Green Belt but conspicuous when viewed from it, will only be acceptable where it is not detrimental to the visual amenity and openness of the Green Belt in terms of scale, form, siting, materials or design.
13. The proposed new building would be located towards the front of the school site, forming a gateway with the tennis courts and access from the car park into the school campus. The building would be two storey in height, represented by a mix of simple volumes which would follow same landform and orientation already established by other buildings and facilities within the school (tennis court). Whilst located to the front of the school, the new building would have very limited views from the main

road, given its significant set-back from the road (approximately 31 metres), and the presence of very high boundary hedgerows at the entrance of the school's grounds. As such, it is not considered there would be any significant impact on the street scene and character of the area. Furthermore, the height, bulk, massing and siting of the new building is considered sensible to its surroundings, as it would adjoin existing buildings/facilities within the school, therefore reducing potential harm from spread of development across the site, nor would it detract from the visual amenity and openness of the Green Belt, in line with the principles set within Policy CP11 of the Core Strategy and TB02 of the MDD Local Plan.

14. In terms of design, whilst the new building would introduce a more modern design on site, it is considered that its proportions would match the form, scale and shape of other buildings within the school. The combination of simple volumetric forms with substantial glazing and timbered facades, would be considered visually sympathetic and in keeping with the distinctive character of the rest of the site, and would not detract from the character of the countryside setting. In support of the design approach, the applicant has commented that the proposal uses modern methods of construction to create a building that is highly energy efficient, economical to building and, of great importance to a working school, quick to erect. It is therefore considered that overall, the proposal would meet the requirements of the NPPF, Core Strategy Policies CP1, CP3, and the principles of the Borough Design Guide.

Heritage and Conservation:

15. Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that development must ensure the preservation of any nearby listed building, including its setting, Paragraphs 192-196 of the NPPF requires consideration of the harm to the significance of a designated heritage asset and Policy TB24 of the MDD Local Plan requires the conservation and enhancement of Listed Buildings, Historic Parks and Gardens, Ancient Monuments and Conservation Areas, including their views and setting. In this respect, the school's eastern boundary directly adjoins the curtilage of the Hinton House, a grade II*listed building, and the brick boundary wall along Waltham Road, which is also grade II designated curtilage structure.
16. Following initial comments from the WBC Conservation Officer, the applicant has submitted an updated Heritage Statement, which provides additional justification and assessment of the historical and architectural significance of the setting of the heritage asset, as well as visualisations showing the spatial relationship of the proposed development with the listed building. The proposal would sit behind well-developed / mature trees and hedge line, which would act as screening providing only limited views between the new building and the heritage assets, and the flat roof design and reduced height would also provide no visual interruption to viewing from Hinton House. Added to this is the fact that the proposed development would be located approximately 50 metres from the nearest part of the listed building, thus it is unlikely that there would be any negative impact upon the heritage assets and its setting, in accordance with the NPPF and policy TB24 of the MDD Local Plan.

Neighbour Amenity:

Loss of light, Overbearing and Overlooking:

17. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and R23 of the Borough Design Guide SPD requires that extensions relate to neighbouring properties. Given the school's isolated location within countryside, the nearest residential properties are currently found towards the south side of the school's grounds, and well over 50 metres from the location of the proposed new building. Likewise, the Hinton House is also at a considerable distance (50 metres) from the proposal site, and the site visit confirmed there are several mature trees and vegetation which would screen this house from the new building. As such, it is considered unlikely there would be any detrimental impact upon neighbouring residential amenities as a result of proposed development.

Noise disturbance:

18. Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact. The submitted Planning Statement refers that no noise generating plant is included within the proposed development. Moreover, given the nature of the proposal and separation distances from nearest noise sensitive receptors, it is considered unlikely there would be any negative impact upon neighbouring amenities. The WBC Environmental Health Officer has evaluated the proposal and has raised no objections to the proposal on this ground.

Highways Access and Parking Provision:

19. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards, including provision for charging facilities. P3 of the Borough Design Guide SPD notes that parking spaces should be safe and convenient and sited to minimise impact upon safety.
20. The proposal would result in the loss of 4-5no parking spaces due to the proposed location of the new building. However, the site visit confirmed there is ample parking space within the curtilage of the school, and the supporting information refers that there would be no increase in student number or members of staff, therefore it is unlikely there would be any negative impact on parking grounds. The WBC Highways Officer has raised no objections to the proposed development on parking grounds.
21. Access to the site would remain unaltered and this is considered acceptable.
22. Following initial comments from the WBC Highways Officer in regard to the submitted Construction Method Statement, a revised document been submitted in support of the application. The WBC Highways Officer has evaluated this but has indicated that additional details are still required, including a plan showing the route for construction vehicles to the site from the strategic road network. These details can be secured via planning condition (condition 7), so that no objections are raised on this aspect.
23. Likewise, a Travel Plan has been submitted as part of the proposal. It sets out some solid initiatives and targets, with a clearly defined monitoring regime. Subject to the implementation of the Travel Plan being secured by condition (condition 8), the WBC Highways Officer has raised no objections to the proposal on this ground.

Flooding and Drainage:

24. Policy CC09 of the MDD Local Plan requires consideration of flood risk from historic flooding, and Policy CC10 of the MDD Local Plan requires sustainable drainage methods and the minimisation of surface water flow. The site and access thereto is located within Flood Zone 1, and the proposal represents no additional flood risk or vulnerability. A Drainage Plan and Drainage Design document have been presented with the application, which consist of detailed information about the soil-ground investigation, project overview and detail description of the proposed surface water drainage strategy – supported with overview detailed drawing and cross-section drawings of the proposal.
25. The main strategy is to store and dispose the surface water into a pond which is inside the development area with maximum control flow of 2 l/s. Calculations presenting the chosen method and supporting its capabilities are presented too. A substantial part of the proposed development would be built over already hardstanding surface, therefore the increase of water surface run-off would not be significant. The WBC Flood Risk and Drainage Engineer has evaluated the proposal and information submitted and has raised no objections to the proposed development on flooding and drainage grounds.

Landscape and Trees:

26. Policy CC03 of the MDD Local Plan states development should protect and retain existing trees, hedges and other landscape features. Policy TB21 of the MDD Local Plan states that proposals must demonstrate how they have addressed the requirements of the Council's Landscape Character Assessment, including the landscape quality; landscape strategy; landscape sensitivity and key issues and 'proposals shall retain or enhance the condition, character and features that contribute to the landscape'.
27. The application site is located in the designated countryside, and within landscape character area K1 Stanlake Farmed Sand and Clay Lowland, which is defined as: *'A lowland landscape with many unique and distinctive features not found elsewhere in the borough, which provide a contrast to the wooded hills to the north and south. The area supports numerous small tributary streams and ponds set amongst the largely arable farmland. The area is characterised by its wooded character and a general absence of settlement resulting in a sense of relative remoteness.'*
28. The site is also set back a short distance from the B3018 Waltham Road, a Green Route designated under Policy CC03 of the MDD Local Plan, that is a rural road edged with trees and well-kept hedgerows. There are intermittent views of the athletics track between the trees and the top of the school buildings from the entrance. The school buildings are set back from the road, with the main building to the far south west of the site that has the appearance of being tucked away to the back of Hinton House.
29. There is a good tree population within the school grounds, and although none of these are protected, they contribute to the site setting and surrounding countryside character. Of particular note is the presence of a mature Oak tree towards the west side of the proposed siting of the new building, the most significant tree in terms of visual amenity and biodiversity.

30. The following documents relating to trees on site have been submitted in support of the application:
- Tree Survey (report) dated 2.2.21
 - Arboricultural Method Statement (AMS) report dated 6.5.21
 - Arboricultural Impact Assessment (AIA) Drawing Arbtech AIA01 dated March 2021
 - Tree Protection Plan (TPP) Drawing Arbtech TPP01 March 2021
31. The WBC Trees and Landscape Officer has evaluated the proposal and information submitted, who has indicated that the technical solutions proposed to mitigate the impact of development on the Oak tree, both foundation design and permeable systems for surfacing are mostly acceptable, subject to recommendations/specifications, which would need to be included within an updated AMS. The revised AMS will be secured by way of planning condition (condition 5). A landscape and tree management plan would be also required and will be secured through condition (6). On this basis, the Officer has raised no objections to the proposal on this ground.
32. Likewise, the Officer has indicated that it is important to retain and enhance the character and appearance of the site's setting and wider countryside area, with provision made for soft and hard landscaping that complements the siting of the new building. A condition requiring the submission of a landscape proposals plan would be sufficient to address this concern (condition 4).

Ecology:

33. Policy CP7 of the Core Strategy 2010 requires the conservation of sites for nature conservation in accordance with national, regional, county, and local biodiversity action plans. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network. The site is located in habitat which matches that where bat roosts have previously been found in the borough.
34. A Preliminary Ecological Appraisal and Preliminary Roost Assessment Survey (Arbtech, January 2021) has been submitted in support of the application. The report concludes that the existing pond on site has some potential to be suitable for Great Crested Newts (GCN) which are known to be present in the local landscape (within 2Km), and as such, an eDNA water sampling test should be completed on the pond to confirm GCN presence or otherwise. A follow up Technical Report (SureScreen Scientifics, April 2021) was carried out and found no presence of GCN within the pond (negative eDNA). As such, no further consideration is necessary in this aspect.
35. The Preliminary Ecological Appraisal further indicates that the proposed development will have no impact on other ecological receptors. However, precautionary methods of working are given in order to mitigate low risk of harm to birds, mammals and common amphibians during construction works. Likewise, the report provides recommendations for additional habitat boxes/bricks to secure on site gains for biodiversity, in line with paragraph 170 of the NPPF. Implementation of these will be secured via planning condition (9).
36. Subject to the above, no objections are raised to the proposal on ecological grounds.

Sustainable Design/Construction:

37. Policy CC04 of the MDD Local Plan and the Sustainable Design and Construction SPD require sustainable design and conservation and R21 of the Borough Design Guide SPD requires that new development contribute to environmental sustainability and the mitigation of climate change.
38. A Sustainability Statement has been included within the supporting Design and Planning Statement and outlines how the development will endeavour to meet the requirements outlined in policies CC04 and CC05 of the MDD Local Plan. It is considered that acceptable measures would be taken to ensure the sustainability of the proposal, so that the scheme is acceptable in this respect.

Community Infrastructure Levy (CIL):

39. Wokingham Borough Council adopted CIL on 6 April 2015. The proposed development will result in an increase in new build floorspace of more than 100sqm and therefore is liable for CIL.

CONCLUSION

40. The proposed development is considered to be acceptable in principle and would succeed in providing a building of adequate size and dimensions to accommodate a much needed new educational unit, in terms of its setting and appropriate scale and design. The proposal would not result in any detrimental impact upon the street scene or significance of the adjoining heritage assets, nor would it harm the intrinsic character of the surrounding countryside. Subject to appropriate conditions, no harmful impact would occur with regard to neighbouring amenity, highways, drainage, biodiversity or trees and landscape. As such, the proposal is considered to accord with national and local planning policies and is therefore recommended for approval.

The Public Sector Equality Duty (Equality Act 2010)
In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. The proposal is intended to support children learning and development, and this is acknowledged in consideration of the application. Aside from this, there is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.



LOCATION PLAN 1:1250

NOTES

THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS AND MAY NOT BE REPRODUCED WITHOUT PERMISSION



PLANNING

© Copyright TRACE Architects Ltd 2021



Basement Studio
263 The Vale
London
W3 7QA
0208 611 2946
office@tracearchitects.co.uk
www.tracearchitects.co.uk

1 The Hermitage
Eastfield Lane
Goring Heath
RG8 7SU
01491 579 544

DATE	-	CLIENT:
DRAWN	-	Dolphin School
CHECKED	-	
SCALE@A3	1:1250	

Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.

LOCATION PLAN

ADDRESS:
Dolphin School
Waltham Road, Hurst, RG10 0FR

JOB No	DRAWING NUMBER
1065	PL01



EXISTING SITE PLAN 1:500

NOTES	
THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS AND MAY NOT BE REPRODUCED WITHOUT PERMISSION	
<p>PLANNING</p> <p style="font-size: x-small;">Copyright TRACE Architects Ltd 2021</p>	
<p>trace ARCHITECTS</p>	
Basement Studio 263 The Vale London W3 7QA 0208 611 2946 office@tracearchitects.co.uk www.tracearchitects.co.uk	1 The Hermitage Eastfield Lane Goring Heath RG8 7SU 01491 579 544
DATE - DRAWN - CHECKED - SCALE@A3 1:500	CLIENT: <p style="font-weight: bold; font-size: small;">Dolphin School</p>
Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.	
EXISTING SITE PLAN	
ADDRESS: <p style="text-align: center;">Dolphin School Waltham Road, Hurst, RG10 0FR</p>	
JOB No <p style="font-weight: bold; font-size: small;">1065</p>	DRAWING NUMBER <p style="font-weight: bold; font-size: small;">PL02 Rev A</p>
A3 Sheet	



PROPOSED SITE PLAN 1:500

NOTES	
THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS AND MAY NOT BE REPRODUCED WITHOUT PERMISSION	
PLANNING	
© Copyright TRACE Architects Ltd 2021	
Basement Studio 263 The Vale London W3 7QA 0208 611 2946 office@tracearchitects.co.uk www.tracearchitects.co.uk	1 The Hermitage Eastfield Lane Goring Heath RG8 7SU 01491 579 544
DATE - DRAWN - CHECKED - SCALE@A3 1:500	CLIENT: Dolphin School
Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.	
PROPOSED SITE PLAN	
ADDRESS: Dolphin School Waltham Road, Hurst, RG10 0FR	
JOB No 1065	DRAWING NUMBER PL03 Rev A
A3 Sheet	



PROPOSED PLAN 1:100

NOTES
 THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS
 AND MAY NOT BE REPRODUCED WITHOUT PERMISSION



PLANNING

© Copyright TRACE Architects Ltd 2021



Basement Studio
 263 The Vale
 London
 W3 7QA
 0208 611 2946
 office@tracearchitects.co.uk
 www.tracearchitects.co.uk

1 The Hermitage
 Eastfield Lane
 Goring Heath
 RG8 7SU
 01491 579 544

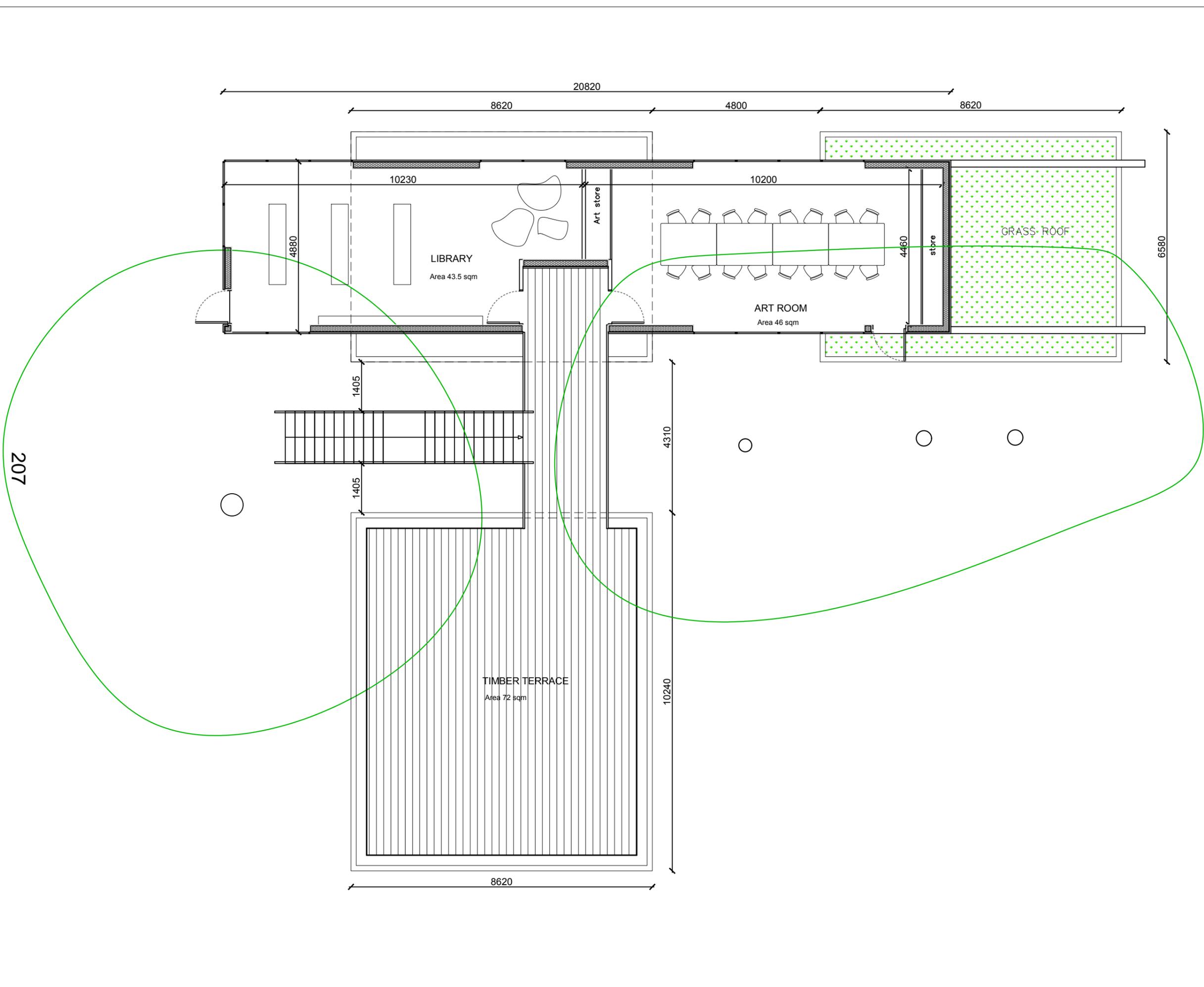
DATE	-	CLIENT:	Dolphin School
DRAWN	-		
CHECKED	-		
SCALE@A3	1:100		

Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.

PROPOSED GROUND FLOOR PLAN

ADDRESS:
 Dolphin School
 Waltham Road, Hurst, RG10 0FR

JOB No	DRAWING NUMBER
1065	PL04



NOTES
 THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS
 AND MAY NOT BE REPRODUCED WITHOUT PERMISSION



PLANNING

© Copyright TRACE Architects Ltd 2021



Basement Studio
 263 The Vale
 London
 W3 7QA
 0208 611 2946
 office@tracearchitects.co.uk
 www.tracearchitects.co.uk

1 The Hermitage
 Eastfield Lane
 Goring Heath
 RG8 7SU
 01491 579 544

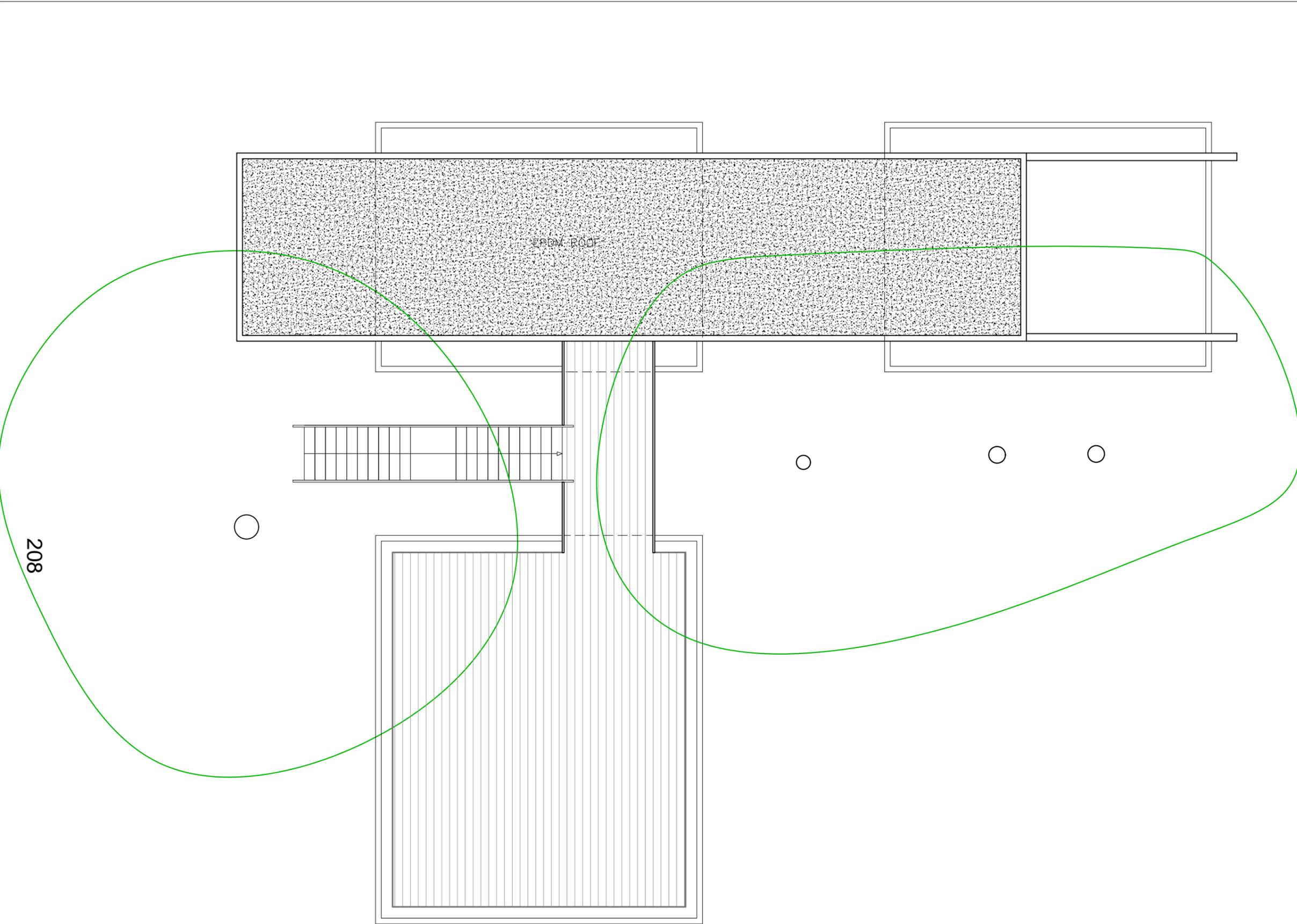
DATE	-	CLIENT:
DRAWN	-	Dolphin School
CHECKED	-	
SCALE@A3	1:100	

Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.

PROPOSED FIRST FLOOR PLAN

ADDRESS:
 Dolphin School
 Waltham Road, Hurst, RG10 0FR

JOB No	DRAWING NUMBER
1065	PL05



NOTES

THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS AND MAY NOT BE REPRODUCED WITHOUT PERMISSION

0 5

PLANNING

© Copyright TRACE Architects Ltd 2021

trace
ARCHITECTS

Basement Studio
263 The Vale
London
W3 7QA
0208 611 2946
office@tracearchitects.co.uk
www.tracearchitects.co.uk

1 The Hermitage
Eastfield Lane
Goring Heath
RG8 7SU
01491 579 544

DATE	-	CLIENT:
DRAWN	-	Dolphin School
CHECKED	-	
SCALE@A3	1:100	

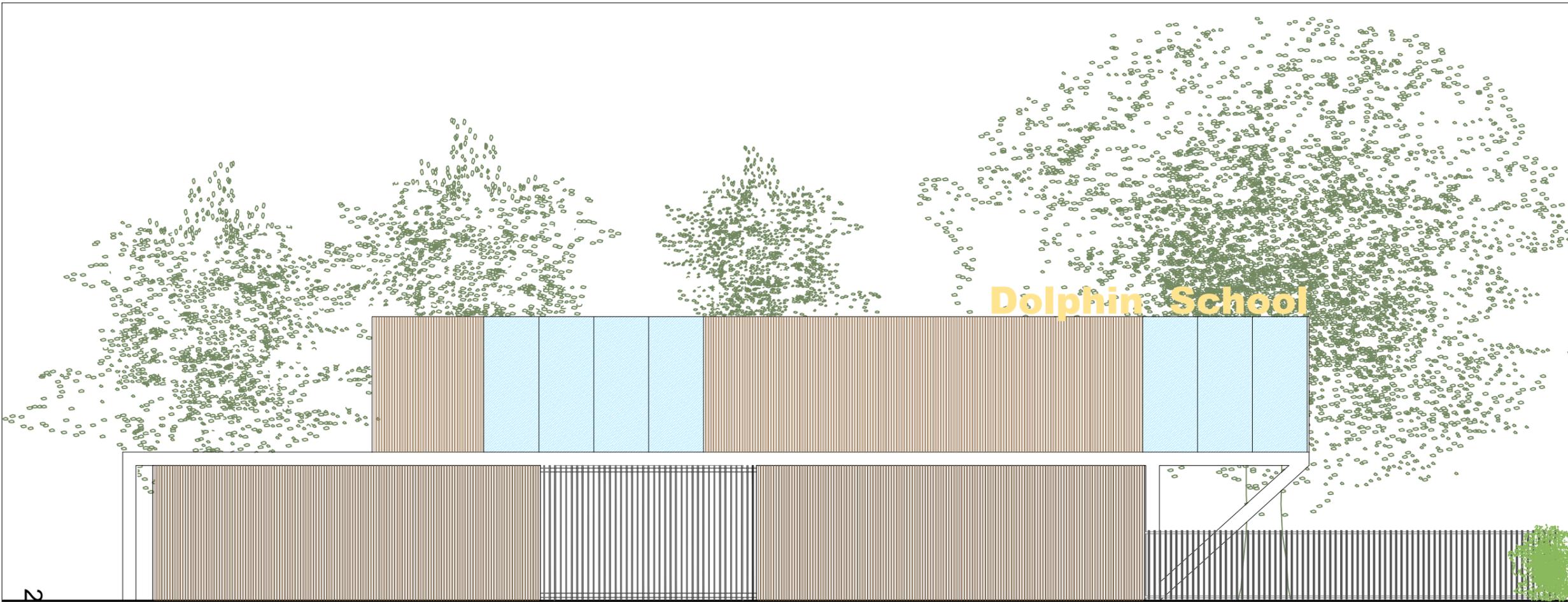
Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.

PROPOSED ROOF PLAN

ADDRESS:
Dolphin School
Waltham Road, Hurst, RG10 0FR

JOB No	DRAWING NUMBER
1065	PL06

A3 Sheet



PROPOSED NORTH EAST ELEVATION 1:100

209

NOTES

THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS AND MAY NOT BE REPRODUCED WITHOUT PERMISSION

THE CONTRACTOR IS RESPONSIBLE FOR CHECKING DIMENSIONS, TOLERANCES AND REFERENCE. ANY DISCREPANCIES TO BE CHECKED BEFORE PROCEEDING WITH THE WORKS

WHERE AN ITEM IS COVERED BY DRAWINGS TO DIFFERENT SCALES, THE LARGER SCALE DRAWING IS TO BE WORKED TO

DO NOT SCALE FROM DRAWING. FIGURED DIMENSIONS TO BE WORKED TO AT ALL TIMES

ALL WORK AND MATERIALS TO BE IN ACCORDANCE WITH THE BUILDING REGULATIONS AND TO COMPLY WITH THE RELEVANT CODES OF PRACTICE AND BRITISH STANDARDS



FEASIBILITY

© Copyright TRACE Architects Ltd 2019



86-88 Lower Mortlake Rd
Richmond
London
TW9 2JG
TEL 0208 611 2946
E-MAIL info@tracearchitects.co.uk
www.tracearchitects.co.uk

12A Hart Street
Henley-on-Thames
Oxfordshire
RG9 2AU
TEL 01491 579 544

DATE	-	CLIENT:
DRAWN	-	DOLPHIN SCHOOL
CHECKED	-	
SCALE@A3	1:100	

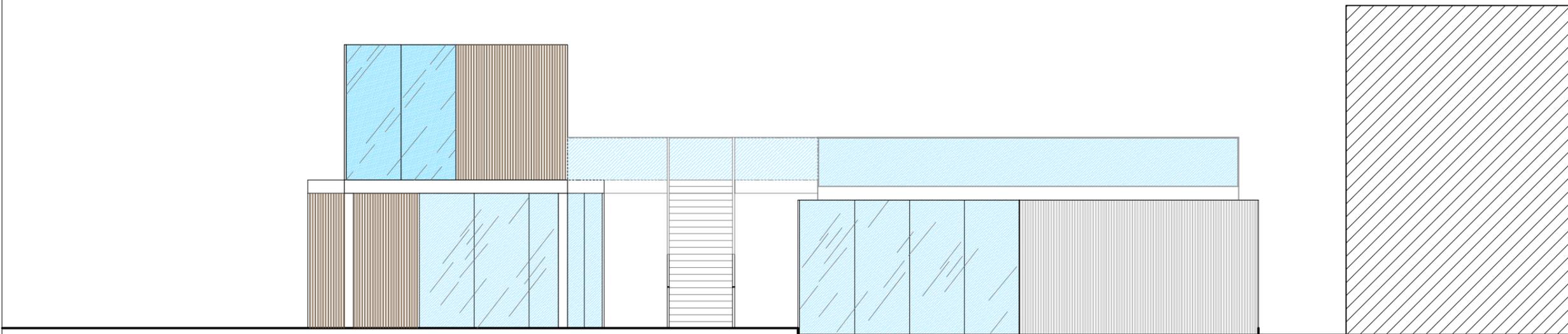
PROPOSAL FOR ADDITIONAL UNITS TO THE SCHOOL

PROPOSED NORTH WEST & NORTH EAST ELEVATIONS

ADDRESS:
Dolphin School
Waltham Road, Hurst, RG10 0FR

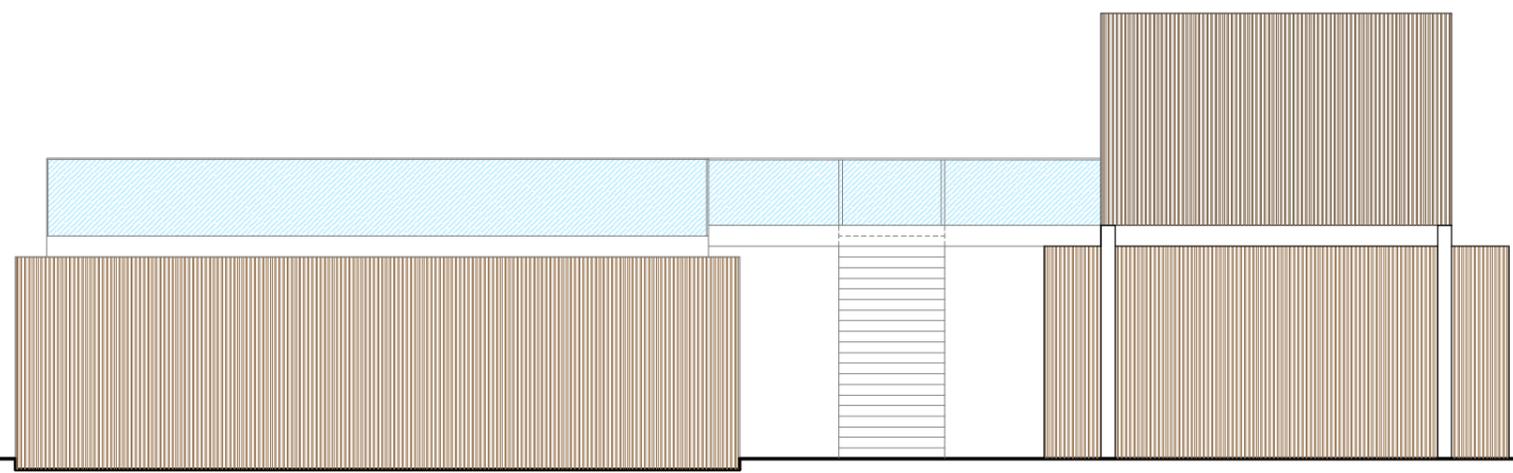
JOB No	DRAWING NUMBER
1065	PL07

A3 Sheet



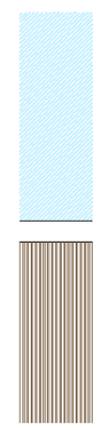
PROPOSED NORTH WEST ELEVATION 1:100

NOTES
 THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS
 AND MAY NOT BE REPRODUCED WITHOUT PERMISSION



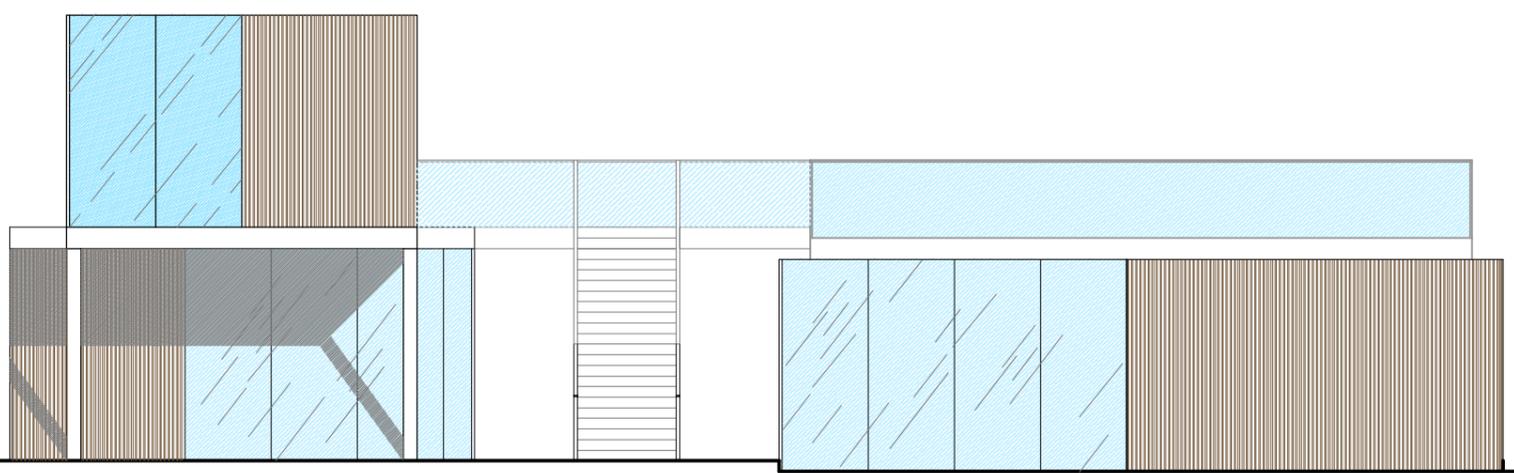
SOUTH EAST ELEVATION 1:100

210



GLAZING

TIMBER CLADDING



NORTH WEST ELEVATION 1:100

PLANNING

© Copyright TRACE Architects Ltd 2021



Basement Studio
 263 The Vale
 London
 W3 7QA
 0208 611 2946
 office@tracearchitects.co.uk
 www.tracearchitects.co.uk

1 The Hermitage
 Eastfield Lane
 Goring Heath
 RG8 7SU
 01491 579 544

DATE	-	CLIENT:	Dolphin School
DRAWN	-		
CHECKED	-		
SCALE@A3	1:100		

Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.

PROPOSED ELEVATIONS

ADDRESS:
 Dolphin School
 Waltham Road, Hurst, RG10 0FR

JOB No	DRAWING NUMBER
1065	PL08 Rev A

A3 Sheet



ILLUSTRATIVE IMAGE OF PROPOSAL

NOTES
 THIS DRAWING IS THE COPYRIGHT OF THE ARCHITECTS
 AND MAY NOT BE REPRODUCED WITHOUT PERMISSION

PLANNING

© Copyright TRACE Architects Ltd 2021

trace
 ARCHITECTS

Basement Studio
 263 The Vale
 London
 W3 7QA
 0208 611 2946

1 The Hermitage
 Eastfield Lane
 Goring Heath
 RG8 7SU
 01491 579 544

office@tracearchitects.co.uk
 www.tracearchitects.co.uk

DATE	-	CLIENT:
DRAWN	-	Dolphin School
CHECKED	-	
SCALE@A3	As Noted	

Proposed erection of a two storey building to create additional classrooms, toilets and library facilities, with associated roof terrace.

CGI

ADDRESS:
 Dolphin School
 Waltham Road, Hurst, RG10 0FR

JOB No	DRAWING NUMBER
1065	PL09

This page is intentionally left blank

PLANNING REF : 210805
PROPERTY ADDRESS : 9 Primrose Lane
: Winnersh, Wokingham
: RG41 5UR
SUBMITTED BY : St Nicholas Hurst Parish
DATE SUBMITTED : 14/04/2021

COMMENTS:

St Nicholas Hurst Parish Council has no adverse comments relating to application 210805.

This page is intentionally left blank

Agenda Item 9.

Application Number	Expiry Date	Parish	Ward
210448	12 th July 2021	Earley	Bulmershe and Whitegates

Applicant	Mr M. Mand.
Site Address	57 Chiltern Crescent, Earley, RG6 1AL, Wokingham
Proposal	Householder application for the proposed loft conversion to create habitable accommodation with rear dormer extension, hip to gable conversion and the installation of 2no. roof lights.
Type	Householder
Officer	Benjamin Hindle
Reason for determination by committee	Listed by Councillor Shirley Boyt for the following reason: <ul style="list-style-type: none"> - Impacts to the character of the area from the introduction of a new roof form (gable end) to the application site. - Overbearing impacts as a result of the scale and position of the proposed.

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 th June 2021
REPORT PREPARED BY	Assistant Director of Place

SUMMARY

This planning application is a Householder application for the proposed loft conversion to create habitable accommodation with a rear dormer extension, hip to gable extension and the installation of 2no. roof lights and is before the planning committee as requested by Councillor Boyt, for the reasons set out above

The application site consists of a moderately sized three-bedroom, two storey dwelling located on Chiltern Crescent in Earley.

The proposed scheme is in keeping with the existing dwelling and would not be a prominent feature in the street scene. This would maintain the overall character and appearance of the area.

The proposed has no detrimental impact on the neighbouring amenities of the adjacent neighbours concerning overshadowing, overbearing or overlooking.

Parking provision is adequate with 3 spaces of 2.5x5 metres shown as existing to be retained for the development. This will effectively serve the proposed 4 bedrooms in line with the Local Authority's parking standards.

The garden depth is deemed acceptable to serve the residents of the dwelling at 225 metres squared and 25 metre length.

Paragraphs 1-28 provide further details to these material considerations. Subject to conditions 1-4, the application is recommended for approval.

PLANNING STATUS

- | |
|--|
| <ul style="list-style-type: none">• Major development location |
|--|

RECOMMENDATION:

That the committee authorise the **GRANT OF PLANNING PERMISSION** subject to the following Conditions and informatives:

Conditions:

1. Timescale - The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details - This permission is in respect of the submitted application plans and drawings numbered: 'CC-01R', 'CC-02R' and 'CC- 03R' received by the local planning authority on 22/02/2021. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. External materials - Except where stated otherwise on the approved drawings, the materials to be used in the construction of the external surfaces of the extension hereby permitted shall be of similar appearance to those used in the existing building, unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: To ensure that the external appearance of the building is satisfactory. Relevant policy: Core Strategy policies CP1 and CP3.

4. Parking as approved - No part of any building(s) hereby permitted shall be occupied or used until the vehicle parking space has been provided in accordance with the approved plans. The vehicle parking spaces as approved shall be permanently maintained and remain available for the parking of vehicles at all times.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

Informatives:

1. Whilst it would appear from the application that the proposed development is to be entirely within the curtilage of the application site, the granting of planning permission does not authorise you to gain access or carry out any works on, over or under your neighbour's land or property without first obtaining their consent, and does not obviate the need for compliance with the requirements of the Party Wall etc. Act 1996.
2. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF.

PLANNING HISTORY

041561 – Application for a certificate of proposed lawful development for the conversion of an existing garage to study – Approved - 03/02/2005.

32250 – Proposed two storey side extension and single storey front extension to dwelling plus new open porch – Approved – 23/07/2003.

SUMMARY INFORMATION

For Residential

Site Area – 400.1 metres squared

Existing parking spaces – 3

Proposed parking spaces – 3

CONSULTATION RESPONSES

None

REPRESENTATIONS

Town/Parish Council:

Object on the following grounds:

- Impacts to the character of the area as a result of the proposed roof form not being in keeping with the character of the area
- Overbearing impacts as a result of the scale of the proposed.
- Design of the proposed not being in keeping with the character of the area, or adjoining dwellings.

Local Members:

- Impacts to the character of the area from the introduction of a new roof form (gable end) to the application site.
- Overbearing impacts as a result of the scale and position of the proposed.

Neighbours:

One comment concerning:

- No notification received on 03/03/2021 (Officer Note: The application was advised correctly and letters were sent to all adjoining properties on 03/03/202)

- Incorrect existing plans as an existing conservatory on the application site is not shown (*Officer Note: Amended plans have been provided depicting the existing conservatory*)
- Party Wall Agreement required (*Officer Note: this is not a material consideration and is a civil matter between the respective parties*)
- Limited hours of work (*Officer Note: due to the minimal relative scale of the proposed, this condition is not considered necessary*).

ACER (Whitegates Residential Association):

Object on the following grounds:

- Impacts to the character of the area from the introduction of a new roof form (gable end) as well as the addition of an uncharacteristic third storey.
- Overlooking Impacts into the rear amenity space of garden to the rear out from the proposed fenestration to the rear.
- Design of the proposed not being in keeping with the character of the area, or adjoining dwellings.

APPLICANTS POINTS

- Similar examples are seen along Chiltern Crescent (images provided with application)
- The proposed would have no detrimental impact on adjoining neighbour amenity.
- Parking provision as recommended by WBC have been met with a total provision of 3 parking spaces to serve 4 bedrooms.
- The proposed is located to the rear of the dwelling and would not be appreciable from the street scene.
- The application site contains large rear amenity space of over 225sqm. This will effectively serve the development.

PLANNING POLICY

National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP6	Managing Travel Demand
	CP9	Scale and Location of Development Proposals
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC06	Noise
	CC07	Parking

		CC09	Development and Flood Risk (from all sources)
		CC10	Sustainable Drainage
Supplementary Documents (SPD)	Planning	BDG	Borough Design Guide – Section 4

PLANNING ISSUES

Site Description:

1. The application site consists of a moderately sized two storey semi-detached, three-bed dwelling located on Chiltern Crescent.
2. The current layout has a large open plan lounge and dining space, a kitchen, family room and en-suite bathroom. The First floor consists of three bedrooms, a bathroom and a study.
3. The existing rear amenity is large at over 225 metres squared, with a length of 25 metres.
4. The existing layout to the front of the dwelling is sufficient to serve 3 parking spaces of 2.5x5 metres. This is fully paved with hardstanding and utilises a shared drop kerb for access to the drive.
5. The site is located in an established residential area in the major development location of Earley. This is well built up and forms the appearance of a suburban estate.

Proposal Description:

6. This application seeks consent for a loft conversion to create habitable accommodation in the roof along with the erection of a rear dormer window extension, hip to gable extension and the installation of 2no. roof lights on the front roof slope.
7. The proposed rear dormer would be set down from the main ridgeline by 0.2 metres and in from the eaves by 0.3 and 0.2 metres respectively. Though the dormer is shown to be set in from the eave, this is facilitated by the hip to gable end conversion.

Principle of Development:

8. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.

9. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories. As the site is within a major development location, the proposal is acceptable in principle.
10. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale of activity, mass, layout, built form, height, materials and character to the area in which it is located and must be of high quality design without detriment to the amenities of adjoining land uses and occupiers.
11. The Borough Design Guide states that any alteration and extension to an existing building should be well-designed, respond positively to the original building, contribute positively to the local character and street scene, and relate well to the neighbouring properties. It also recommends that where there is a regular pattern to the built form, with a repeated building type on a consistent building line, then any alteration or extension that is visible from the street should not unbalance the rhythm of the frontage.
12. The Borough Design Guide states that side extensions should be set back from the building line by 1 metre, preferably with a lower roof line and should be at least 1 metre from the plot boundary.

Character of the Area:

13. The rear dormer extension complies with the Borough Design Guide in that it would be set down from the ridgeline by 0.2 metres and in from the eaves by 0.3 and 0.2 metres respectively. This is subservient to the extended dwelling and is in keeping with the character of the area when compared to various dormers along Chiltern Crescent, one of which features similar gable end (No.88). Though visible, the proposed dormer would not be appreciable from the street scene due to location at the rear. This would not detrimentally impact the character and appearance of the area.
14. The hip to gable end roof alteration would change the appearance of the principal elevation. Though part of the roof style is changing, it is to be noted that the actual end of No.57 (two storey side extension) will retain the hip style commonly seen along Chiltern Crescent. This will effectively match adjoining neighbours and be in keeping with the character of Chiltern Crescent.
15. The local character and roof styles are mixed (Chiltern Crescent contains examples of: cat slide, mono pitch end, gable end, flat roof, parapet end and hip end roof types), where very few of the existing semi-detached houses balance adjoining neighbours. Though there is only one gable end property along Chiltern Crescent (No.88) there are several along adjoining Bridges Grove and London Road within the directly local area to the application site. Therefore, with the existing mixture of style and design in

mind, the proposed roof alterations will not detrimentally impact this mixed character of the area.

16. The proposed loft conversion would facilitate use of the roof space as habitable accommodation. There is a wide variety of loft conversions along Chiltern Crescent and therefore this is not considered out of keeping to the character of the area.
17. Though the rear dormer is large in scale, the position away from the street scene and relative size when compared with the mass of the existing dwelling will not detrimentally impact the character of the area. Further, the flat roof style of the proposed dormer is a design shared with other existing dormers on Chiltern Crescent to which the proposed is in keeping with.
18. The additional fenestration present on the rear dormer would not be visible from the street scene, and therefore would not detrimentally impact the character of the area.
19. The Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) permits extensions and alterations to the roof, including dormer windows and hip to gable enlargements, as permitted development i.e. without the requirement for a planning application. The existing street scene is testament to this permitted development fall-back, with a number of properties having undertaken various works to their roof's e.g. the flank dormer window to No. 88. With this in mind, and the potential for any neighbour or resident along Chiltern Crescent to do the same under permitted development, the proposed cannot be considered detrimental to the character of the area.
20. To conclude, though the proposed does minimally change the appearance from the street scene, this cannot be considered as an adequate reason for refusal due to the theoretical fall-back position of any resident along Chiltern Crescent constructing a more contrived dormer under permitted development, wide mixed character of the area (including existing gable end example) and minimal relative scale.

Neighbouring Amenity:

21. The rear dormer would contain a Juliet balcony and one window to the rear. This is located approximately 55 metres from the rear elevation of No.296 London Road (also situated offset with no direct line of vision) and therefore will provide no detrimental overlooking impact to neighbours. This further complies with the Borough Design Guide, which recommends minimum back-to-back separation of 30 metres for properties of three floors.
22. The outlook from the Juliet balcony at second floor level is orientated towards the rear garden of the application site. This is an orthodox relationship for all residential properties set out in a traditional linear layout. Peripheral views from the fenestration of the dormer would not detrimentally impact neighbouring amenity by reason of loss of privacy.
23. Due to location on the ridge of the dwelling, the proposed would not interact with any neighbouring habitable rooms, and therefore cannot be said to detrimentally overbear.

24. The proposal does not fail the 45 degree to any neighbouring windows, as set out in the Borough Design Guide SPD and there would be no detrimental loss of light to any neighbouring windows. The proposed extension does not project any further than the existing rear elevation and there would be no detrimental loss of light to the neighbouring rear garden.

25. A representation letter advised that a condition should be including restricting the hours for development. The application is for a domestic extension and it is considered that such a condition would be not necessary or reasonable for the size of development proposed. Noise nuisance is controlled under other Environmental Health Legislation.

26. For the reasons set out, the proposal would not detrimentally impact the amenities of neighbouring residents.

Access and Movement:

27. The proposed would increase the amount of bedrooms by 1, from 3 to 4. The 3 spaces of 2.5x5 metres shown as existing are to be retained for the development. This will effectively serve the proposed 4 bedrooms in line with the Local Authority's parking standards and therefore is acceptable. A condition has been attached to ensure this is retained as approved at all times.

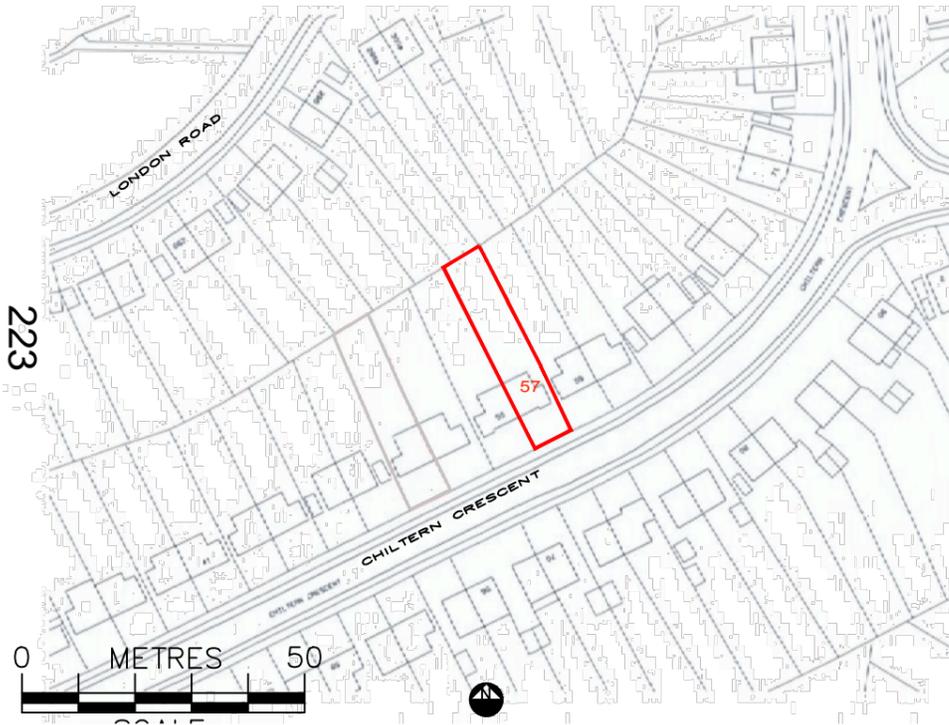
CONCLUSION:

The scheme would be subservient to the host dwelling and in keeping with the mixed character of the area, whilst having an acceptable impact on the neighbouring amenities of the adjacent neighbours. Parking provision is in line with the Local Authority's parking standards. Subject to conditions 1-4 outlined above, the application is therefore recommended for approval.

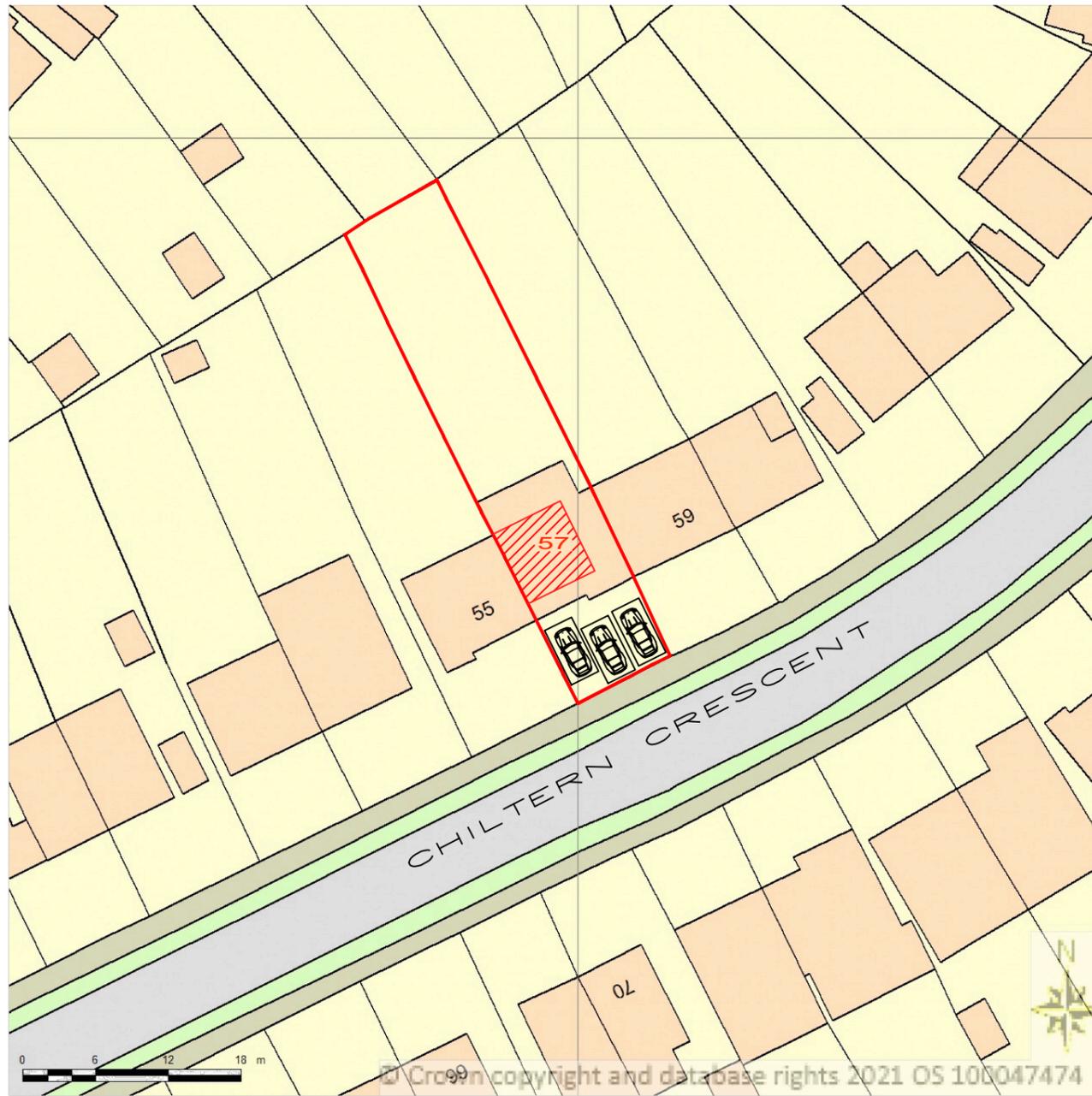
The Public Sector Equality Duty (Equality Act 2010)

In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. There is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.

CENTRE COORDINATES: 474498 , 173566

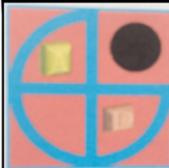


LOCATION PLAN
SCALE 1:1250@A3



BLOCK PLAN
SCALE 1:500@A3

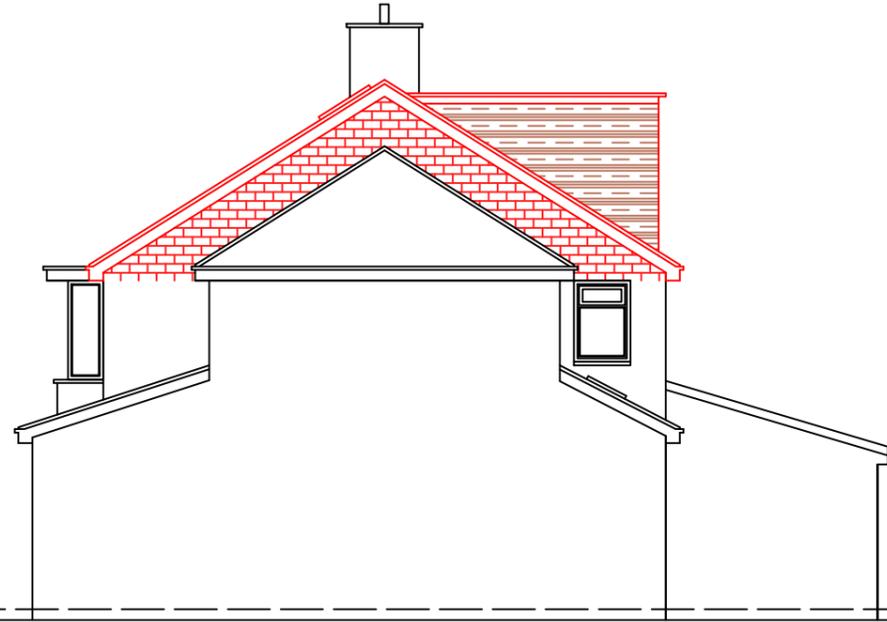
Supplied by Streetwise Maps Ltd
www.streetwise.net
Licence No: 100047474

	PROJECT:	57 Chiltern Crescent Earley	DRAWN BY:	P.S.	DRAWING TITLE:		ArchDezine Limited archdeziner7@gmail.com
	PROPOSED LOFT CONVERSION WITH REAR DORMER EXTENSION		DATE:	01/02/2021	LOCATION AND BLOCK PLAN		
			SCALE:	AS SHOWN@A3	DRAWING NUMBER:	CC-01R	
			FILENAME:	20077			

This page is intentionally left blank



PROPOSED FRONT ELEVATION

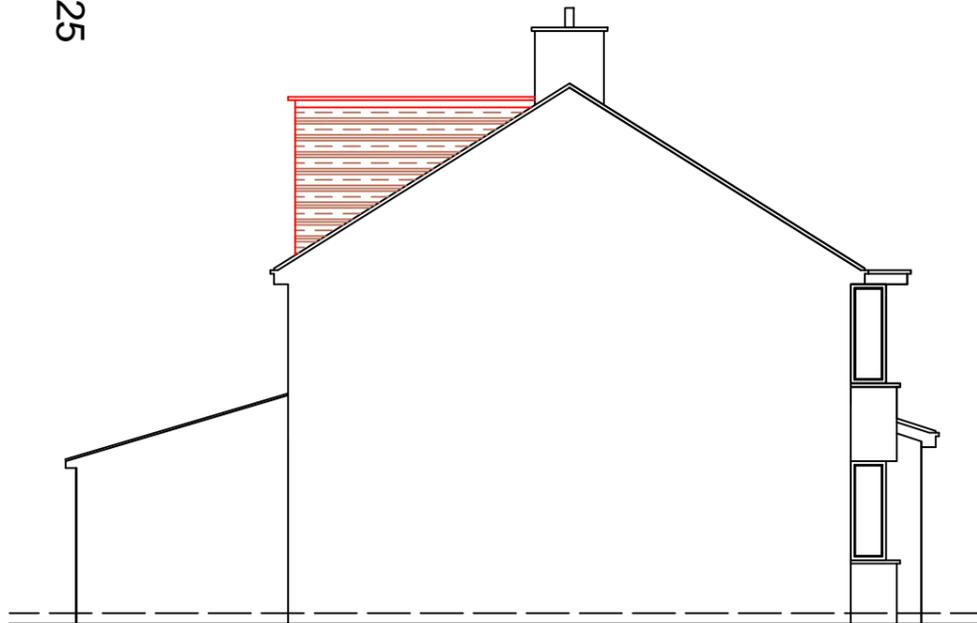


PROPOSED SIDE-1 ELEVATION

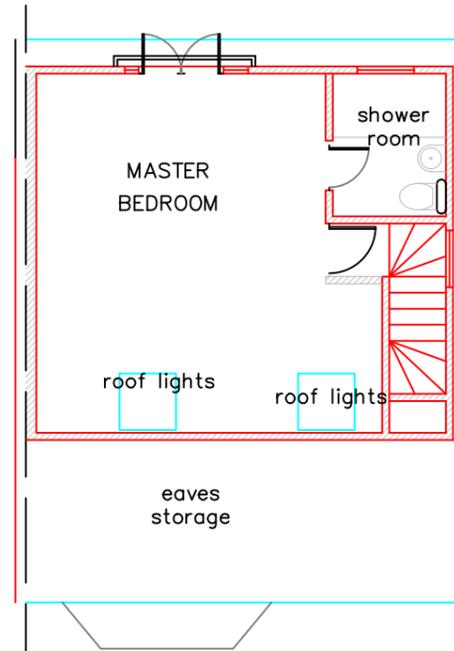


PROPOSED REAR ELEVATION

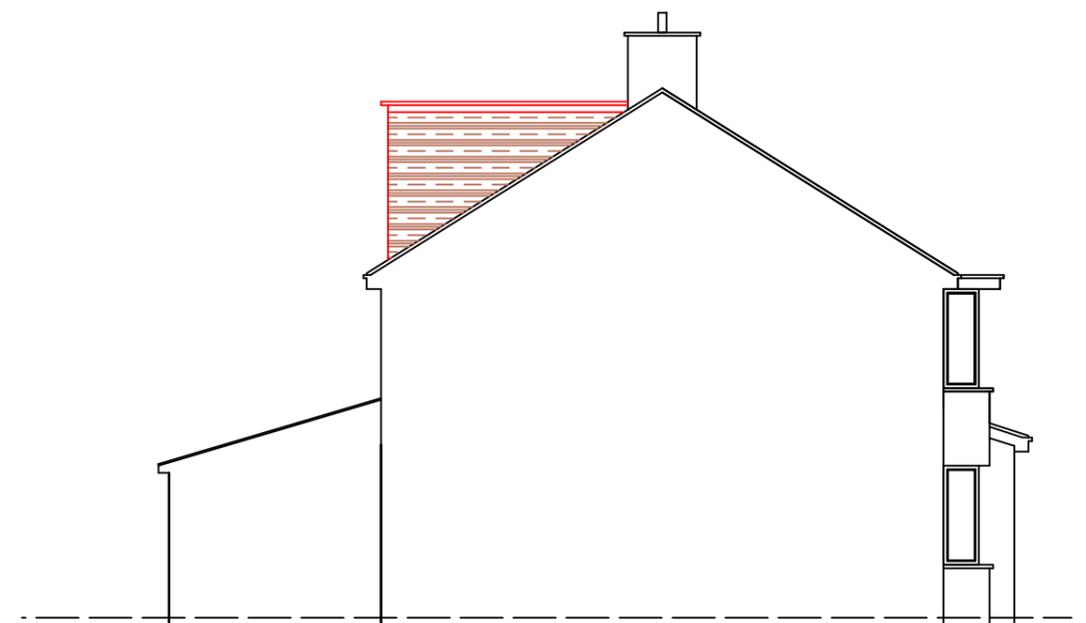
225



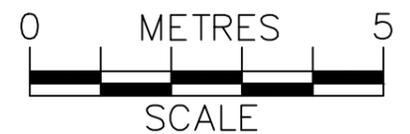
PROPOSED SIDE-2 ELEVATION PROFILE

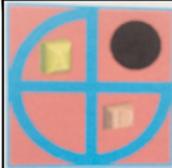


PROPOSED LOFT FLOOR PLAN



PROPOSED SIDE-2 ELEVATION PROFILE



	PROJECT:	57 Chiltern Crescent Earley	DRAWN BY:	P.S.	DRAWING TITLE:		ArchDezine Limited archdeziner7@gmail.com	
	PROPOSED LOFT CONVERSION WITH REAR DORMER EXTENSION		DATE:	01/02/2021	PROPOSED PLANS & ELEVATIONS			
			SCALE:	1:100@A3	DRAWING NUMBER:	CC-03R1		
			FILENAME:	20077				

This page is intentionally left blank

PLANNING REF : 210448
PROPERTY ADDRESS : Radstock House
: Radstock Lane, Earley, Wokingham
: RG6 5UL
SUBMITTED BY : Earley Town Council
DATE SUBMITTED : 10/03/2021

COMMENTS:

Recommend Refusal due to the large and overbearing character of the roof dormer contrary to Policy CP3 and Design Policy R23, with regards to the design parameters for roof dormers. It is considered there is inadequate offset from the margins of the roof and is out of scale and character with the area, and does not respond sympathetically to the existing character or relate well to neighbours, contrary to Policy CP3 and Design Policy R23. the resultant gable ended roof is out of character with the general hip end roofs in the surrounding properties.

This page is intentionally left blank

Agenda Item 10.

Application Number	Expiry Date	Parish	Ward
210378	09/06/2021	Sonning	Sonning

Applicant	BREO TVP4 LTD & BREO TVP5 LTD
Site Address	Buildings 4 and 5 Microsoft Campus Thames Valley Park Drive Earley RG6 1WG
Proposal	Full application for proposed alterations to external areas to provide improved landscaping, outdoor gym, amenity and presentation areas, plus erection of refuse store and reconfiguration of car parking.
Type	Full
Officer	Baldeep Pulahi
Reason for determination by committee	Major application (Site area greater than 1 hectare)

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 June 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The proposal comprises of works to the external areas of Buildings 4 and 5 within the Microsoft Campus at Thames Valley Business Park which include the reconfiguration of existing parking provision and layout and enhanced landscaping and facilities including an outdoor gym and outdoor amenity and presentation area.</p> <p>The proposal satisfies Paragraph 80 of the National Planning Policy Framework and Policy CP15 of the Core Strategy through the improvement and refurbishment of the existing buildings to provide enhanced accommodation and facilities to attract new occupiers, to assist in developing and diversifying the local economy and generate new and additional employment. Therefore, the principle of development is acceptable.</p> <p>There are no objections to the scheme on Landscape and Trees, Ecology, Environmental Health, Highways and Drainage grounds. There were also no comments or objections received from Sonning Parish Council, the ward Member or surrounding properties. The application represents a positive outcome for the site and is recommended for approval subject to various pre commencement conditions for the reasons set out in this report.</p>

PLANNING STATUS
<ul style="list-style-type: none"> • Major development location • Core Employment Areas – Thames Valley Park • Bat Roost Habitat Suitability • Minerals Site Consultation Area • Flood zone 1

RECOMMENDATION
<p>That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:</p>

a) The following conditions and informatives:

Conditions

1. Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details

This permission is in respect of the submitted application plans and drawings received by the local planning authority on 22/02/2021 and numbered:

- a) Location and Site Plan PL0006
- b) Existing Landscape Plan PL0003

and the submitted application plans and drawings received by the local planning authority on 10/03/2020 and numbered:

- c) Proposed External Workout Space and Bin Store Site Layout Plan 106
- d) Building 4 - External Proposed Presentation and Staff Amenity Layout Plan 109a

and the submitted application plans and drawings received by the local planning authority on 13/04/2021 and numbered:

- e) Proposed Landscape Plan PL0004 Rev P2

and the submitted application plans and drawings received by the local planning authority on 15/04/2021 and numbered:

- f) Proposed Refuse Store Plans and Elevations PL0007 Rev P2

The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure the development is carried out in accordance with the application form and associated details hereby approved.

3. Materials

The materials to be used for the new bin store are to be in accordance with those specified on the approval drawings and application form.

Reason: In the interests of visual and neighbouring amenities and functional development. Relevant policy: Core Strategy CP3 and Managing Development Delivery Local Plan policy CC04.

4. Landscaping details

Prior to the commencement of development, full details of both hard and soft landscape proposals have been submitted to and approved in writing by the local planning authority. These details shall include hard surfacing materials. Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable. Non-invasive species are recommended.

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

5. Retention of trees and shrubs

No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

6. Protection of trees

- a) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
- b) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- c) Any fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning

authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

7. Landscape management

Prior to the commencement of the development a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

8. Parking and Turning (As Approved)

No part of the development hereby permitted shall be occupied or used until the vehicle parking and turning space has been provided in accordance with the approved plans. The vehicle parking and turning space shall be retained and maintained in accordance with the approved details and the parking space shall remain available for the parking of vehicles at all times and the turning space shall not be used for any other purpose other than vehicle turning.

Reason: To ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 and Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07 and Appendix 2 and the Council's Parking Standards Study Report (2011).

9. Cycle Parking (To be approved)

Prior to the occupation of the development hereby permitted details of secure and covered bicycle storage/ parking facilities for the occupants of and visitors to the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before occupation of the development hereby permitted and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

10. Electric charging details

Prior to the commencement of development, details for an Electric Vehicle Charging Strategy serving the development shall be submitted for approval in writing by the Local Planning Authority. This strategy should include details relating to on-site infrastructure, the location and installation of charging points and future proofing of the site unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 and Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07 and Appendix 2 and the Council's Parking Standards Study Report (2011).

Informatives

1. Changes to the Approved Drawings

The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

2. Pre-Commencement Conditions

The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

3. Discussion

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF.

PLANNING HISTORY – BUILDING 4		
Application No.	Description	Decision & Date
28578	Outline application for business park information age project, hotel and local centre and full application for country park, associated roads and engineering operations all at North Earley comprising former power station and adjoining land.	Approved 22/11/1988
RM/1997/66434	Reserved Matters on Approval 28578 for erection of building for B1 and ancillary use	Approved 04/03/1998
RM/2000/2543	Reserved matters on consent 28578 for the erection of B1 office accommodation and ancillary use (Amendment to consent RM/1997/66434)	Approved 30/11/2000
RM/2001/3310	Application for reserved matters on consent 28578 for the erection of B1 office accommodation and ancillary use (amendment to consent RM/2000/2543)	Approved 21/03/2001

203498	Full application for the proposed erection of a replacement front entrance canopy and reconfiguration of basement.	Approved 01/02/2021
--------	--	------------------------

PLANNING HISTORY – BUILDING 5		
Application No.	Proposal	Decision & Date
28578	Outline application for business park information age project, hotel and local centre and full application for country park, associated roads and engineering operations all at North Earley comprising former power station and adjoining land.	Approved 22/11/1988
RM/1997/66434	Reserved Matters on Approval 28578 for erection of building for B1 and ancillary use	Approved 04/03/1998
RM/2000/2543	Reserved matters on consent 28578 for the erection of B1 office accommodation and ancillary use (Amendment to consent RM/1997/66434)	Approved 30/11/2000
RM/2001/3310	Application for reserved matters on consent 28578 for the erection of B1 office accommodation and ancillary use (amendment to consent RM/2000/2543)	Approved 21/03/2001
210377	Full application for the proposed erection of a replacement front entrance canopy, reconfiguration of basement and provision of new external stairs and steps to basement level.	Approved 12/03/2021
210435	Application to remove condition 4 of planning consent VAR/2007/1716 for the Proposed variation to condition 9 of consent RM/2000/2543 for the use of part of the undercroft as a gymnasium. Condition 4 refers to the fitness centre and the variation is to widen the use of the gym	Pending Decision The determination of this application is subject to the outcome of 210378

SUMMARY INFORMATION	
Site Area	2.72 Hectares
Existing Floor Area	18,212 sqm2 (both buildings)
Proposed Floor Area	No change
Existing Parking Spaces	546
Proposed Parking Spaces	587
No. of jobs created/lost	No change

CONSULTATION RESPONSES	
WBC Drainage	No objections
WBC Environmental Health	No objections
WBC Cleaner and Greener	No comments received
WBC Highways	No objections subject to conditions 8 9 and 10
WBC Landscape and Trees	No objections subject to conditions 4 5 6 and 7
WBC Ecology	No comments received
Berkshire Archaeology	No objections

REPRESENTATIONS	
Parish/Town Council	No comments received
Ward Members	No comments received
Neighbours	No comments received

APPLICANTS POINTS
<ul style="list-style-type: none"> The buildings were formally occupied by Microsoft as part of their campus within the Thames Valley Park and these have been sold to BREO TVP4 Ltd and BREO TVP5 Ltd respectively who are proposing to upgrade and improve the accommodation and site surroundings to meet modern standards to attract new occupiers to the park. The proposed works to the building and external areas will improve the sites environment and provide a healthy place to work.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP9	Scale and Location of Development Proposals
	CP15	Employment Development
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB11	Core Employment Areas
	TB12	Employment Skills Plan
	TB20	Service Arrangements and Deliveries for Employment and Retail Use
Supplementary Planning Documents (SPD)	TB21	Landscape Character
	TB23	Biodiversity and Development
	BDG	Borough Design Guide

PLANNING ISSUES

Description of Development

1. The proposal seeks planning permission for works to the external areas of the site. This includes improving the amenity and landscaping provision externally for users of the building and reconfiguration of the car parking, delivery servicing facilities, bin storage provision and additional car parking.
2. The proposals will provide an additional 41 car parking spaces associated with both buildings, increasing the car parking provision from 546 spaces to 587 spaces.
3. The loading area for both buildings to the rear of the site that caters for deliveries and refuse collection is to be reconfigured to serve the buildings needs and cater for the new bin enclosure, which is to replace the current open bin storage areas.
4. The proposal will deliver improved facilities for building users and enhanced landscaping. This includes the provision of an outdoor exercise area, outdoor amenity and presentation area which will allow employees to both host and watch outdoor presentations / conduct outdoor meetings ,improved pathways and pedestrian connections, and new soft landscaping.

Principle of Development

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. The site is located within the Thames Valley Park which is an identified Core Employment Area. Policy CP15 of the Core Strategy recognises the importance to secure a range of employment facilities and opportunities.
7. The proposal satisfies Policy CP15 of the Core Strategy through the improvement of existing external areas to provide enhanced facilities for existing and future occupiers and assists in developing and diversifying the local economy and generate new and additional employment. It also accords with Paragraph 80 of the National Planning Policy Framework in this respect; therefore, the principle of development is acceptable.

Character of the Area

8. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass layout, built form, height and character of the area and must be of high-quality design. Policy NR1 of the Borough Design Guide states development should respond to key characteristics and features of the site. NR2-NR12 are also applicable in terms of ensuring a positive entrance, presentation, car park layout, boundary treatments and servicing and appropriate height, roof form and materials that are compatible with the area.
9. This site is located within the Thames Valley Business Park, and Buildings 4 and 5 are located within the south-eastern part of the wider site. Buildings 4 and 5 are existing 4 storey commercial buildings with basements provided within external car

parking and landscaped sites. The site is surrounded by the remaining Microsoft Buildings 1-3 which are of a similar scale and design.

10. The proposed external works to the site and provision of additional amenities for users of the buildings will improve the use of the external areas of the site and make better use of underutilised areas. Specific details of these works are provided below:

11. Outdoor Gym

A new external gym space is proposed in place of the existing loading bay. The area will have an external area of 221.8sqm2 and will be hard landscaped, providing a flexible space for outdoor activity, workout classes and boot camps. New soft landscaping will be installed in place of the existing decking to creating an inviting entrance to the gym; the landscaping will be primarily wildflowers and prairie planting. Landscaping details are required in Condition 4.

12. Outdoor Amenity Space

The outdoor amenity and presentation area will be located to the south of the site in an area of existing staff amenity. The existing soft landscaping will be stepped to provide outdoor seating to improve the usability for staff of the buildings, to deliver an improved space to gather, to take a break and eat lunch and to watch presentations . The proposal in this location provides improved use of the space and an enhanced function for users of the building.

13. Connecting Pathways

Two new pathways are proposed to create a connection between Buildings 4 & 5 and the park. This will allow for a safe pedestrian route. Subject to final hard landscaping details in Condition 4, there are no concerns with this layout.

14. Bin storage:

The bin storage will be 11 wide and 2.1m high. Due to its modest height and footprint, it would not result in harm to the character of the wider commercial setting. The proposed materials consisting of timber cladding are considered to be acceptable within the commercial setting.

15. Car Park:

The works to the car park will be achieved through a number of areas of reconfiguration of the external site. The majority will be delivered through the formalisation of informal parking on existing hard surfacing areas within the site and the additional spaces will be provided where the former access points to the neighbouring sites (Buildings 1-3) have been closed off as a result of the change in ownership and internalisation of the circulation.

16. Overall, the proposal will enhance the appearance , function and usability of the site, providing opportunities for enhanced fitness and wellness. In this respect it is acceptable in terms of Policy CP3 and the Borough Design Guide and no objections are raised.

Neighbouring Amenities

17. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and Policy TB20 of the MDD Plan requires that there be no harmful impact on the amenity of adjoining land uses in terms of noise, fumes and disturbance.

18. The proposed outdoor gym and outdoor amenity and presentation area would be located at least 90m away from the nearest commercial building (Building 3 Microsoft Campus) and 340m from the nearest residential property. At this distance and against the backdrop of train noise, it is considered there would be no adverse impact upon this property in terms of outlook, privacy and noise.

Access and Movement

Car Parking

19. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards.

20. The proposal will make use of a number of areas of the site which are underutilised to provide an enhanced car parking layout.

21. The proposal will provide an additional 41 car parking spaces associated with both buildings, increasing the car parking provision from 546 spaces to 587 spaces (including the internal parking spaces within Building 4 and 5). The Council's Parking Standards recommends 607 spaces for the buildings however the original permission for the buildings was approved with fewer parking spaces than this. The additional 41 spaces will meet the Council's Parking Standards and is deemed to be acceptable by the Council's Highways Officer. Two of these additional spaces are assigned disabled spaces which are deemed to be acceptable against the standards in Table 1.2 of the MDD Local Plan, which requires a total of 2.8 spaces.

22. The increase will be achieved through a number of areas of reconfiguration of the external site. The majority are delivered through the formalisation of informal parking on existing hard surfacing areas within the site, whilst additional spaces are provided where the former access points to the neighbouring sites (Buildings 1-3) have been closed off as a result of the change in ownership and internalisation of the circulation.

23. Electric vehicle charging points will be required based on the overall number of new parking spaces, the Council's emerging standards recommend a minimum of 2 x passive and 2 x active EV charging points. As such details are to be secured via Condition 10.

Cycle Parking

24. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum cycle parking standards.

25. The provision of 6 x short-term cycle parking spaces is considered acceptable for public use of the proposed gym (subject of application 210435). This is in addition to the long-term cycle parking also provided in that application which is 34 cycle spaces. The details for secure cycle storage/facilities are to be acquired via Condition 9.

Access

26. Due to the separate ownership of Buildings 4 & 5 there is no longer a need for accesses to connect to the adjoining business units. The submitted plans indicate a one-way operation to ensure acceptable circulation of the car park and use of the

parallel parking spaces. No objections are raised by the Council's Highways Officer on this aspect.

27. The access for the service and delivery vehicles will be maintained and a new loading bay provided between the buildings. Swept paths have been demonstrated which are acceptable to the Council's Highways Officer.

28. Improved pedestrian facilities are proposed to the main entrance through the front car parking areas for each building which are deemed to be acceptable.

Flooding and Drainage

29. Section 10 of the NPPF, Policy CP1 of the Core Strategy and Policies CC09 and CC10 of the MDD requires flooding protection, sustainable drainage methods and the minimisation of surface water flow.

30. The site is located within Flood Zone 1. The proposed outdoor reconfigurations are already located on existing hardstanding and the surface water runoff is managed within the main sewer. Since there are no changes to the surface water drainage strategy and the existing method can continue to operate as above no objections are raised by the Council's Drainage Officer.

Landscape and Trees

31. Policy CC03 of the MDD Local aims to protect green infrastructure networks, retain existing trees and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character.

32. Buildings 4 and 5 have three protected Oak trees to the rear boundary of Building 4. The rear of Building 5 is protected woodland at Little Goggs. The Council's Landscape Officer has raised no objections to the proposal given there is no adverse impact upon any of the protected trees.

33. It is recommended there is additional tree planting for both the proposed outdoor gym area and outdoor presentation and staff amenity area. This would allow to mitigate local impacts of climate change and enhance visual mitigation for the existing buildings on the wider landscape. Details of the soft landscaping scheme is to be secured via Condition 4. An ongoing management scheme is also necessary to ensure that the benefits of the landscape enhancements are maintained as part of the precinct wide operations. See Condition 7.

Environmental Health

34. No objections have been raised by the Council's Environmental Officer on the grounds of contaminated land or noise.

Ecology

35. Policy TB23 of the MDD required the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider greener infrastructure network.

36. The site is located within a bat roosting habitat; however, the proposals represent no change to the existing buildings that could potentially host bat roosts including any minimal tree works capable of accommodating bats or nesting birds. The site is sufficiently removed from the Thames River and is located within an area of

significant hardstanding which is unlikely to have an adverse impact on ecological grounds therefore no objection is raised.

Archaeology

37. The site is within a wider area of archaeological potential, as demonstrated by previous archaeological investigations which identified significant prehistoric archaeology. The site of the proposals has been investigated and any archaeological features have been recorded and there is little chance of further deposits being affected by the proposed works, therefore no objections are raised on archaeological grounds.

Community Infrastructure Levy

38. Being a commercial development, the application is not liable for CIL payments.

The Public Sector Equality Duty (Equality Act 2010)

39. In determining this application, the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. There is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.

CONCLUSION

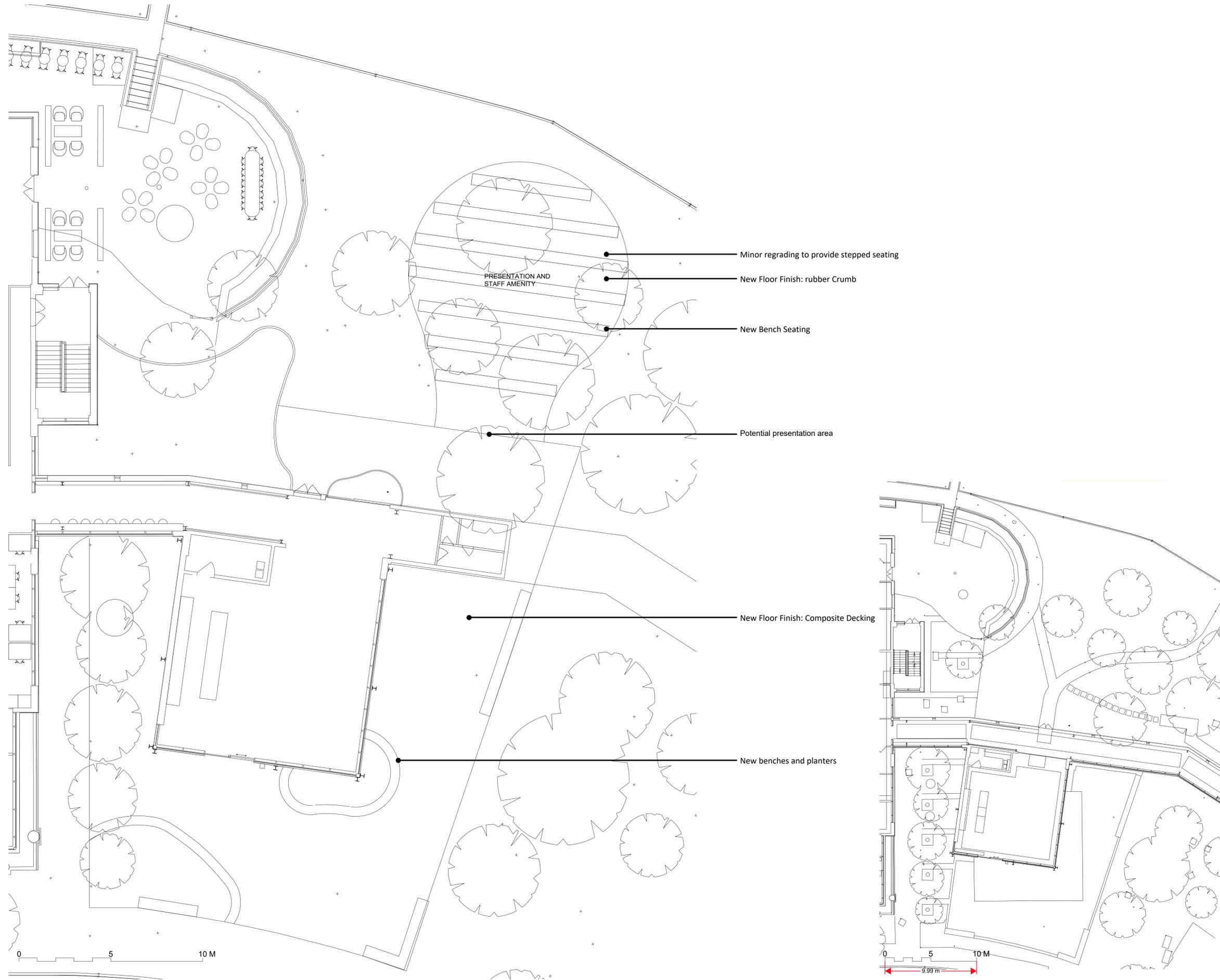
40. The proposal will enhance the appearance, functionality and usability of the site, providing opportunities for enhanced fitness and wellness. The proposal would not have a harmful impact on the landscape and built character and appearance of the wider area and the nearby commercial buildings and will maintain an acceptable level of parking provision.

41. No harmful impact would occur in regard to drainage, landscape and trees and environmental health therefore it is recommended that the application is approved subject to the conditions included in this report.

No implied license exists. This drawing should not be used to calculate areas for the purposes of valuation. Do not scale this drawing. All dimensions to be checked on site by the contractor and such dimensions to be their responsibility. All work must comply with relevant British Standards and Building Regulations requirements. Drawing errors and omissions to be reported to the architect.

Revisions:
For Information 09/02/21

241



Proposed Plan, 1:100

Existing Plan, 1:200

Project
Thames Valley Park

Drawing
Building 4
External Proposed Presentation
and Staff Amenity Plan

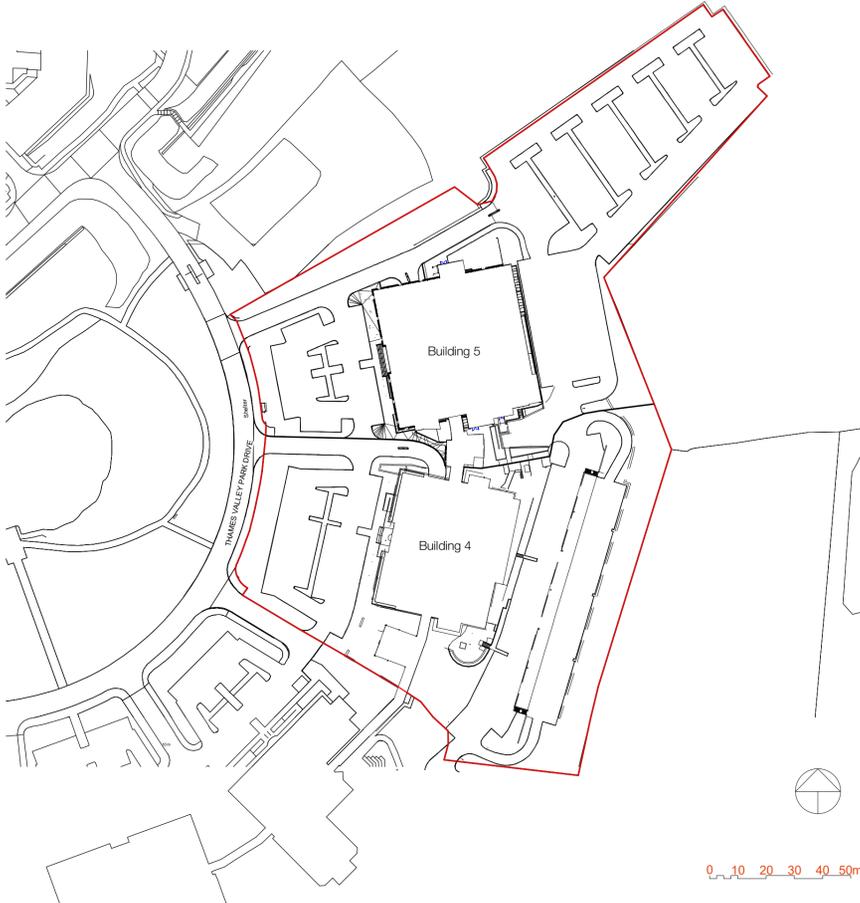
Scale	Date
1:100	11.09.20

Drawn by	Checked by
SP	MT

Job No.	Status	Purpose of Issue
200161	S3	Planning

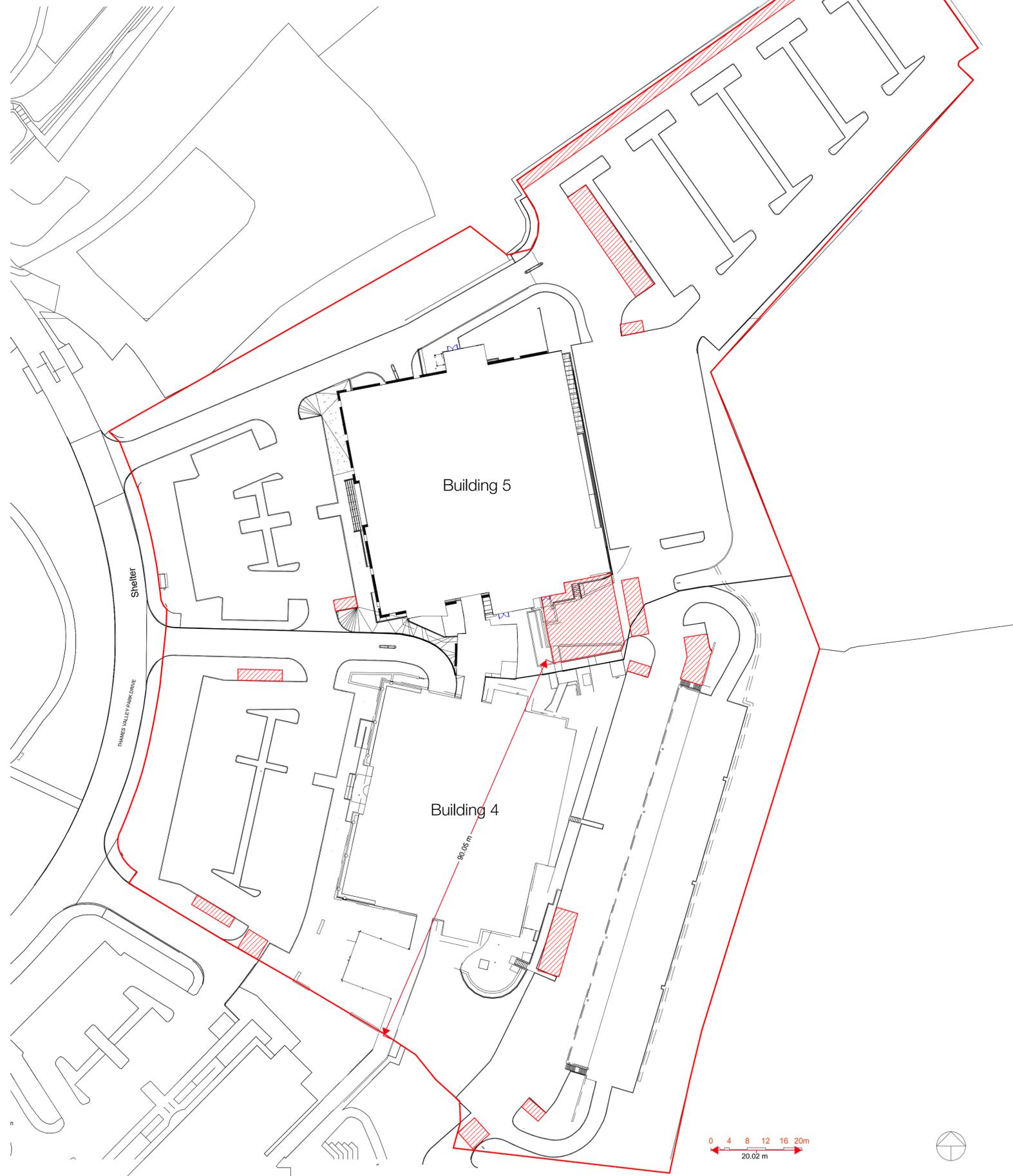
Drawing No. & Revision
TVPB4-HBA-B4-00-DR-A-04-109A

This page is intentionally left blank



243

Location Plan - 1:1250 @ A1



Site Plan - 1:500 @ A1

Copyright Hawkins\Brown Architects
 No implied licence exists. This drawing should not be used to calculate areas for the purposes of valuation. Do not scale this drawing. All dimensions to be checked on the site by the contractor and such dimensions to be their responsibility. All work must comply with relevant British Standards and Building Regulations requirements. Drawing errors and omissions to be reported to the architect. To be read in conjunction with Architect's specification and other consultant information.

Rev	Description	Date
P1	Planning Updates	15.02.21

- Key**
- Ownership Boundary
 - Site Boundary [Area = 27,033sqm]

159 St John Street
 London EC1V 4QJ
 mail@hawkinsbrown.com
 hawkinsbrown.com

Hawkins\Brown

Project
 Thames Valley Park Building 5
 Thames Valley Park, Reading

Drawing
 Existing Site and Location
 Plan_App#9

Scale @ A1 As indicated	Date 02/02/21
----------------------------	------------------

Drawn By Author	Checked By Checker
--------------------	-----------------------

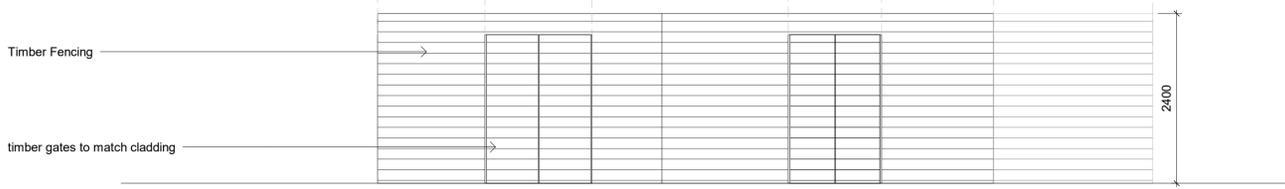
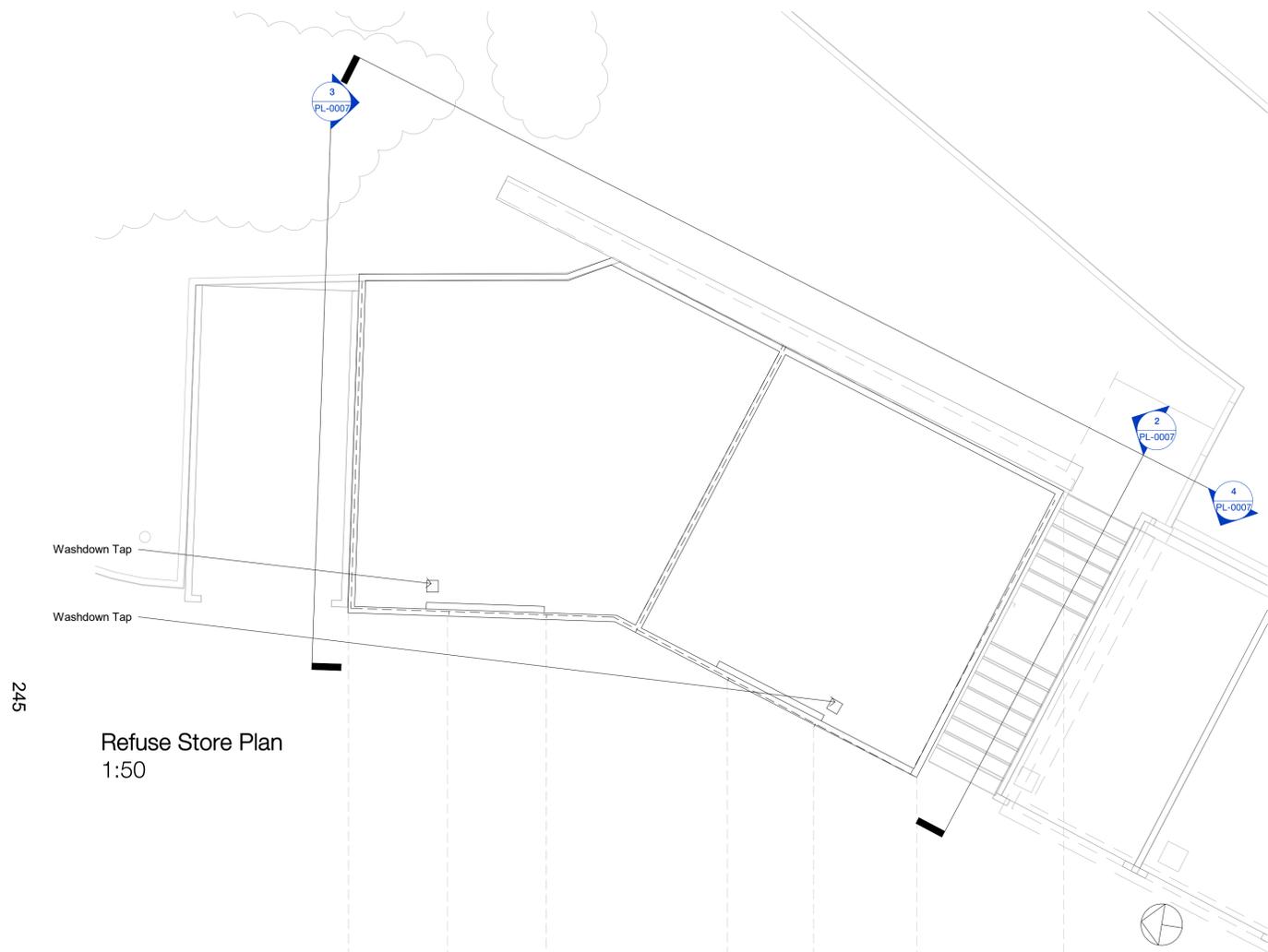
Job Number 200162	Status S1	Purpose of Issue Planning
----------------------	--------------	------------------------------

Drawing No. TVP-HBA-B5-SW-DR-A-PL-0006	Rev P1
---	-----------

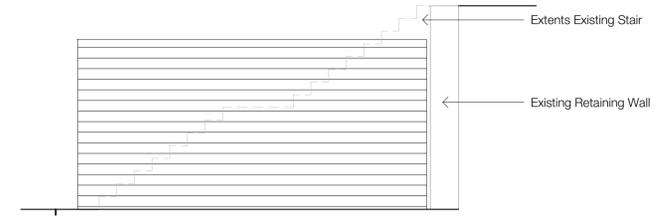
This page is intentionally left blank

Copyright Hawkins\Brown Architects
 No implied licence exists. This drawing should not be used to calculate areas for the purposes of valuation. Do not scale this drawing. All dimensions to be checked on the site by the contractor and such dimensions to be their responsibility. All work must comply with relevant British Standards and Building Regulations requirements. Drawing errors and omissions to be reported to the architect. To be read in conjunction with Architect's specification and other consultant information.

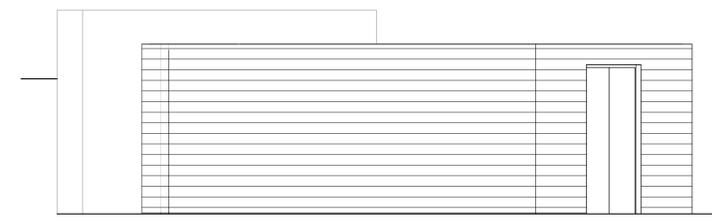
Rev	Description	Date
P1	Planning Updates	15.02.21
P2	Planning Update	09.03.21



Refuse Store Elevation 1:50



Refuse Elevation 02 1:50



Refuse Elevation 03 1:50



Refuse Elevation 04 1:50

159 St John Street London EC1V 4QJ
 mail@hawkinsbrown.com hawkinsbrown.com

Hawkins\Brown

Project
 Thames Valley Park Building 5
 Thames Valley Park, Reading

Drawing
 Proposed External Refuse Store

Scale @ A1 1 : 50	Date 02/17/21
----------------------	------------------

Drawn By WK	Checked By NR
----------------	------------------

Job Number 200162	Status S1	Purpose of Issue Information
----------------------	--------------	---------------------------------

Drawing No. TVP-HBA-B5-00-DR-A-PL-0007	Rev P2
---	-----------

This page is intentionally left blank

Copyright Hawkins\Brown Architects
 No implied licence exists. This drawing should not be used to calculate areas for the purposes of valuation. Do not scale this drawing. All Dimensions to be checked on the site by the contractor and such dimensions to be their responsibility. All work must comply with relevant British Standards and Building Regulations requirements. Drawing errors and omissions to be reported to the architect. To be read in conjunction with Architect's specification and other consultant information.

Rev	Description	Date
P1	Planning Issue	29.01.21
P2	Planning Update	07.04.21



- one way system
- Proposed Car Parking 2.4 x 4.8m
- Proposed Car Parking 2.5 x 5m
- Proposed Car Parking 2.5 x 6m

Existing External Spaces = 462
 Existing Internal Spaces Building 4 = 54
 Existing Internal Spaces Building 5 = 30

Total Existing = 546

Proposed External Spaces = 506
 Proposed Internal Spaces Building 4 = 47
 Proposed Internal Spaces Building 5 = 34

Total Proposed = 587

Additional Spaces = 41

159 St John Street London EC1V 4QJ mail@hawkinsbrown.com
hawkinsbrown.com



Project
 Thames Valley Park Building 5
 Thames Valley Park, Reading

Drawing
 Proposed Landscape Plan

Scale	Date
1 : 1250 @ A3	29/01/21

Drawn By	Checked By
WK	NR

Job Number	Status	Purpose of Issue
200162	S1	Planning

Drawing No.	Rev
TVP-HBA-B5-SW-DR-A-PL-0004	P2

Proposed Landscape Plan - 1:1250 @ A3

This page is intentionally left blank